

COMMITTEE OF THE WHOLE DECEMBER 5, 2005

**DRAFT PLAN OF SUBDIVISION FILE 19T-05V07
ZONING BY-LAW AMENDMENT FILE Z.04.045
SITE DEVELOPMENT FILE DA.05.038
2294 MAJOR MACKENZIE LTD.
REPORT #P.2005.61 & #P.2005.1**

Recommendation

The Commissioner of Planning recommends:

1. THAT Draft Plan of Subdivision File 19T-05V07 (2294 Major Mackenzie Ltd.) as shown on Attachment #4, BE APPROVED, subject to the conditions set out in Attachment #1.
2. THAT the Subdivision Agreement shall contain a provision requiring the Owner to pay to the City of Vaughan by way of certified cheque, cash-in-lieu of the dedication of parkland equivalent to 5% of the value of the subject lands for the residential component and 2% for the commercial component, prior to the issuance of a Building Permit, in accordance with the Planning Act and the City's Cash-In-Lieu Policy. The Owner shall submit an appraisal of the subject lands, in accordance with Section 42 of the Planning Act, prepared by an accredited appraiser for approval by the Vaughan Legal Department, Real Estate Division, and the approved appraisal shall form the basis of the cash-in-lieu payment.
3. THAT the following street name for Draft Plan of Subdivision File 19T-05V07 (2294 Major Mackenzie Ltd.), as shown on Attachment #12, BE APPROVED:

STREET

PROPOSED NAME

Street 'A'

Golden Spruce Lane (continuation of previously approved street name)

4. THAT Council pass the following resolution with respect to the allocation of sewage and water servicing capacity:

"NOW THEREFORE BE AND IT IS HEREBY RESOLVED THAT the proposed Draft Plan of Subdivision Application 19T-05V07 be allocated sewage capacity from the Maple Service Area of the York/Durham Servicing Scheme and water supply capacity from Pressure District No. 7 of the York Water Supply System, for a total of 27 townhouse units, following the execution of a subdivision agreement to the satisfaction of the City."

5. THAT Zoning By-law Amendment File Z.04.045 (2294 Major Mackenzie Ltd.) BE APPROVED, subject to the following:
 - a) That By-law 1-88, be amended, as follows:
 - i) rezone the subject lands from R1 Residential Zone to C1 Restricted Commercial Zone with an exception to permit a three-storey, mixed-use building abutting Major Mackenzie Drive with 315m² of ground floor business and professional office uses and eight (8) residential units above; and, four individual three-storey townhouse blocks comprising a total of nineteen (19) dwelling units within the interior of the site, as shown on Attachment #3;
 - ii) permit the required zoning exceptions to implement the proposed site plan shown on Attachment #3 as follows:

- deem the subject lands to be one lot for the purposes of zoning compliance regardless of the number of buildings on the lot, future severance or easements;
- permit a minimum front yard (Major Mackenzie Drive) of 1.9m; whereas 9m is required
- permit a minimum rear yard (north) of 7.5m; whereas 9m is required;
- permit a minimum building setback to an "R" Residential Zone of 7.5m; whereas 15m is required;
- permit a maximum building height of three (3) storeys;
- permit a maximum porch encroachment of 1.6m into the front yard and 2.4m into the rear yard;
- require a minimum of 10 parking spaces for the business and professional office use; whereas 11 spaces are required; and
- include any other zoning exceptions as may be required to implement the final approved site plan.

6. THAT Site Development File DA.05.038 (2294 Major Mackenzie Ltd.) BE APPROVED, subject to the following:

a) that prior to the registration of the site plan agreement:

- i) the Development Planning Department shall approve the final site plan, landscaping plan, and building elevations;
- ii) the landscape plan shall include a wood privacy fence around the perimeter of the site and between each residential townhouse unit to the satisfaction of the Development Planning Department and Engineering Department;
- iii) the Engineering Department shall approve the final site servicing and grading plan(s) and stormwater management report;
- iv) all hydro requirements of PowerStream Inc. shall be satisfied;
- v) all access and transportation requirements of the Region of York Transportation and Works Department shall be satisfied; and
- vi) the related Draft Plan of Subdivision 19T-05V07 shall have been registered; and

b) That the site plan agreement include the following provisions:

- a clause requiring the Owner to agree to prepare the appropriate reference plan(s) and provide all easements (vehicular, maintenance, etc.), as may be required in favour of the lands to the east (10028-10036 Keele Street), prior to the issuance of a building permit; and,
- a clause requiring the Owner to implement the final recommendations of the approved Noise Study.

Economic Impact

There are no requirements for new funding associated with this report. The proposed development will add new assessment to the local tax base.

Purpose

The Owner has submitted the following applications to implement the proposed development:

1. A Zoning By-law Amendment Application to rezone the subject lands shown on Attachment #3 from R1 Residential Zone to C1 Restricted Commercial Zone with a site-specific exception;
2. A Draft Plan of Subdivision Application for the subject lands shown on Attachment #4 to create one block, under a single registered M-Plan, which will facilitate the development of the 0.64 ha parcel for a mixed-use, 27-unit townhouse and office/commercial development, on a common element condominium road; and,
3. A Site Development Application to facilitate the development of the subject lands shown on Attachment #3 with the following:
 - i) two, three-storey mixed-use buildings having 315m² of ground floor business and professional office uses and eight (8) residential units above, along the Major Mackenzie Drive frontage; and,
 - ii) four separate three-storey street townhouse blocks with a total of nineteen (19) units located within the interior of the site.

Background - Analysis and Options

The subject lands shown on Attachment #2 are located on the north side of Major Mackenzie Drive, west of Keele Street, (2294 Major Mackenzie Drive), being Part of Lot 21, Concession 4, City of Vaughan. The vacant 0.64ha rectangular-shaped parcel has 70m frontage on Major Mackenzie Drive, and an average lot depth of 99.3m.

The surrounding land uses are:

- North - detached residential (R3 Residential Zone)
- South - Major Mackenzie Drive; Jason Street; existing mix of commercial and residential uses (C1 Restricted Commercial Zone and RM2(H) Multiple Residential Zone with a Holding Symbol "H")
- East - existing commercial – Shoppers Drug Mart (C1 Restricted Commercial Zone) and vacant lot/approved residential and commercial connecting to the subject proposal (C1 Restricted Commercial subject to Exception 9(1149))
- West - existing commercial (C1 Restricted Commercial Zone)

A Notice of Public Hearing for the Zoning By-law Amendment and Draft Plan of Subdivision Applications were circulated on December 23, 2004 and September 30, 2005, respectively. The notices were circulated to all property owners within 120 metres of the subject lands, and to the Maple Village Ratepayer Association and the Gates of Maple Ratepayers Association. To date, no comments have been received. The recommendations of the Committee of the Whole to receive the respective Public Hearing reports of January 17, 2005 and October 24, 2005, for the zoning subdivision applications, respectively and to forward a comprehensive report to a future Committee meeting were ratified by Council on January 24, 2005 and October 31, 2005, respectively.

Official Plan

The subject lands are designated "Maple Commercial Core Area" by OPA #350 (Maple Community Plan), as amended by OPA #533 (Maple Focus Review) which permits the proposed

mixed-use commercial/residential development. OPA #533 requires that the review of development applications in the Maple Core Area have regard to certain sections of the Maple Streetscape and Urban Design Guidelines. The relevant policies of the Maple Official Plan are identified below, as follows:

a) Proposed Land Use

OPA #350 as amended, permits mix-use development to a maximum height of three storeys.

Section A, Planning Strategy and Policies, Subsection III, Paragraph 6, of OPA #350 states the following with respect to the Maple Commercial Core Area (in part):

“The vitality and function of the existing commercial areas of Maple at the Major Mackenzie Drive and Keele Street crossroads is to be reinforced by encouraging the redevelopment and intensification of property for businesses, residential and commercial purposes.”

The Maple Community Plan also includes a policy under Section II, Goals, subparagraph f), which identifies the provision of a full range and mix of housing types as a goal of the plan.

The proposed mixed use development at a height of 3-storeys conforms to the Official Plan.

b) Minimum Frontage Requirement

The Official Plan requires that buildings comprise a minimum of 70% of the lot frontage at grade level for street-related retail, office and service uses. The main buildings on Major Mackenzie Drive include grade related business and professional office units with a length equal to 78% of the lot frontage.

c) Vehicular Circulation

OPA #350, as amended, requires that a vehicular circulation system be developed that minimizes impact on Keele Street and Major Mackenzie Drive. The proposal contemplates business and professional office uses and a total of 27 residential units with one access onto Major Mackenzie Drive, and with a road connection to the adjacent approved development (Tonlu Holdings Ltd., File DA.04.069), which has a single access to Keele Street.

d) Pedestrian Linkages

The Official Plan promotes the creation of new pedestrian linkages and references Sections 2.4, 2.6 and 2.7 of the Maple Streetscape and Urban Design Guidelines (MSUDG's). The proposed site plan includes unit pavers along the Major Mackenzie Drive frontage and hard surface landscape on both sides of the main driveway into the site. A sidewalk is also provided along the north side of the main road, which will ultimately provide a pedestrian connection from Major Mackenzie Drive to Keele Street.

The Owner has been requested by the Development Planning Department to extend the interlocking stone to be used in the sidewalk Major Mackenzie to the walkway on each side of the front buildings at the main access. The applicant is required to satisfy the requirements of the Maple Streetscape Guidelines with respect to the treatment of the Major Mackenzie Drive boulevard respecting, street furniture and materials.

e) Parking/Lay-by Parking

OPA #350, as amended, requires that adequate and convenient off-street parking facilities be provided and that regard be had for Sections 5.2 and 5.3 of the Maple Streetscape and Urban

Design Guidelines. The MSUDG's indicate that parking should be located at the rear of the buildings or in underground facilities. The parking areas for the proposed development are located behind the buildings and away from Major Mackenzie Drive.

As encouraged by the Guidelines, the Owner originally proposed two (2) lay-by parking spaces on Major Mackenzie Drive, however due to the proximity of these spaces to the main site access and to Jackson Street on the south side of Major Mackenzie Drive, the Region of York Transportation and Works Department advised that lay-by parking spaces at this location would be dangerous and conflict with vehicles turning left heading west-bound from Jackson Street, and that the lay-by spaces were to be removed. Accordingly, the applicant's proposal no longer includes the lay-by parking spaces.

f) Massing Scale and Building Height

Policies are included in the Official Plan respecting the massing, scale and building height of a new development in the Maple Commercial Core Area. The policies require that the scale and form of the building be complementary with adjacent development, that buildings fronting onto Major Mackenzie Drive not exceed three storeys, and that regard shall be had to Section 6.4 of the Maple Streetscape and Urban Design Guidelines. The main buildings on Major Mackenzie Drive are a maximum of three storeys in height, and are compatible with adjacent development from a scale and massing perspective, and also to the similar Tonlu development that is being constructed directly to the east. The townhouse blocks abut commercial development, and low density residential to the northwest of the site. The massing, design, and building form for the townhouse blocks is compatible with the adjacent development.

g) Site Plan Control

OPA #350, as amended, requires that development in the Maple Core Area be subject to a site plan agreement. The Applicant has applied for site plan approval (File DA.05.038), which is also the subject of this report.

h) Servicing

OPA #350 states that development or redevelopment will be permitted only where adequate water and sewer services are available. On November 14, 2005, Council adopted a Strategy for Designation/Allocation of Servicing Capacity which identified the subject application as a priority 2 category application and recommended that servicing capacity be reserved and allocated to at the site development application stage. In this respect, the Engineering Department has advised that servicing is available for the development, and an allocation resolution is provided in the recommendation section of this report.

OPA #350, as amended, further requires engineering plans (i.e. storm water management and grading) to be prepared for the proposed development. A condition of approval has been included requiring that the Engineering Department approve all necessary engineering plans.

Summary

In view of the above, the Development Planning Department is satisfied that the Zoning By-law Amendment and Site Development Applications are consistent with the development criteria in the Official Plan, as amended.

