

## **SPECIAL COMMITTEE OF THE WHOLE - AUGUST 31, 2010**

### **OFFICIAL PLAN REVIEW - VOLUME 2 LANDS SUBJECT TO EXISTING SECONDARY PLAN POLICIES AND SITE AND AREA SPECIFIC POLICIES RESPONSE TO PUBLIC, GOVERNMENT AND AGENCY SUBMISSIONS FILE: 25.1.1(b) WARDS 1 TO 5**

#### **Recommendation**

The Commissioner of Planning recommends:

1. That Attachment No. 1, forming the existing secondary plans and area and site-specific amendments of Volume 2 (August 2010), together with the five new secondary plans (North Kleinburg Nashville Secondary Plan, Woodbridge Centre Secondary Plan, Vaughan Metropolitan Centre Secondary Plan, Yonge Steeles Corridor Secondary Plan and the West Vaughan Employment Area Secondary Plan) be consolidated into Volume 2 of the Official Plan; and that Volume 2 proceed to Council for adoption at its September 7, 2010 meeting.

#### **Contribution to Sustainability**

The Provincial Policy Statement and the Growth Plan for the Greater Golden Horseshoe, as well as the draft Region of York Official Plan (December 2009), have established a vision and policy framework that provides for more efficient growth management. The new policy directions included within the City's Official Plan will also apply to Volume 2. Policies included relate to the creation of complete communities including sustainability, natural and built heritage protection, green and intensified built form, and transportation demand measures.

#### **Economic Impact**

The new Official Plan, which includes Volume 2, establishes the planning framework for development throughout Vaughan to 2031. The Official Plan will have a positive impact upon the City of Vaughan in terms of managing growth and fostering employment opportunities while fulfilling the City's obligations to conform to Provincial policies and meet Regionally imposed targets for residential and employment growth.

#### **Communications Plan**

The project status, analysis and results of the new Vaughan Official Plan review were communicated to the public through an extensive public engagement and consultation process. Likewise, each of the Secondary Plans emanating from the focus area reviews (i.e. Vaughan Metropolitan Centre, Yonge Steeles Corridor, West Vaughan Employment Area, North Kleinburg-Nashville and Woodbridge Centre) and which will ultimately form part of Official Plan-Volume 2, has been subject to an extensive consultation process, including discussions with stakeholders. Reports for each secondary plan, which outline their respective consultation process, are before Council at this Special Committee of the Whole meeting (August 31, 2010).

With respect to the existing site and area-specific policies that also form part of Volume 2, a Notice of the Public Hearing was published in the May 19, 2010 edition of the Vaughan Citizen and posted on the City's website through the City Page and on the Policy Planning Public Hearing page. A notice was also mailed to all individuals requesting notification of the Public Hearing. The site and area-specific policies of each secondary plan, which are approved Official

Plan Amendments being consolidated into Volume 2, were previously subject to the planning approval process.

### **Purpose**

The purpose of this report is to present that portion of Volume 2 containing the lands subject to existing secondary plan and area and site-specific amendments.

The Official Plan will be produced in two volumes: Volume 1 will include the City-wide policies. Volume 2 will include a consolidation of five new secondary plans resulting from the focused area reviews; (the North Kleinburg-Nashville Secondary Plan, the Woodbridge Centre Secondary Plan, the Yonge Steeles Corridor Secondary Plan, the Vaughan Metropolitan Centre Secondary Plan and the West Vaughan Employment Area Secondary Plan) and approved secondary plans and area and site-specific amendments being carried forward with the new Official Plan. Specific comments relating to each Secondary Plan are addressed in five separate reports being considered at the August 31, 2010 Special Committee of the Whole meeting. This report deals only with already approved secondary plans and area and site-specific policies which are being carried forward with the new Official Plan. The policies being carried through to Volume 2 are those that derive from:

- recent OPAs which are consistent with the new Official Plan (e.g. Carville Centre Secondary Plan)
- specific land use issues (e.g. Heritage Districts, Keele Valley Landfill)
- recent Ontario Municipal Board decisions (e.g. Kipling Avenue and Highway #7)

Specifically, the existing land use documents to be maintained in Volume 2 are as follows:

#### i) Secondary Plans

- Carrville Centre Secondary Plan (OPA #651)
- Steeles West Secondary Plan (OPA #620)
- Highway 400 Employment Lands (OPA #637)
- Kipling Avenue Secondary Plan (OPA #695)

#### ii) Area Specific Amendments and Policies

- Heritage Conservation Districts (Kleinburg-Nashville, Woodbridge, Maple and Thornhill)
- Keele Valley Landfill Area (OPA #332 as amended by OPA #535)
- Kleinburg Core (OPA #633 and #703)
- Adult Entertainment Uses (OPA #265, #394 and #565)
- Vaughan Mills Mall (OPA #505 and OPA #510)
- Northeast Quadrant of Vellore Centre (OPA #713)
- Block 61 West "Nashville Heights" (OPA #699)
- Yonge Street Corridor in Thornhill (OPA #669 and OPA #700)
- Thornhill Town Centre (OPA #671)
- Centre Street Corridor (OPA #672)

#### iii) Site-Specific Plan Policies

- Bathurst and Teston (South West Corner) (OPA #638)
- 1125 Highway 50 (OPA #606)
- 7242 Highway 27 (OPA #621)

- Thornhill Liberty (OPA #584)
- Northwest Corner of Bathurst Street and Rutherford Road (OPA #689)
- Southwest Corner of Kipling Avenue and Highway #7 (OPA #701)
- Vaughan Healthcare Campus (OPA #715)

## **Background - Analysis and Options**

### Location

The lands subject to the existing secondary plans and site specific area policies are shown on Attachment #1 (City of Vaughan Official Plan Volume 2: A Plan for Transformation – August 2010 Draft).