Region of York Official Plan

The Region of York Official Plan establishes as objectives, the need to promote a transit supportive urban structure that includes compact development, and a broad mix and range of housing including different housing forms, types and tenures to satisfy the needs of the Region's

residents. The Regional Plan identifies that the housing stock in the Region is primarily detached units. The housing market is faced with demands for a broader variety of housing forms to meet the needs of different kinds of households. The proposed development provides intensification on Major Mackenzie Drive and introduces live/work and townhouse units into the Maple Commercial Core Area, which are currently limited in availability. The proposal is consistent with the goals and objectives of the Regional Plan.

Provincial Policy Statement

The Provincial Policy Statement (PPS) includes policies that encourage residential intensification, densities which make more efficient use of land and public infrastructure, public transit supportive development, and the provision of a full range of housing types. The proposed development is consistent with the goals and objectives of the PPS.

Zoning

The subject lands are zoned R1 Residential Zone by By-law 1-88, which permits one single detached dwelling unit. The current proposal does not comply with the zoning by-law. By-law 1-88 does not include a mixed-use zone category and accordingly, an amendment to the zoning by-law is required. The Owner has submitted a Zoning By-law Amendment Application to rezone the subject lands from R1 Residential Zone to C1 Restricted Commercial Zone, and has identified the necessary site-specific zoning exceptions that will be required to implement the proposed site plan shown on Attachment #3. In order to implement the policies of the Official Plan, the Development Planning Department can support the rezoning of the subject lands to C1 Restricted Commercial Zone, with the following site-specific zoning exceptions to implement the mixed-use development, in the absence of mixed-use zoning standards in By-law 1-88:

- permit a minimum front yard setback to Major Mackenzie Drive of 1.9m; whereas, 9m is required;
- permit a minimum rear yard setback (north) of 7.5m; whereas 15m is required;
- permit a minimum setback to an 'R' Residential Zone (north of) 7.5m; whereas, 9m is required;
- permit a maximum building height of three (3) storeys;
- permit a maximum porch encroachment into the 1.9m front yard of 1.6m and into the 7.5m rear yard of 2.4m;
- require a minimum of 10 parking spaces for the business and professional office uses, whereas the by-law requires 11 spaces; and
- include any other zoning exceptions as may be required to implement the final approved site plan

A clause will be included in the implementing zoning by-law that deems the subject lands to be one lot for the purposes of zoning regardless of future conveyances, severances, etc. This will ensure that technical variances will not be created when the condominium application is considered.

The zoning exceptions noted above are considered to be technical in nature from the perspective that By-law 1-88 does not have a mixed-use zone category and the C1 Restricted Commercial Zone was not originally intended to implement the proposed form of development.

Subdivision Design

The Draft Plan of Subdivision shown on Attachment #4 consists of one 0.61ha block (Block 1) with 70m of frontage on Major Mackenzie Drive, an average lot depth of 99.3m, and a block for a Regional road widening. The ultimate development of Block 1 will consist of four (4) townhouse blocks (19 units) accessed by a private driveway, and two mixed-use buildings having

approximately 315m² of ground floor business and professional office uses and eight (8) residential units above, along the Major Mackenzie Drive frontage, as shown on Attachment #3.

The co-ordination of land use, servicing, and traffic connections with the adjacent easterly Tonlu lands, is being considered in the context of the Site Development Application (DA.05.038) and Zoning By-law Amendment Application (Z.04.045), as discussed in this report. Both applications facilitate the proposed townhouse development, which will be sited on freehold lots, with access onto a common element road, and provided with common amenity areas.

The proposed block within one registered M-Plan will enable the individual townhouse lots to be created through the lifting of Part Lot Control. The common blocks for the private road and amenity areas will be created through a Draft Plan of Condominium.

Compatibility

i) Land Use

The "Maple Commercial Core Area" designation, as amended by OPA #533 permits the proposed business and professional office and residential uses. The subject property is located adjacent to existing commercial uses to the west and east, Major Mackenzie Drive and existing low-density (detached dwellings) residential uses to the north and south.

From a planning perspective, including residential uses in the core area is appropriate and can result in many positive contributions, such as, increased pedestrian traffic and vitality; a population to support viable commercial uses and public transit initiatives; and, a reduced dependency on the automobile resulting from the close proximity of office, commercial and residential uses. The applications propose a three-storey mixed-use office/residential building fronting onto Major Mackenzie Drive, which is a development form often found on an arterial road in a core area. The interior of the site will be developed with a compact residential development in the form of three-storey townhouses.

The Official Plan policies encourage the proposed use in two specific ways. First, the Plan encourages a variety of housing types in the Maple area. The Region Official Plan and the Provincial Policy Statement further reinforce this policy objective. Second, the Plan encourages the redevelopment and intensification of the property for business, residential and commercial purposes at the Major Mackenzie Drive and Keele Street crossroads. The site is located on a major arterial road, providing future residents accessibility to many services.

In view of the above, the proposed land uses are compatible with adjacent land uses.

ii) Building Form

Compatibility of the proposed building form with adjacent development is important and required by the Official Plan. Compatibility of building forms does not necessarily mean that new development is identical to the existing development. Certain building designs, architecture and site planning techniques can successfully integrate a new structure with its surroundings.

The lands to the immediate west and the southerly half of the lands to the east of the subject lands are occupied with commercial buildings. The remainder of the easterly lands are approved and under construction for townhouse and mixed-use development as shown on Attachment #12 that is consistent with this proposal.

The proposed buildings are a maximum of three-storeys in height as permitted by the Official Plan, and the main buildings fronting onto Major Mackenzie Drive incorporate variations in the roofline and building height in order to provide relief to the massing of the building. The interior townhouse units are also three-storeys in height and there are many examples of townhouse

development successfully integrated adjacent to existing residential and commercial uses. Given the proposed building heights and those surrounding the site, the Development Planning Department is satisfied that compatibility between the existing and proposed structures from a building massing and form perspective is acceptable. Landscaping and fencing will be further utilized to achieve compatibility between the adjacent properties.

Site Design

The proposed site plan is shown on Attachment #3. The plan consists of six buildings developed around a private road providing access into the site from Major Mackenzie Drive. Abutting Major Mackenzie Drive are two mixed-use buildings, one on each side of the access road, which are three-storeys in height, and contain ground floor commercial uses and four residential units in each building. The building setback is close to front lot line (after the required road widening), and each has a porch structure in the front yard. Parking for the commercial units is provided at the rear of the buildings. A deck above the driveway and commercial parking spaces provides private amenity area for the residential live/work units.

The other four buildings located in the interior of the site are townhouse blocks containing a total of 19 units. These buildings will be three storeys in height, with at-grade access to a single car garage. The buildings are proposed to have an architectural design that is consistent with the two mixed-use buildings fronting onto Major Mackenzie Drive.

The site is accessed from a proposed private road that will ultimately connect Major Mackenzie Drive to Keele Street through the adjacent property to the east (Tonlu 10028-10036 Keele Street) for which, a Zoning By-law Amendment (File Z.03.094) and a Site Development Application (File DA.04.069) have been approved and construction has commenced. The overall concept plan for the two parcels is shown on Attachment #12. The two parcels will be developed independently and will contain reciprocal easements from each landowner to facilitate private vehicular movement between the two sites. A condition of site plan approval has been included in this respect.

The proposed parking area at the rear of the mixed/use buildings are immediately adjacent to proposed townhouse Units #9C and #27C as shown on Attachment #3. The applicant has provided landscaping to screen the parking area from the adjacent townhouse units.

It is intended that the project will proceed by way of a draft plan of condominium, which will be submitted at a future date.

Transportation

Access to the proposed development is from Major Mackenzie Drive, which is a Regional road. The Region of York Transportation and Works Department has reviewed the applications and has no objections to the development concept in principle.

As discussed earlier, the proposed site plan previously showed lay-by parking on Major Mackenzie Drive. The Region of York Transportation Works Department has advised that there is insufficient separation distance from the intersection of Major Mackenzie Drive and Jackson Street with the proposed site access (ie. no lay-by spaces within 15m either side of the site access), and have therefore, recommended that the lay-by parking spaces be deleted.

Building Elevations

The proposed building elevations are illustrated on Attachments #6 to #11 inclusive. The Major Mackenzie Drive elevations are comprised of a variety of materials containing brick, stone and stucco articulated in a manner to create visual interest to the building and address the massing of the structure. The elevations for each building includes half of the structure finished in red brick,

with the balance of each completed with stone at the ground floor level and stucco at the upper level as shown on Attachments #6 and #7. The Owner has not submitted plans identifying proposed signage requirements for the commercial portion of the buildings. However, the Owner has verbally advised that the commercial units will be identified using yard arm and gooseneck lighting fixtures. Prior to final approval of the proposed elevations by the Development Planning Department, the applicant must submit a detailed signage plan.

The four remaining buildings are comprised of townhouse blocks, shown on Attachments #8 to #11 inclusive with a total of 19 units, fronting onto a private road. These units are three-storeys in height with grade level garages and will be finished in red and buff masonry brick that is complementary to the mixed-use buildings fronting onto Major Mackenzie Drive. Parking for each unit is provided in a single car garage and a driveway space.

Maple Streetscape and Urban Design Guidelines (MSUDGs)

The Maple Community Plan, requires that development applications have regard for specific sections of the Maple Streetscape and Urban Design Guidelines. In addition, the balance of the guidelines are used to assess development proposals in the Major Mackenzie Drive and Keele Street corridors within Maple. The vision for the Maple Core Area from a physical built form perspective is implemented through the Maple Streetscape and Urban Design Guidelines. In this context, Council has established the Maple Streetscape Advisory Committee (MSAC) to implement the MSUDG's. The Committee has considered the proposed development, and is satisfied with the proposal. In addition, the Major Mackenzie Drive elevations have been reviewed and approved by Phillip H. Carter, the principle consultant for the Maple Heritage Conservation District Study.

Parking

The proposed site plan indicates that 71 parking spaces will be provided. By-law 1-88 requires that parking for the development be calculated as follows:

	<u>Required</u>	<u>Provided</u>
• 27 residential units X 1.5 spaces/per unit	= 41 spaces	54 spaces
• Visitor Parking @ 0.25 spaces/unit	= 7 spaces	7 spaces
• <u>Office Uses: 315m² @ 3.5 spaces/100m²</u>	<u>= 11 spaces</u>	<u>10 spaces</u>
TOTAL PARKING REQUIRED	59 spaces	71 spaces

Although, the overall parking supply complies with the By-law requirements, the amount of parking provided for the Office use is deficient by one (1) space. This parking deficiency is considered minor and a zoning exception is provided in this report.

The site plan currently identifies both the residential visitor and commercial parking as visitor spaces. Ultimately on-site demarcation is required to ensure on-site parking spaces are appropriately allocated for both residential visitor and commercial purposes. The final site plan must be revised to identify residential and commercial parking spaces, to the satisfaction of the Development Planning Department.

Landscaping

A landscape plan on shown Attachment #5 has been submitted to the City for review and approval. The plan provides for a street tree where possible and a rear yard landscaped patio for each townhouse unit. The plan must incorporate all the requirements of the Maple Streetscape and Urban Design Guidelines including the prescribed sidewalk treatment, street furnishing, lighting and landscaping, to the satisfaction of the Development Planning. Development Planning Staff is generally satisfied with the proposed landscape plan, however the additional walkway

treatments discussed earlier and the provision of additional privacy fencing around the site and between each unit must be addressed.

Environmental

a) Noise

A noise study has been prepared by Valcoustics Canada Inc., dated July 29, 2005 which has been approved by the Engineering Department.

b) Phase 1 Environmental Report

The Engineering Department has reviewed the Phase 1 and 2 Environmental reports prepared by Winchurch Environmental Inc. and the findings are acceptable.

Servicing

The Engineering Department has reviewed the proposed development plan and has advised that the subject lands is located within the Maple Service Area and is a natural tributary to the Keele Street Collector. Based on the current Vaughan Water Pressure Districts (2026 Condition) Plan, the development falls within the servicing area of Pressure District 7 of the York Water Supply System. The existing watermains, and storm and sanitary sewers on Keele Street will provide connections for this development. Connections to the existing infrastructure will be subject to confirmation that there are no downstream constraints in the existing system.

Financial contributions for the proportionate share towards any external municipal services that have been designed and oversized by others to accommodate the plan will be required from the subject development.

The Engineering Department has advised that allocation is available for this development and a resolution is provided in this respect

The Owner is required to contact PowerStream Inc. in order to assess the electrical requirements for the proposal. The Owner will be required to comply with all hydro requirements of PowerStream Inc.

Parkland Dedication

The implementing Subdivision Agreement will contain a provision that cash-in-lieu of parkland dedication in accordance with the Planning Act shall be paid at the rates stipulated in accordance with the City's approved Cash-In-Lieu of Parkland Policy.

Region of York

The Region of York has no objection to the approval of the draft plan of subdivision, or site plan approval, provided water and sewage capacity has been allocated, and subject to the conditions of approval provided in Attachment #1, and the Owner satisfying all access, transportation and servicing requirements in accordance with site plan approval.

School Boards

The York Region District School Board York Catholic District School Board and Conseil Scolaire de District Catholique Centre-Sud do not have any objections to the proposed draft plan of subdivision and site plan applications.

Canada Post

Canada Post Corporation has no objection to the proposed plan of subdivision, subject to the conditions of approval provided in Attachment #1.

Street Name

The Vaughan Development Planning Department and the Region of York have reviewed the proposed street name of "Golden Spruce Lane", which is identified as "Private Drive" on Attachment #3, and have no concerns regarding the name. The report includes a recommendation to approve the proposed street name, which will form an extension to the private road that was approved for the easterly adjacent Tonlu Holdings Ltd. (File No.s 19T-05V03, DA.04.069 and Z.03.094).

Relationship to Vaughan Vision 2007

This report is consistent with the priorities set forth in Vaughan Vision 2007, particularly 'A-5', "Plan and Manage Growth".

Conclusion

The Development Planning Department has reviewed the proposed applications in accordance with the goals and objectives of the Provincial Policy Statement, Regional Official Plan, OPA #350 as amended, and the Maple Streetscape and Urban Design Guidelines. The proposed development concept is considered to conform to the intent of the Maple Community Plan (OPA #350) and the Region of York Official Plan. The concepts inherent in the proposal, specifically as they relate to residential intensification and providing for a mix and broad variety of housing are further supported by the Provincial Policy Statement. With respect to density, the proposal complies with the Official Plan and Maple Streetscape and Urban Design Guidelines, which implements density through site design and by limiting the maximum building height to three storeys.

The Development Planning Department has reviewed the proposed application for Draft Plan of Subdivision to facilitate the creation of one (1) block, in consideration of the Official Plan, Zoning By-law 1-88, the proposed site plan, and the existing and planned land uses in the surrounding neighbourhood. The proposed single subdivision block will facilitate the creation of freehold townhouse lots to be created through the lifting of Part Lot Control, with the common blocks for the private road and amenity areas to be created through a Draft Plan of Condominium. The Development Planning Department is satisfied that the proposed Draft Plan of Subdivision, Zoning By-law Amendment with site-specific exceptions, and Site Development Plan is appropriate to facilitate a future townhouse and office/commercial mixed-use development.

Accordingly, the Development Planning Department can support the approval of Draft Plan of Subdivision application, subject to the conditions of approval provided in Attachment #1 and the Zoning By-law Amendment and Site Development applications, subject to the conditions of approval that are provided in the recommendation of this report.

Attachments

1. Conditions of Draft Approval
2. Location Map
3. Site Plan
4. Draft Plan of Subdivision 19T-05V07
5. Landscape Plan
6. Elevations - Block A
7. elevations - Block B
8. Elevations - Block C
9. Elevations - Block D
10. Elevations - Block E
11. Elevations - Block F
12. Comprehensive Design Scheme

Report prepared by:

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Respectfully submitted,

JOHN ZIPAY
Commissioner of Planning

MARCO RAMUNNO
Director of Development Planning

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ATTACHMENT NO. 1

**DRAFT PLAN OF SUBDIVISION 19T-05V07
2294 MAJOR MACKENZIE LTD.
PART OF LOT 21, CONCESSION 4, CITY OF VAUGHAN**

THE CONDITIONS OF THE COUNCIL OF THE CITY OF VAUGHAN THAT SHALL BE SATISFIED PRIOR TO THE RELEASE FOR REGISTRATION OF PLAN OF SUBDIVISION 19T-05V07, ARE AS FOLLOWS:

City of Vaughan Conditions

1. The Plan shall relate to the Draft Plan of Subdivision, prepared by Krcmar, Ontario Land Surveyors, Dwg. Name 05-009DM01, dated August 16, 2005 and red-lined December 5, 2005.
2. The lands within this Plan shall be appropriately zoned by a site-specific zoning by-law (Zoning By-law Amendment Application Z.04.045), which shall come into effect in accordance with the provisions of The Planning Act. The zoning category to be applied is as follows:
 - a) C1 Restricted Commercial Zone, including site-specific exceptions to facilitate the related site development application (DA.05.038).
3. The Owner shall pay any and all outstanding application fees to the Development Planning Department, in accordance with Tariff of Fees By-law 148-2005.
4. The Owner shall enter into a subdivision agreement with the City of Vaughan to satisfy all conditions, financial or otherwise of the City, with regard to such matters as the City may consider necessary, including payments of development levies, the provision of roads and municipal services, landscaping and fencing. The said agreement shall be registered against the lands to which it applies.
5. Prior to final approval, easements required for utility, drainage and/or construction purposes shall be created and granted to the appropriate authority(ies), free of all charge and encumbrances. The Owner shall advise that satisfactory arrangements have been made with a suitable telecommunication provider to provide their services underground at the approved locations and to the satisfaction of the City.
6. The "Private Drive" identified as Street "A" on Attachment #12 shall be named "Golden Spruce Lane". The Vaughan Development Planning Department and the Planning Department for the Region of York do not have an objection to the proposed name, which forms an extension of the subject name in the adjacent easterly subdivision plan.
7. Prior to final approval, or prior to the initiation of grading or stripping of topsoil, whichever comes first, the Owner shall submit an Erosion and Sedimentation Control Plan, including topsoil storage plan, detailing the location, size, side slopes, stabilization methods and time period, for approval by the City; topsoil storage shall be limited to the amount required for final grading with the excess removed from the site.
8. Prior to final approval of the Plan, the City and Region of York Transportation and Works Department-Water and Wastewater Branch shall confirm that adequate water supply and sewage treatment capacity are available to accommodate the proposed development.
9. The Owner shall agree in the subdivision agreement that the construction access shall be provided only in a location approved by the City and the Region of York Transportation and Works Department.

10. The Owner shall agree in the subdivision agreement that no building permits will be issued until the City is satisfied that adequate road access, municipal water supply, sanitary sewers and storm drainage facilities are available to service the proposed development.
11. Prior to final approval of the plan, or any phase thereof and prior to the initiation of any grading or any phase thereof, the Owner shall submit for review and approval of the City, the following:
 - a) A detailed engineering report(s) that describes the storm drainage system for the proposed development. The report shall include:
 - i) the manner in which stormwater will be conveyed from the site, including how this subdivision will be tied into existing development and how it conforms to the approved Municipal Services Report;
 - ii) appropriate Stormwater Management Practices (SWMP's) to be used to treat stormwater;
 - iii) the location and description of all outlets and other facilities;
 - iv) proposed methods of controlling or minimizing erosion and siltation on-site and in downstream areas during and after construction; and,
 - b) Overall grading plans for the subject lands. The Owner shall agree to carry out or cause to carry out, the recommendations set out in any and all aforementioned reports to the satisfaction of the City.
12. Prior to final approval of the plan, any and all appropriate revisions, as required, shall be made to the Municipal Services Report and all associated reports to the satisfaction of the City.
13. The Owner shall agree to design, purchase materials, and install a buried hydro distribution system, compatible with the existing and/or proposed systems in surrounding Plans, all in accordance with the latest standards and specifications of PowerStream Inc. (former Hydro Vaughan Distribution Inc.) and the City.
14. Prior to final approval, the Owner shall submit to the satisfaction of the City, a listing prepared by an Ontario Land Surveyor of the Block Area, Lot Frontage and Lot Depth in accordance with the approved Zoning By-law for the Block within the Plan.
15. The Owner shall agree to design, purchase materials and install a street lighting system along Major Mackenzie Drive, compatible with the existing and/or proposed systems in surrounding plans, all in accordance with City standards and specifications.
16. The Owner shall agree to erect fencing in the locations and of the types as shown on the approved construction drawing and as required by the City, in accordance with the Urban Design and Architectural Design Guidelines for the Maple Core.
17. Notwithstanding the general provisions (Subsection 5.1) that are typically included in the Subdivision Agreement, the City may issue model home building permits provided that the land is zoned to the satisfaction of the City and the appropriate conditions (Subsection 5.18) of the Subdivision Agreement are fulfilled.
18. The Owner shall agree to enter into a Site Development Agreement and any other necessary agreements, satisfactory to the City or any other appropriate authority, prior to any development within the plan.

The Agreement may deal with matters including, but not limited to, the following: engineering matters such as municipal services; road widening; construction and reconstruction; signals; grading; fencing; noise mitigation, and warning clauses; financial issues, such as cash contributions, levies (development charges); land dedications or reserves; securities, or letters of credit; planning matters such as residential reserve blocks, buffer blocks, site plan and landscape plan approvals, and conservation heritage matters.

19. Prior to final approval, the Owner shall not remove any vegetation or topsoil or start any grading of the lands, without a fill permit issued by the City, and a development agreement, if necessary.
20. That the following warning clause that Council approved on September 29, 1997, with respect to "Tree Fees" shall be included in the subdivision agreement as follows:
 - "Purchasers and/or tenants are advised that the City has not imposed a "Tree Fee", or any other fee which may be charged as a condition of purchase, for the planting of trees. Any "Tree Fee" paid by a purchaser for boulevard trees does not guarantee that a tree will be planted on the boulevard adjacent to their residential dwelling."
 - "Purchasers and/or tenants are advised that the planting of trees in City boulevards in front of residential units is a requirement of the subdivision agreement. A drawing depicting conceptual location for boulevard trees shall be provided by the Owner and shall be included as a schedule in this subdivision agreement. This is only a conceptual plan and while every attempt will be made to plant trees as shown, the City reserves the right to relocate or delete any boulevard tree without further notice."
21. The following warning clause shall be included in all Offers of Purchase and Sale or Lease for the Block on the Plan:
 - "Purchasers and/or tenants are advised that the proposed grading of all lots in conformity with the Subdivision Grading Plans is a requirement to this subdivision agreement."
 - "Purchasers and/or tenants are advised that mail delivery will be from a designated community mailbox, the location of which will be identified by the Owner prior to any home closings."
 - "Purchasers and/or tenants are advised that the private driveway ending in a temporary turn around may be extended in the future to facilitate development of the adjacent lands, without further notice."
22. The following noise warning clauses shall be included in all Offers of Purchase and Sale or Lease for all Lots and Blocks on the Plan:
 - "The dwelling units shall be designed and constructed with a forced air heating system which includes central air conditioning. The air-cooled condenser unit shall have a maximum ARI rating of 7.6 bels, or shall emit noise not exceeding 61 dba at a distance of 4.57 metres or at the nearest point on the closest property line, whichever distance is greater. No building permit shall be issued for a unit on any of the said lots unless the building plans include central air conditioning."
 - "Purchaser/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic, may occasionally interfere with some activities of the dwelling occupants and the outdoor sound level may exceed the Municipality's and the Ministry of the Environment's noise criteria."