### New Official Plan

Each existing secondary plan and the site or area-specific policies that will form part of Volume 2, will be consolidated into the new Official Plan with no change to its purpose and effect. Section 10.1.4.6 of the new Official Plan enables this administrative process. Any adjustment to policies made for the purpose of adaptation into the Official Plan, will not affect the intent or substance of the document, and will be required to effect the following:

- deletion of obsolete provisions;
- changes or corrections to format, wording, mapping, reference errors; or,
- alteration in the number and arrangement of any provisions.

The documents that form Volume 2 (the secondary plans and site specific and area policies) are also being revised to ensure that the nomenclature is consistent with the Official Plan (Volume 1) as it relates to matters such as land use designations or road classifications.

Policies from the new Official Plan (Volume 1) will apply to the Amendments that form Volume 2, as discussed below:

#### i) Policies

The draft Official Plan (Volume 1) contains current planning initiatives (e.g. sustainability and natural heritage policies) that conform to recent Provincial and Regional land use policy directions and are intended to apply to all lands within the City. The secondary plans and site and area-specific amendments that form Volume 2 are intended to be read and applied together with Volume 1. An exception is that if both Volumes 1 and 2 include a policy relating to the same issue and they conflict, the Volume 2 policy will prevail. However, if there is a policy in Volume 1 relating to an issue that is not included in Volume 2, then the policy in Volume 1 will apply to the lands subject to Volume 2. Section 10.2.1.5 of the Official Plan (Volume 1) sets out the intended implementation of Volumes 1 and 2.

#### ii) Mapping

Some inconsistencies occur between the Land Use Schedules in the draft Official Plan (Volume 1) and Volume 2. Upon review of the comments received, the Land Use and other schedules will be revised to ensure consistency prior to adoption of Volumes 1 and 2 by Council.

### Zoning

The provisions of Zoning By-law 1-88 will remain in effect until they are updated or replaced by zoning consistent with the new Official Plan, including Volume 2. With approval of the Official

Plan, it is anticipated that the preparation of a new by-law will be commenced to bring the City's zoning provisions into conformity with the new Official Plan. A budget and work schedule to include the zoning review in the 2012 budget, will be prepared for consideration by Council next year.

### Council Direction

The statutory public hearing for the draft Secondary Plan was held on June 14, 2010. The following recommendation of the Committee of the Whole was ratified by Council, as amended (in part), on June 29, 2010:

#### **“Recommendation**

The Commissioner of Planning recommends:

THAT the Public Hearing report for File 25.1.1(b) (City of Vaughan Official Plan – Volume 2) BE RECEIVED; and, that any issues raised at the public meeting and comments submitted in writing be addressed by the Policy Planning Department in a comprehensive report to the Committee of the Whole.”

The five draft Secondary Plans that form part of Volume 2, were made available for public review on May 25, 2010, and comments have been received prior to, and at the public hearing on June 14, 2010. Submissions have continued to be received beyond the formal commenting period and staff has endeavoured to respond to those received until mid-August. Any later submissions may be addressed through York Region's approval process.

### Submissions Received

The majority of submissions received on the various components of Volume 2 were concerned with how the policies and land use designations in the proposed Secondary Plans affect specific properties, and others with general policy issues. These comments have been reviewed and are contained in the individual reports being considered for each new Secondary Plan at the August 31, 2010 Special Committee of the Whole meeting. The comments received with respect to the existing secondary plans and area and site-specific amendments being carried in Volume 2 relate primarily to two issues: the application of Volume 1 policies to the approved amendments; and, maintaining existing development approvals in Volume 2. These issues are discussed above in this report.

### Transition Period

In the period between the adoption and final approval of the Official Plan and Volume 2, the City will be operating with the existing official plan policies still in full force. It is expected that applications to amend the existing Official Plan and Zoning By-law 1-88 will continue to be submitted during this period. In evaluating these applications, conformity to the new Official Plan will be taken into consideration. The objective is to bring any such amendments into conformity with the intent of the new Official Plan and Secondary Plans. While not in force, the Official Plan and Secondary Plans represent the City's most contemporary expression of planning principles, developed over several years through extensive study and public consultation. This approach will help to ensure that the integrity of the new plan is maintained during the transition period.