- "This dwelling unit has been supplied with an air conditioning system, which will allow windows and exterior doors to remain closed thereby reducing the indoor sound levels to within the Municipality's and the Ministry of the Environment's noise criteria."
 - "This dwelling unit has been fitted with a forced air heating system and the ducting, etc. was sized to accommodate central air conditioning. Installation of central air conditioning will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the Municipality's and the Ministry of the Environment's noise criteria. (Note: The location and installation of the outdoor air conditioning device should be done so as to minimize the noise impacts and comply with criteria of MOE Publication NPC-216, Residential Air Conditioning Devices)."
 - "Purchaser/tenants are advised that due to the proximity of nearby commercial uses, sounds from these uses may at times be audible."
 - "Purchasers and/or tenants are advised that Canadian National or Canadian Pacific railway company(s), or its assigns or successors in interest, is in the vicinity of the subject lands, and there may be future alterations or expansions to the rail facilities or operations which may affect the living environment of the residents in the vicinity, notwithstanding any noise and vibration attenuating measures included in the development and individual dwelling(s); CNR/CPR will not be responsible for any complaints or claims arising from use of such facilities and/or operations on, over or under the aforesaid right-of-way."
23. The Owner shall, if required, agree in the subdivision agreement to erect a permanent 1.8m high acoustic fence barrier or approved equivalent along the limits of the property where required to be provided by the noise report.
24. The following warning clause shall be included in all Offers of Purchase and Sale or Lease:
- "The Owner shall inform the public and all purchasers and tenants that this development will function as a common element condominium and all details and associated costs shall be presented in the sales office, and through marketing material etc."
25. The Owner shall cause the following to be displayed on the interior wall of the sales office, information approved by the City of Vaughan, prior to offering any units for sale, to be monitored periodically by the City, and no building permit shall be issued for a sales office or model home, or a residential unit until such information is approved by the City of Vaughan:
- the Neighbourhood Plan for the broader area, showing surrounding land uses, arterials/highways, railways and hydro lines, etc;
 - the location of street utilities, community mailboxes, entrance features, fencing and noise attenuation features, together with the sidewalk plan approved in conjunction with draft plan approval;
 - the location of parks, open space, stormwater management facilities and trails;
 - the location of institutional uses, including schools, places of worship, community facilities;
 - the location and type of commercial sites;
 - colour-coded residential for townhouses and units permitting business and professional office uses;

- the following notes in BOLD CAPITAL TYPE on the map:

"For further information, on proposed and existing land uses, please call or visit the City of Vaughan Development Planning Department, at 2141 Major Mackenzie Drive, (905) 832-8585."

"For detailed grading information, please call the developer's engineering consultant, (name) at _____".

"This map is based on information available as of (date of map), and may be revised or updated without notification to purchasers."

[In such circumstances, the Owner is responsible for updating the map and forwarding it to the City for verification.]

26. Where the Owner proposes to proceed with the construction of a model home(s) prior to registration of the Plan, the Owner shall enter into an agreement with the City, setting out the conditions, and shall fulfill relevant conditions of that agreement prior to issuance of a building permit.
27. Prior to final approval, the Owner shall provide a tree preservation study to the satisfaction of the City. The study shall include an inventory of all existing trees, assessment of significant trees to be preserved and proposed methods of tree preservation. The Owner shall not remove trees, without written approval by the City.
28. The Owner shall permit any telephone or telecommunications service provider to locate its plan in a common trench within the proposed Plan of Subdivision prior to release of the plan for registration, provided such service provider has executed a Municipal Access Agreement with the City. The Owner shall ensure that any such service provider will be permitted to install its plan so as to permit connection to individual dwelling units within the subdivision as and when each dwelling unit is constructed.

Region of York Conditions

29. The Region shall confirm that adequate water supply capacity and sewage treatment capacity are available and have been allocated by the City of Vaughan for the development proposed within this draft plan of subdivision or any phase thereof.
30. The Owner shall agree in the subdivision agreement, in wording satisfactory to The Regional Municipality of York Transportation and Works Department, to implement the recommendations of the functional transportation report/plan as approved by The Regional Municipality of York Transportation and Works Department.
31. Prior to final approval, the Owner shall provide a set of engineering drawings, approved by the area municipality, which indicates the storm drainage system, the overall grading plans and all proposed accesses onto The Regional Municipality of York roads, for all lands within this plan of subdivision, to The Regional Municipality of York Transportation and Works Department for verification that all The Regional Municipality of York's concerns have been satisfied.
32. The location and design of the construction access for the subdivision work shall be completed to the satisfaction of The Regional Municipality of York Transportation and Works Department and illustrated on the Engineering Drawings.
33. Any existing driveway(s) along The Regional Municipality of York road frontage of this subdivision must be removed as part of the subdivision work, at no cost to The Regional Municipality of York.

34. The Owner shall submit drawings depicting the following to the satisfaction of the Region of York Transportation and Works Department:
- i. all existing woody vegetation within the Regional Road right-of-way;
 - ii. tree protection measures to be implemented on and off the Regional Road right-of-way to protect right-of-way vegetation to be preserved;
 - iii. any woody vegetation within the Regional Road right-of-way that is proposed to be removed or relocated. However, it is to be noted that tree removal within Regional Road right's-of-way shall be avoided to the extent possible/practical. Financial or other compensation may be sought based on the value of trees proposed for removal;
 - iv. a planting plan for all new and relocated vegetation to be planted within the Regional Road right-of-way, based on the following general guideline; and
 - v. tree planting shall be undertaken in accordance with York Region standards as articulated in Streetscaping Policy and using species from the Regional Street Tree Planting List. These documents may be obtained from the Forestry Section. If any landscaping or features other than tree planting (e.g. flower beds, shrubs) are proposed, they will require the approval of the local municipality and be supported by a maintenance agreement between the municipality and York Region for area municipal maintenance of these features. In addition, the agreement should indicate that where the area municipality does not maintain the feature to the Region's satisfaction, the area municipality will be responsible for the cost of maintenance or removal undertaken by the Region.
35. The Owner shall engage the services of a consultant to prepare and submit for review and approval, a noise study to the satisfaction of The Regional Municipality of York Transportation and Works Department recommending noise attenuation features and the Owner shall agree to implement these noise attenuation features to the satisfaction of The Regional Municipality of York's Transportation and Works Department.
36. The Owner shall agree in the subdivision agreement, in wording satisfactory to the Regional Transportation and Works Department, that prior to the release of any security held by York Region in relation to this plan of subdivision, and where berms, noise walls, window and/or oversized forced air mechanical systems are required, these features shall be certified by a professional engineer to have been installed as specified by the approved Noise Study and in conformance with the Ministry of Environment guidelines.
37. The following warning clause shall be included in a registered portion of the subdivision agreement with respect to the lots or blocks affected:
- "Purchasers are advised that despite the inclusion of noise attenuation features within the development area and within the individual building units, noise levels will continue to increase, occasionally interfering with some activities of the building's occupants."
38. Where noise attenuation features will abut a Regional right-of-way, the Owner shall agree in the subdivision agreement, in wording satisfactory to The Regional Municipality of York's Transportation and Works Department, as follows:
- i. that no part of any noise attenuation feature shall be constructed on or within the Regional right-of-way;

- ii. that noise fences adjacent to The Regional Municipality of York roads may be constructed on the private side of the 0.3 metre reserve and may be maximum 2.5 metres in height, subject to the area municipality's concurrence;
 - ii. that maintenance of the noise barriers and fences bordering on The Regional Municipality of York right-of-ways shall not be the responsibility of The Regional Municipality of York; and
 - iii. that any landscaping provided on The Regional Municipality of York right-of-way by the Owner or the area municipality for aesthetic purposes must be approved by The Regional Municipality of York Transportation and Works Department and shall be maintained by the area municipality with the exception of the usual grass maintenance.
39. The following lands shall be conveyed to The Regional Municipality of York for public highway purposes, free of all costs and encumbrances:
 - i. a widening across the full frontage of the site where it abuts Major Mackenzie Drive of sufficient width to provide a minimum of 15.0 metres from the centreline of construction of Major Mackenzie Drive;
 - ii. a 5.0 metre by 5.0 metre daylight triangle at the northeast and northwest corners of Major Mackenzie Drive and Future Access/Jackson Street intersection, for the purposes of driver and pedestrian sightlines; and,
 - iii. a 0.3 metre reserve across the full frontage of the site where it abuts Major Mackenzie Drive and adjacent to the above noted widenings.
40. The Owner shall convey to The Regional Municipality of York, a 0.3 metre reserve along the entire frontage of the site except at the approved access location, adjacent to the above noted widening, free of all costs and encumbrances, to the satisfaction of The Regional Municipality of York Solicitor.
41. The Regional Municipality of York requires the Owner of the site to conduct an environmental audit of the lands by a qualified professional in the field. The audit will contain the requirements of the Phase 1 Environmental Site Assessment, as per applicable Ontario standards, guidelines and regulations. Based on the findings of this assessment, The Regional Municipality of York may require further study to determine any remedial action required to remove contaminants. The Owner shall agree in the subdivision agreement, in wording satisfactory to the Regional Transportation and Works Department, to certify that all lands to be conveyed to The Regional Municipality of York are free of all noxious, deleterious materials on or under the surface. This certification shall be done at no cost to The Regional Municipality of York.

A Phase 1 & 2 Environmental Site Assessment prepared by Winchurch Environmental Ltd, dated, April/ 2002 has been received by this Department.
42. Direct vehicle access from the proposed dwellings to Major Mackenzie Drive will not be permitted. Access must be obtained through the internal road network.
43. Future Access to Major Mackenzie Drive shall be designed to intersect Major Mackenzie Drive at a right angle (90°) and shall be located directly opposite Jackson Street.
44. The throat width of the Future Access to Major Mackenzie Drive shall be designed to accommodate a minimum of one 3.5m metre inbound lane and one 3.5 metre outbound lane for a distance of 15.0 metres from the widened limit of Major Mackenzie Drive.

45. Subject to approval by the City of Vaughan and the Region of York, the Owner shall agree to construct a sidewalk along the subject lands' frontage onto Major Mackenzie Drive, if not already provided, to allow easy pedestrian access to existing and planned transit services.

Existing YRT/Viva transit services operate on the following roadway(s) in the vicinity of the subject lands:

- i. Major Mackenzie Drive
- ii. Keele Street

Concrete pedestrian access shall be provided from within the site to the Regional roadway as follows:

- i. Leading out to the sidewalk on Major Mackenzie Drive, adjacent to the site's driveway access.

The concrete pedestrian access shall be provided at no cost to York Region and, where necessary, concurrent with construction of any necessary sidewalks.

Sidewalks and concrete pedestrian access shall be provided in accordance with OPSD 310.010, 310.020, 310.030 and should be provided "at grade" (i.e. without stairs, inclines, etc.).

46. Prior to site plan execution, the Owner shall submit drawings showing the sidewalk location(s), concrete pedestrian access, passenger standing area(s) and shelter pad(s) for York Region Development Approval Department's review and comments.
47. As part of the subdivision agreement, the Owner shall agree to advise all potential purchasers of the current and possible future introduction of transit services in this development as identified in Condition 45. This includes potential transit routes, bus-stops and shelter locations. This can be achieved through distribution of information/marketing materials (YRT route maps, Future Plan maps and providing YRT website contact information) at sales offices and appropriate notification clauses in purchase agreements. The YRT route maps and the Future Plan maps are available from YRT upon request.
48. The Owner shall make provision for continuous flow-through circulation for YRT's Mobility Plus specialized vehicles within the property. Since the subject site will likely become a destination for residents/visitors with disabilities, servicing provision (i.e. passenger boarding and disembarking) must be considered. Internal driveways and designated pick-up areas should be identified to facilitate movement and circulation of Mobility Plus smaller buses/vehicles. Due to safety concerns, Mobility Plus fleet will not manoeuvre reverse direction.
49. The Owner shall agree in the subdivision agreement to submit plans for the Region's approval that identify the above Transit requirements.
50. Prior to final approval, the Owner shall provide a signed copy of the subdivision agreement to the Regional Transportation and Works Department, outlining all requirements of the Regional Transportation and Works Department.
51. The Owner shall provide a solicitor's certificate of title to the Region's Corporate and Legal Services Department, to the satisfaction of the Regional Solicitor, at no cost to the Region, with respect to the conveyance of lands to The Regional Municipality of York.
52. The Owner shall enter into an agreement with the Region of York, agreeing to satisfy all conditions, financial and otherwise, of the Regional Corporation; Regional Development Charges are payable prior to final approval in accordance with By-laws DC-0005-2003-050 and DC-0005(a)-2005-060.

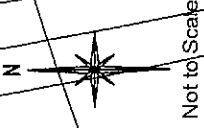
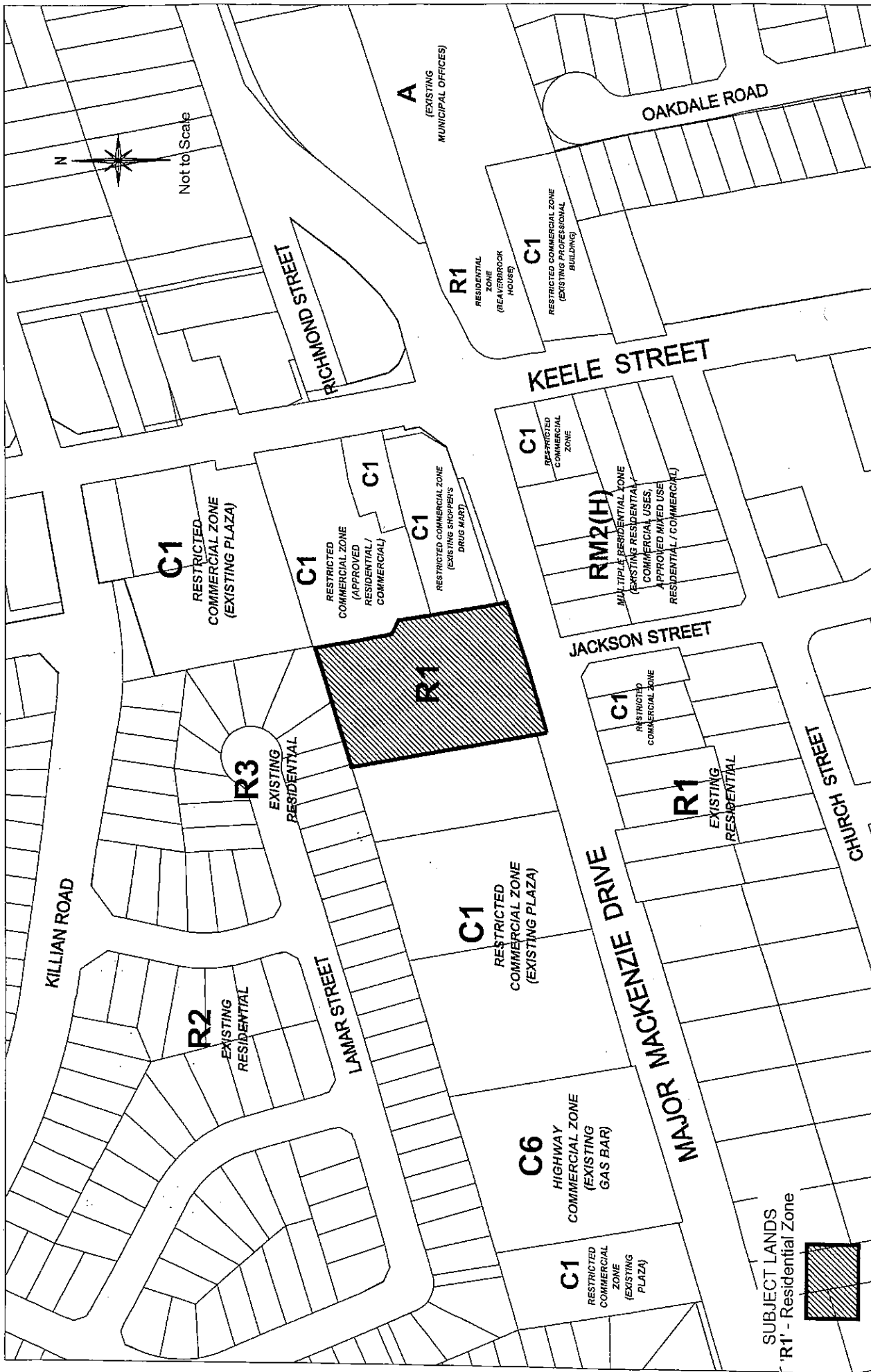
Canada Post Conditions

53. The Owner agrees:

- a) to include in all Offers of Purchase and Sale or Lease, a statement that advises the prospective purchaser or tenant that mail delivery will be from a designated Community Mailbox. Further, the Owner/developer will be responsible for officially notifying the purchasers of the exact Community Mailbox locations prior to the closing of any home sales. Canada Post requests that the Owner/developer consult with Canada Post to determine suitable locations for the placement of Community Mailboxes and to indicate these locations on appropriate servicing plans. The Owner/developer will be responsible for locating temporary Community Mailbox locations, which may be utilized by Canada Post until the permanent mailbox pads, curbs, sidewalks and final grading have been completed at the permanent Community Mailbox site location(s). This will enable Canada Post to provide mail service to new residences as soon as homes are occupied. The Owner/developer further agrees to fit up the temporary area 30 to 60 days prior to the first occupancy and notify Canada Post of the first occupancies at that time;
- b) to provide the following for each Community Mailbox site, as shown on the servicing plan:
 - a sidewalk section (concrete pad), as per municipal and Canada Post standards, to the support the mailboxes;
 - any required walkway across the boulevard, as per municipal standards; and
 - any required curb depressions;
- c) to provide a suitable temporary Community Mailbox location(s) until the curbs, sidewalks and final grading have been completed to the permanent location(s); and
- d) to provide a copy of the executed subdivision agreement to Canada Post.

Other Conditions

54. Final approval for registration may be issued in phases to the satisfaction of the City, subject to all applicable fees provided that:
- a) phasing is proposed in an orderly progression, in consideration of such matters as the timing of road improvements, infrastructure, schools and other essential services; and
 - b) all government agencies agree to registration by phases and provide clearances, as required in Conditions 55 to 57 inclusive, for each phase proposed for registration; furthermore, the required clearances may relate to lands not located within the phase sought to be registered.
55. The City shall advise that Conditions 1 to 28 inclusive, have been satisfied; the clearance letter shall include a brief statement detailing how each condition has been met.
56. The Regional Municipality of York shall advise that Conditions 29 to 52 inclusive, have been satisfied; the clearance letter shall include a brief statement detailing how each condition has been met.
57. Canada Post shall advise that Condition 53 has been satisfied; the clearance letter shall include a brief statement of how this condition has been met.



Attachment 2

FILE No's:
19T-05V07,
Z.04.045 & DA.05.038
November 11, 2005



Development Planning Department

Location Map

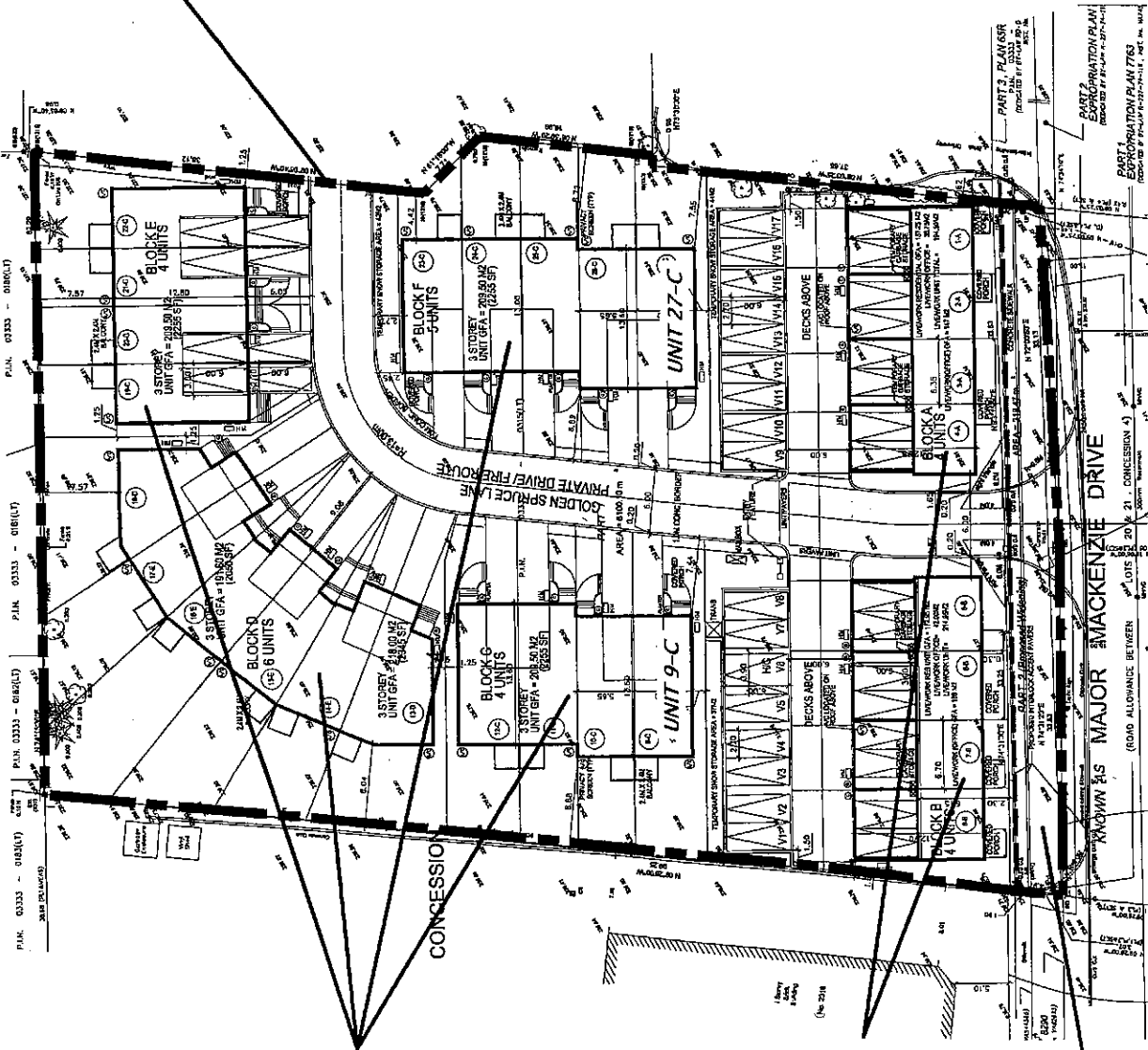
Part Lot 21,
Concession 4
APPLICANT:
2294 MAJOR MACKENZIE LTD.
N:\DPT\1 ATTACHMENTS\19\191-05v07

**REZONE SUBJECT LANDS
FROM 'R1' RESIDENTIAL ZONE
TO 'C1' RESTRICTED
COMMERCIAL ZONE
(Proposed Residential /
Commercial Use)**

**SITE PLAN
2294 MAJOR MACKENZIE DR
MAPLE
2294 MAJOR MACKENZIE LTD
DEVELOPMENTS**

SITE STATISTICS:	0.6418HA (6118.71m ²)
PARCEL AREA =	0.0318HA (318.47m ²)
LESS ROAD WIDENING =	0.6100HA (6100.10m ²)
NET LOT AREA =	0.2251HA (2251m ²) (37%)
BLOS COVERAGE =	0.3389HA (3389m ²) (51%)
BLOS GPA =	
RESIDENTIAL UNITS:	
TYPE A (LIVE/WORK) =	4
TYPE B (LIVE/WORK) =	2
TYPE C =	4
TYPE D =	27
TOTAL UNITS =	37
PARKING:	
PARKING REQ'D AT 1.5 PER UNIT = 41	64 SPACES
VISITOR PARKING REQ'D AT .25 PER UNIT =	9 SPACES
TOTAL PARKING REQ'D = 50	61 PROVIDED
OFFICE SPA 3.15 M ²	10 PROVIDED
OFFICE PARKING =	71 PROVIDED
TOTAL SPACES REQ'D = 58	
LANDSCAPED OPEN SPACE:	
BLOS COVERAGE =	225 m ² (37%)
PRIVATE DRIVE =	989m ²
VISITOR PARKING =	316m ²
UNIT DRIVEWAYS =	426m ²
LANDSCAPED OPEN SPACE = 209m² (34%)	
MAXIMUM BUILDING HEIGHT = 12.58M	
TEMPORARY SNOW STORAGE	
2% LOT AREA = 121 M ² REQ'D	122M ² PROVIDED

SUBJECT LANDS



Not to Scale

**PROPOSED
TOWNHOUSE
UNITS**

**PROPOSED
LIVENETWORK UNITS -
GROUND FLOOR
COMMERCIAL WITH
RESIDENTIAL
UNITS ABOVE**

ROAD WIDENING

Attachment 3

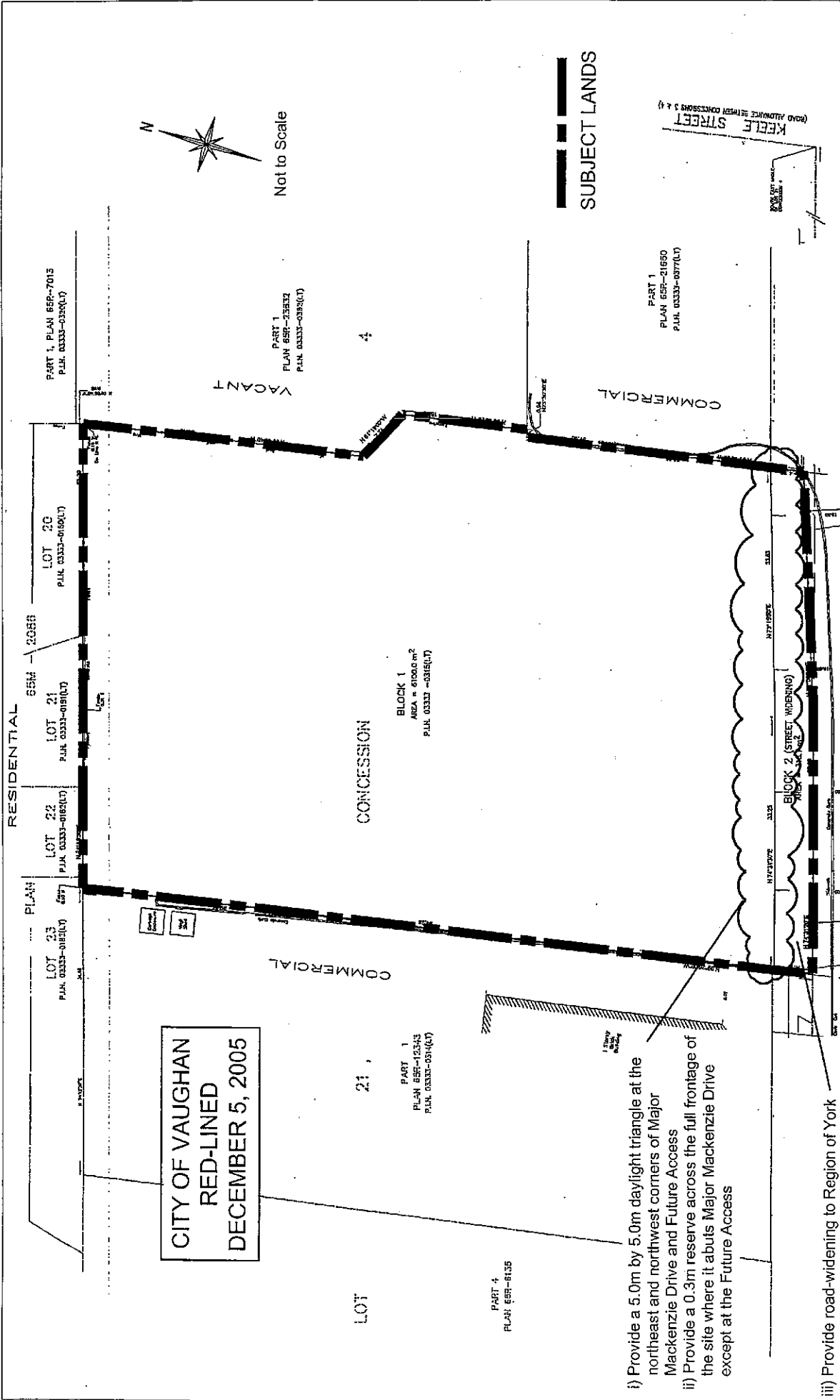
FILE No.: 19T-05V07
RELATED FILES: Z.04.045 & DA.05.038
November 23, 2005



Development Planning Department

Proposed Site Plan

Part Lot 21,
Concession 4
APPLICANT:
2294 MAJOR MACKENZIE LTD.
N:\DPT\1 ATTACHMENTS\19\19T-05\07



CITY OF VAUGHAN
 RED-LINED
 DECEMBER 5, 2005

- i) Provide a 5.0m by 5.0m daylight triangle at the northeast and northwest corners of Major Mackenzie Drive and Future Access
- ii) Provide a 0.3m reserve across the full frontage of the site where it abuts Major Mackenzie Drive except at the Future Access

iii) Provide road-widening to Region of York

Draft Plan of Subdivision

Part Lot 21,
 Concession 4

APPLICANT:
 2294 MAJOR MACKENZIE LTD.

NO. OF T.V. ATTACHMENTS \18\181-05907



Development Planning Department

Attachment 4

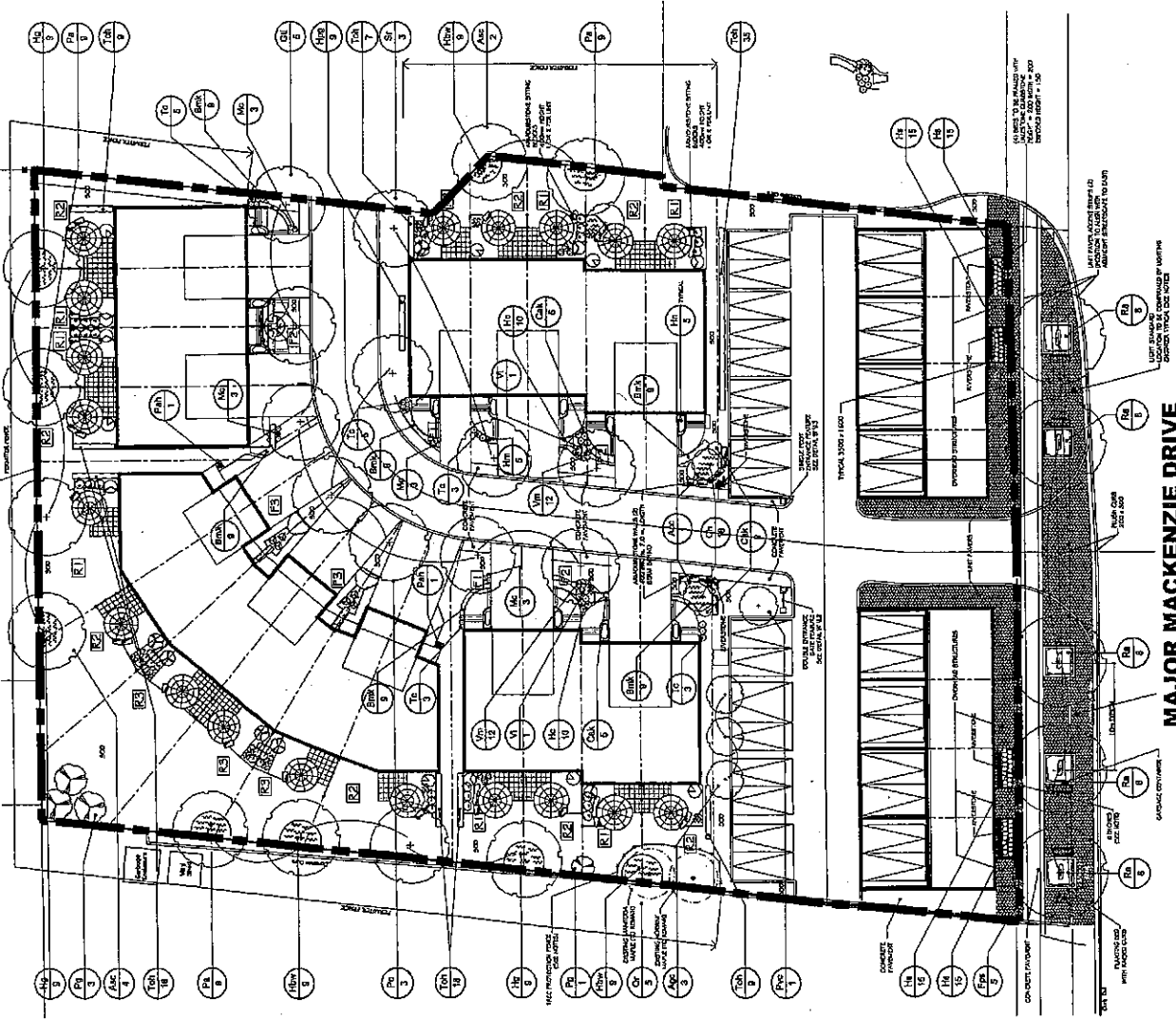
FILE No's:
 19T-05V07,
 Z.04.045 & DA.05.038

November 21, 2005



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SUBJECT LANDS



Attachment 5

FILE No's.:
19T-05V07,
Z.04.045 & DA.05.038

November 23, 2005



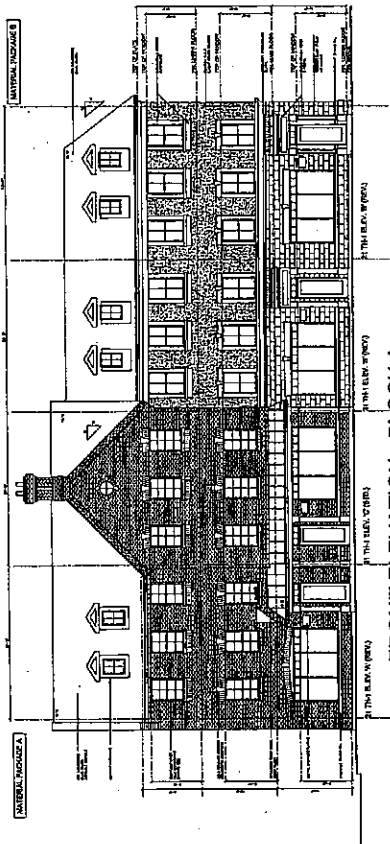
Development Planning Department

Landscape Plan

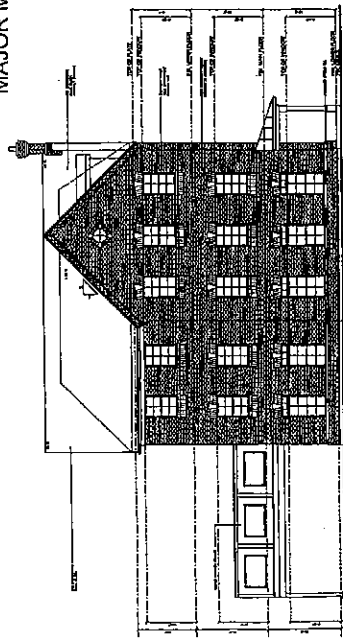
Part Lot 21,
Concession 4

APPLICANT:
2294 MAJOR MACKENZIE LTD.

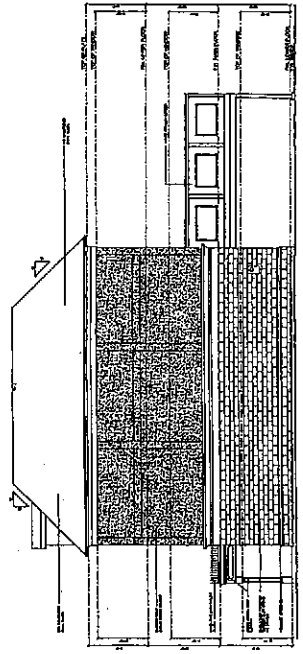
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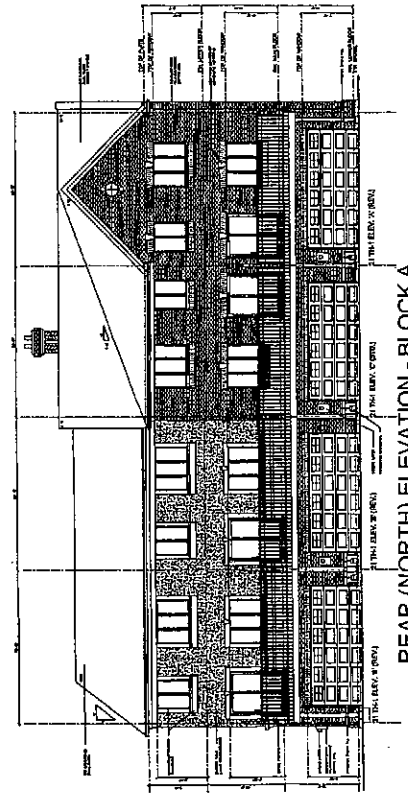
FRONT ELEVATION - BLOCK A
MAJOR MACKENZIE DRIVE



LEFT SIDE (WEST) ELEVATION - BLOCK A



RIGHT SIDE (EAST) ELEVATION - BLOCK A



REAR (NORTH) ELEVATION - BLOCK A

Not to Scale

Elevations - Block A

Part Lot 21,
Concession 4
APPLICANT:
2294 MAJOR MACKENZIE LTD.

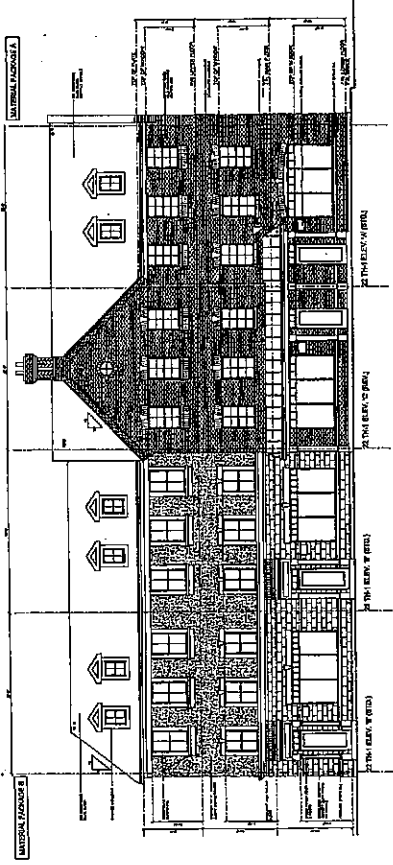
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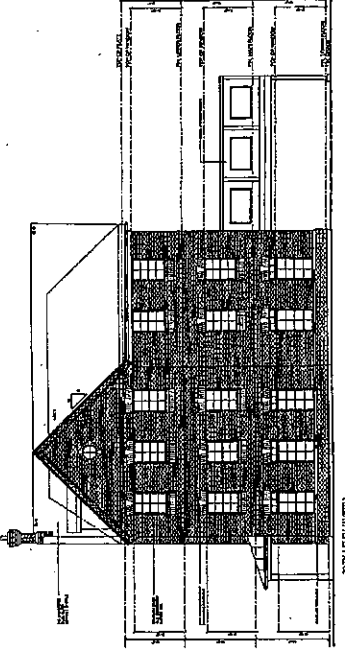
Development Planning Department

Attachment 6

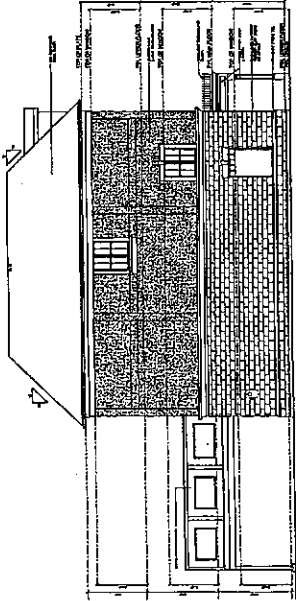
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Z.04.045 & DA.05.038
November 23, 2005



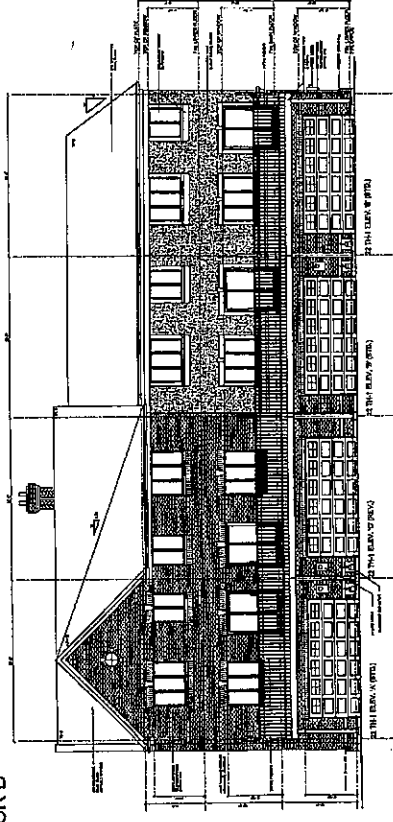
FRONT ELEVATION - BLOCK B
MAJOR MACKENZIE DRIVE



RIGHT SIDE (EAST) ELEVATION - BLOCK B



LEFT SIDE (WEST) ELEVATION - BLOCK B



REAR (NORTH) ELEVATION - BLOCK B

Not to Scale

Attachment 7

FILE No's:
19T-05V07,
Z.04.045 & DA.05.038

November 23, 2005

City of Vaughan

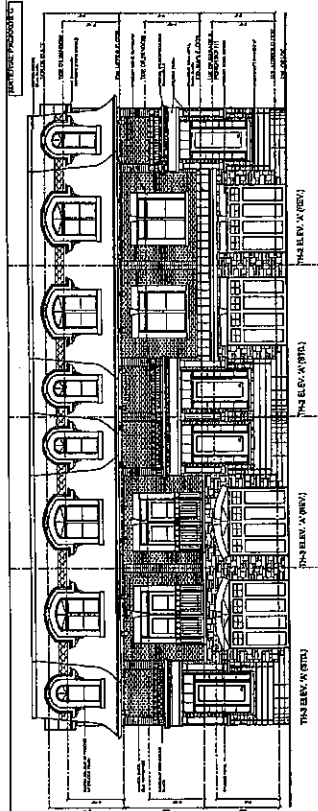
Development Planning Department

Elevations - Block B

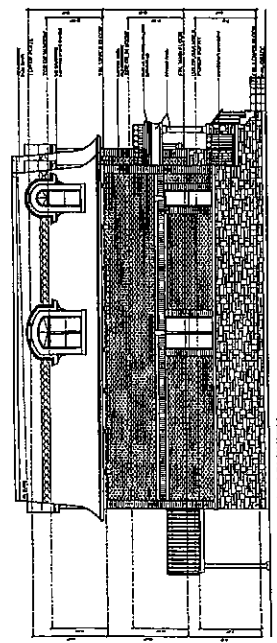
Part Lot 21,
Concession 4

APPLICANT:
2294 MAJOR MACKENZIE LTD.

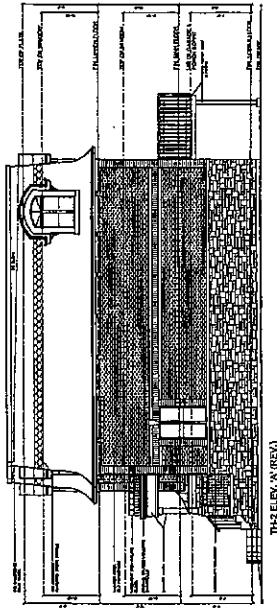
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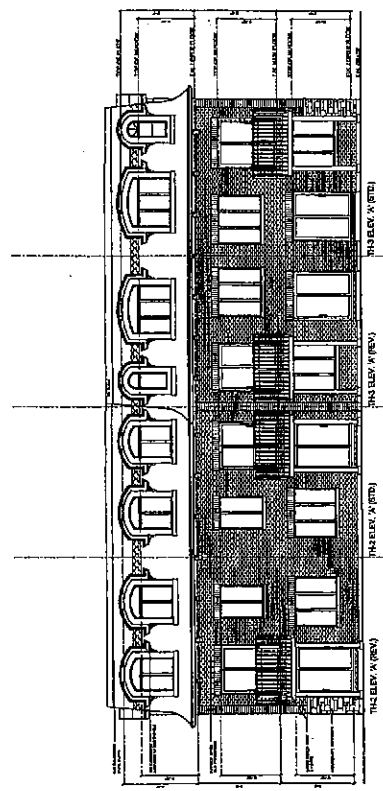
FRONT (EAST) ELEVATION - BLOCK C



LEFT SIDE (SOUTH) ELEVATION - BLOCK C



RIGHT SIDE (NORTH) ELEVATION - BLOCK C



REAR (WEST) ELEVATION - BLOCK C

Not to Scale

Elevations - Block C

Part Lot 21,
Concession 4
APPLICANT:
2294 MAJOR MACKENZIE LTD.

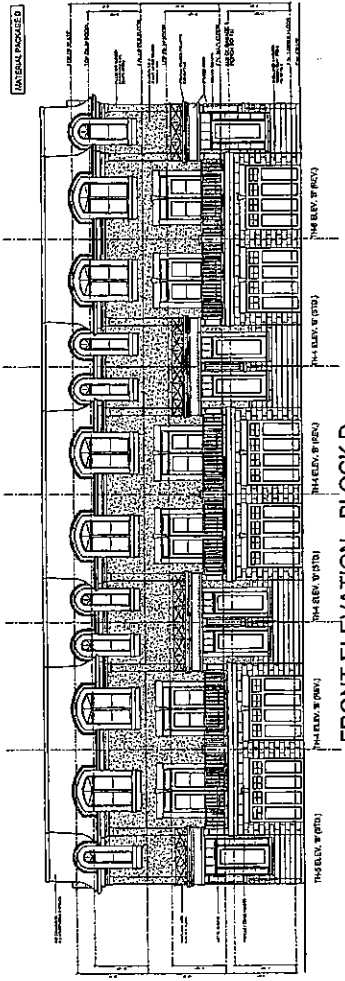


Development Planning Department

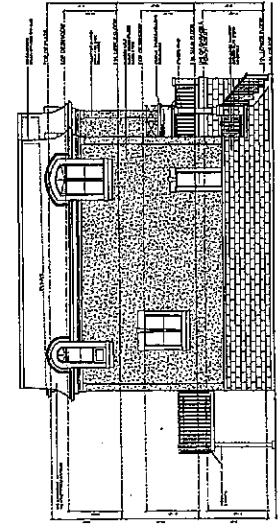
Attachment 8

FILE No's.:
19T-05V07,
Z.04.045 & DA.05.038

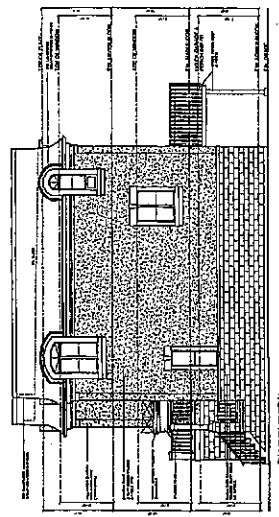
November 23, 2005



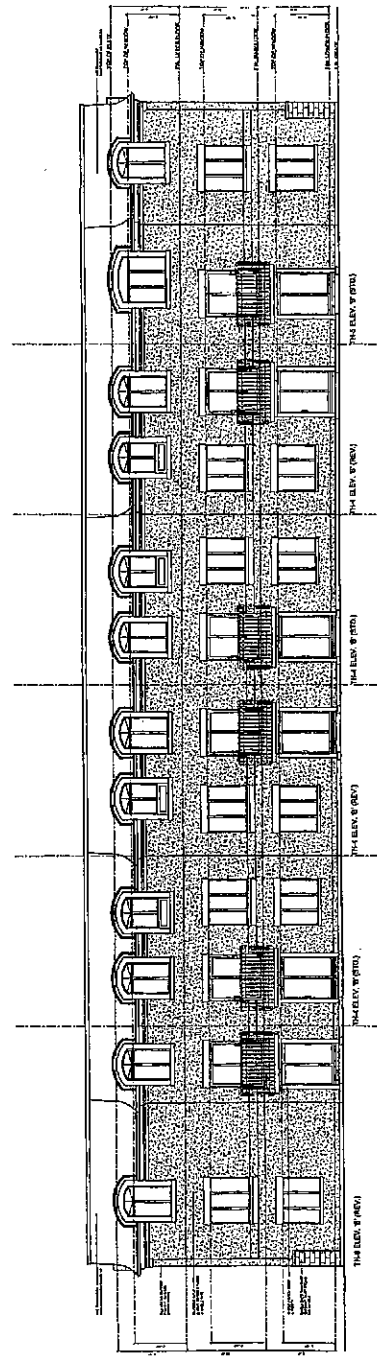
FRONT ELEVATION - BLOCK D



LEFT SIDE (SOUTH) ELEVATION - BLOCK D



RIGHT SIDE (EAST) ELEVATION - BLOCK D



REAR ELEVATION - BLOCK D

Not to Scale

Elevations - Block D

Part Lot 21,
Concession 4

APPLICANT:
2294 MAJOR MACKENZIE L.T.D.

NA\DFY\1 ATTACHMENTS\19\191-05\07

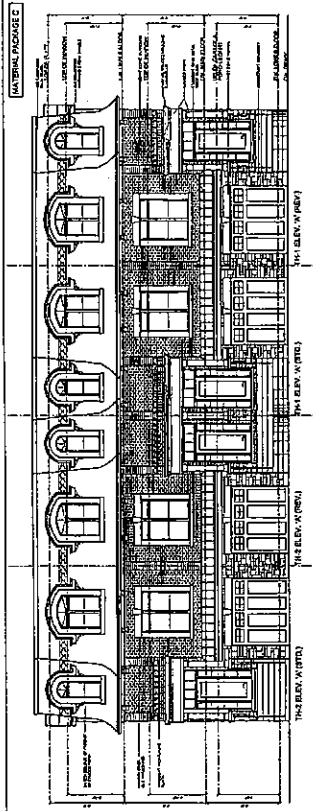


Development Planning Department

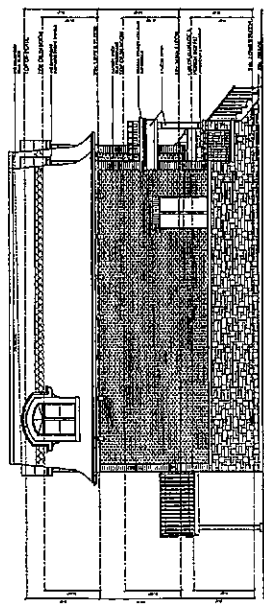
Attachment 9

FILE No's.:
19T-05V07,
Z.04.045 & DA.05.038

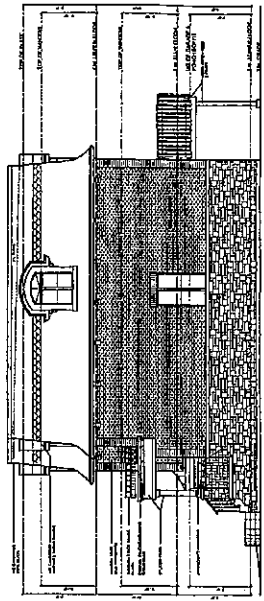
November 23, 2005



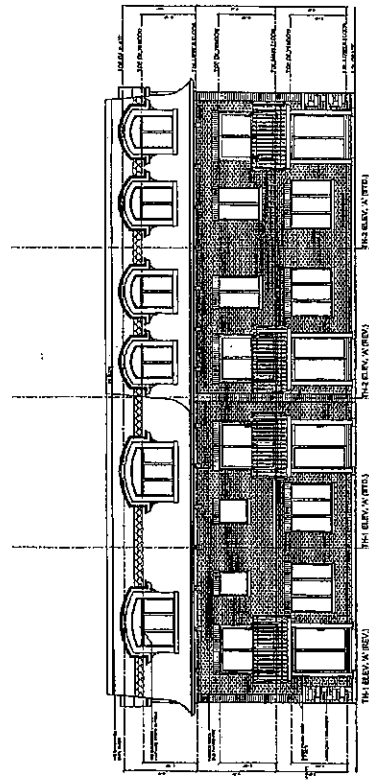
FRONT ELEVATION - BLOCK E
2294 MAJOR MACKENZIE DRIVE



LEFT SIDE (WEST) ELEVATION - BLOCK E



RIGHT SIDE (EAST) ELEVATION - BLOCK E



REAR (NORTH) ELEVATION - BLOCK E

Not to Scale

Elevations - Block E

Part Lot 21,
Concession 4
APPLICANT:
2294 MAJOR MACKENZIE LTD.

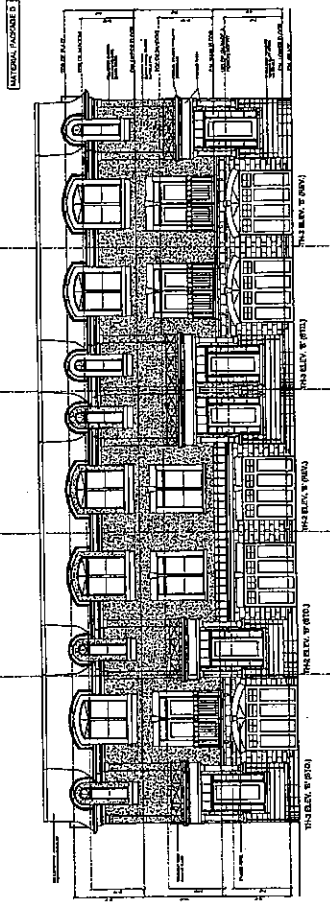
N:\DEVL ATTACHMENTS\191-05-07



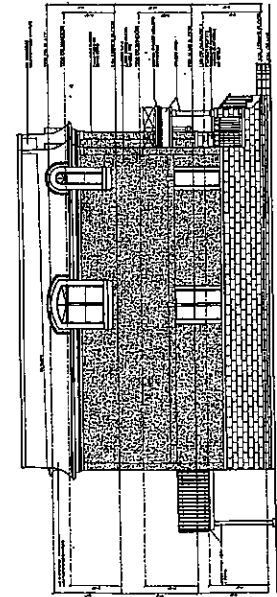
Development Planning Department

Attachment 10

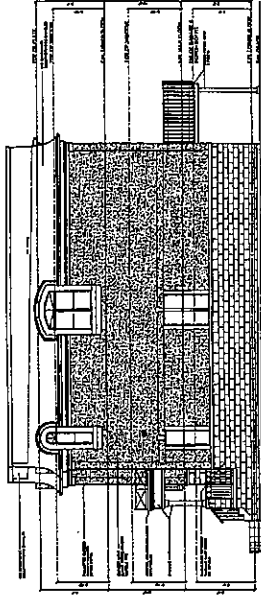
FILE No's:
19T-05V07,
Z.04.045 &
DA.05.038
November 23, 2005



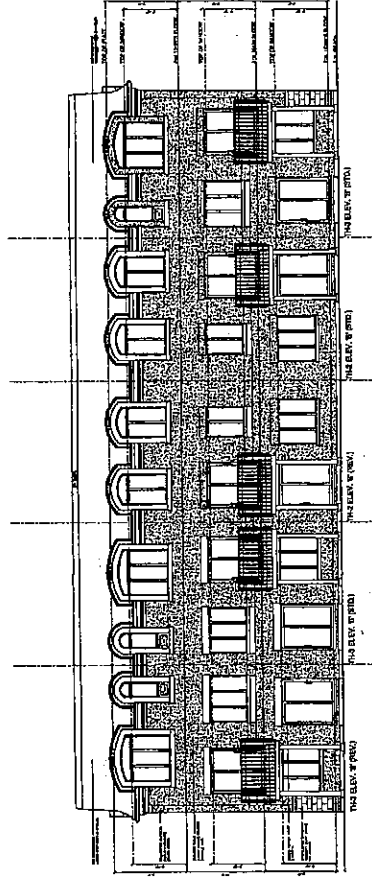
FRONT (WEST) ELEVATION - BLOCK F



LEFT SIDE (NORTH) ELEVATION - BLOCK F



RIGHT SIDE (SOUTH) ELEVATION - BLOCK F



REAR (EAST) ELEVATION - BLOCK F

Not to Scale

Elevations - Block F

Part Lot 21,
Concession 4

APPLICANT:
2294 MAJOR MACKENZIE LTD.

HA/PTA/ ATTACHMENTS \ 19 \ 19-05-07

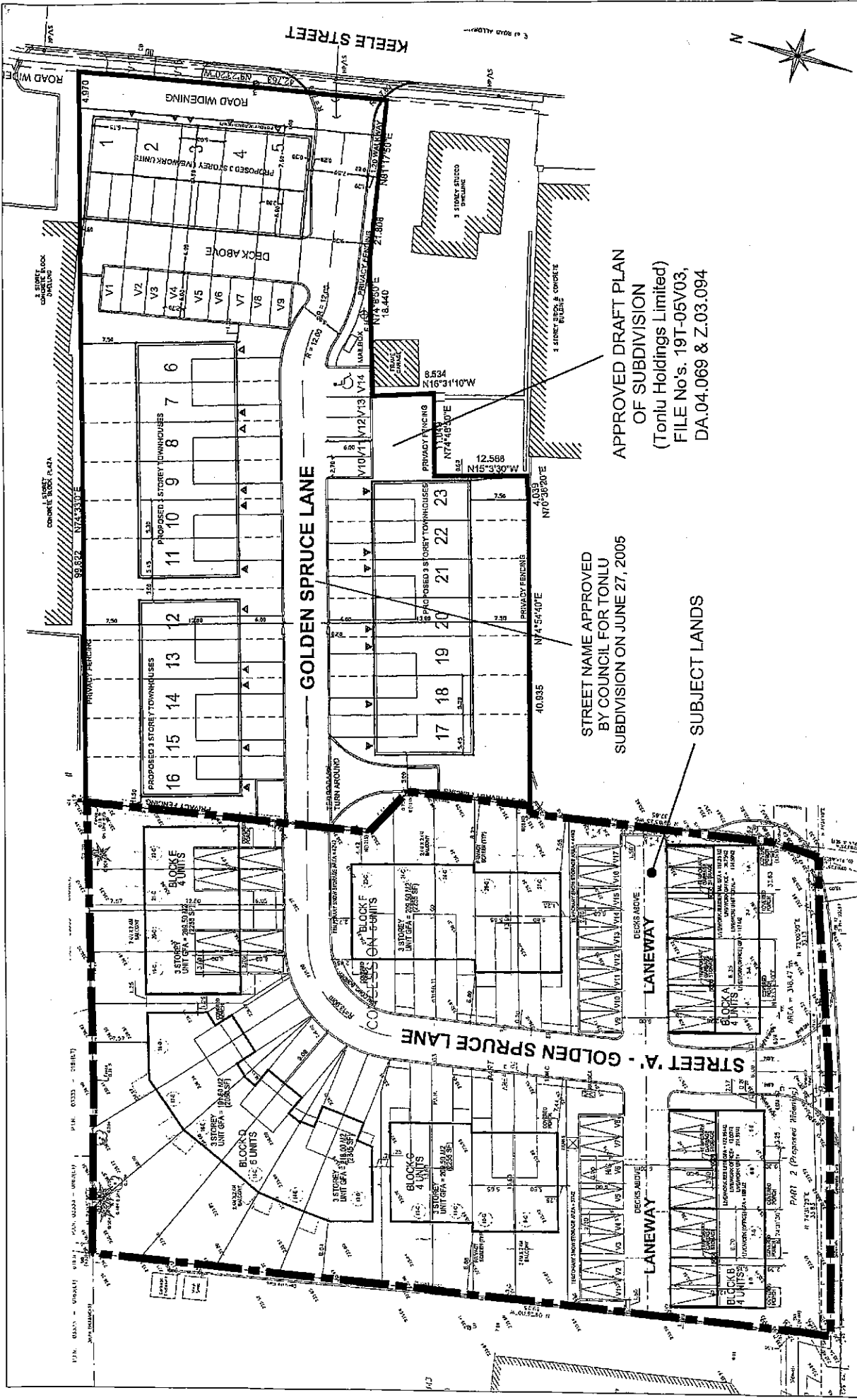


Development Planning Department

Attachment 11

FILE No's.:
19T-05V07,
Z.04.045 &
DA.05.038

November 23, 2005



MAJOR MACKENZIE DRIVE

STREET NAME APPROVED BY COUNCIL FOR TONLU SUBDIVISION ON JUNE 27, 2005

APPROVED DRAFT PLAN OF SUBDIVISION (Tonlu Holdings Limited) FILE No's. 19T-05V03, DA.04.069 & Z.03.094

SUBJECT LANDS

Not to Scale

Comprehensive Design Scheme

APPLICANT:
2294 MAJOR MACKENZIE LTD.

Development Planning Department



Attachment 12

FILE No's:
19T-05V07,
Z.04.045 &
DA.05.038

November 15, 2005