Direction to finalize the new Official Plan for adoption on September 7, 2010 was received at the July 28, 2010 Special Committee of the Whole meeting. As well, a resolution specifying that all applications for official plan and zoning by-law amendments, received between the adoption and final approval of the Plan by the Region of York, will be evaluated on the basis of both the existing

and new Official Plan policies. This will help to ensure that the integrity of the new plan is maintained during the transition period.

### **Relationship to Vaughan Vision 2020/Strategic Plan**

Volume 2 pertains to the section of Vaughan Vision 2020 dealing with “Plan and Manage Growth and Economic Vitality”, including the following specific components:

- Complete and implement the Growth Management Strategy (“Vaughan Tomorrow”)
- Conduct the 5-year comprehensive review of the Official Plan as part of the Growth Management Strategy 2031
- Support and coordinate land use planning for high capacity transit at strategic locations in the City of Vaughan

This report is consistent with the priorities previously set by Council and the necessary resources have been allocated and approved.

### **Regional Implications**

The existing secondary plans and area and site-specific amendments in Volume 2 have already been approved through the appropriate planning process. Accordingly, no Regional implications are expected.

### **Next Steps**

The existing secondary plans and area and site specific amendments that are the subject of this report will form part of Volume 2 of the Official Plan. It is anticipated that the Official Plan (including Volume 2) will be considered by Council on September 7, 2010 and subsequently forwarded to the Region of York for final approval.

### **Conclusion**

This report identifies existing approved secondary plans and area and site-specific amendments that will be consolidated into Volume 2 of the Official Plan with no change to its purpose or effect. Attachment No. 1 to the report identifies the location and form of each amendment. Any adjustments to the policies made for the purpose of adaptation into the new Official Plan will not affect the substance of the document and is required to effect the following: deletion of obsolete provisions; changes, errors or corrections to the wording, mapping or format; alteration in numbers and arrangement of any provisions; and, nomenclature.

The City's new Official Plan will be produced in 2 Volumes. Volume 1 will contain the policies that generally apply to the City. Volume 2 will comprise the existing secondary plans and area and site-specific amendments (Attachment No. 1) and five new secondary plans resulting from the focused area reviews (North Kleinburg Nashville, Woodbridge Centre, Vaughan Metropolitan Centre, Yonge Steeles Corridor and the West Vaughan Employment Area Secondary Plans. The new Official Plan (Volumes 1 and 2) are anticipated to be considered by Council on September 7, 2010.

Therefore, it is recommended that Attachment No. 1, forming the existing secondary plans and area and site-specific amendments of Volume 2 (August 2010), together with the five new secondary plans be consolidated into Volume 2 of the Official Plan; and that Volume 2 proceed to Council for adoption at its September 7, 2010 meeting.

**Attachments**

1. City of Vaughan Official Plan Volume 2 - A Plan for Transformation, August 2010 Draft

**Report prepared by:**

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Respectfully Submitted,

John Zipay  
Commissioner of Planning

Diana Birchall  
Director of Policy Planning

/lm



# City of Vaughan Official Plan

Volume 2



## A Plan for Transformation

August 2010  
DRAFT

Vaughan Tomorrow

OUR CITY. OUR PLAN.



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# 11 SECONDARY PLAN POLICIES



## 11.1 Areas Subject to Secondary Plans

### 11.1.1 Areas Subject to Secondary Plans

It is the policy of Council that:

- 11.1.1.1. The lands subject to the Carrville Centre Secondary Plan are identified on Schedule 14-A and subject to policies set out in Section 11.2 of this Plan.
- 11.1.1.2. The lands subject to the Steeles West Secondary Plan are identified on Schedule 14-A and subject to policies set out in Section 11.3 of this Plan.
- 11.1.1.3. The lands subject to the Highway 400 North Employment Lands Secondary Plan are identified on Schedule 14-A and subject to policies set out in Section 11.4 of this Plan.
- 11.1.1.4. The lands subject to the Kipling Avenue Secondary Plan are identified on Schedule 14-A and subject to policies set out in Section 11.5 of this Plan.



## 11.2 Carrville Centre Secondary Plan

### 11.2.1 General Intent

The lands within the Carrville Centre shall become the focus for higher order land uses within the community of Carrville. The Centre shall evolve to be an urban centre, with a physical form that is compact, human in scale, and designed to be ‘pedestrian-friendly’ and transit-supportive. The Carrville Centre shall provide opportunities for community scale commercial facilities, along with more intense residential forms of development, and encourage mixed-use development. A strong ‘live/work’ relationship is a key objective, wherein residents in the Carrville Centre and surrounding community have opportunities to work and shop within the Centre, rather than commuting to jobs or services elsewhere.

The lands within the Carrville Centre encompass approximately 57.0 hectares. The Centre has been planned at the intersection of two Regional Arterial Roads, Rutherford Road and Dufferin Street. In keeping with the Regional Official Plan, the achievement of the vision of this Secondary Plan requires a modification to the image and function of these Regional roads within the Centre, from that of primarily high-speed vehicle routes to multi-purpose urban streets that are corridors for traffic, transit and pedestrians.

This Secondary Plan establishes appropriate planning policy for the Carrville Centre. This Secondary Plan recognizes that the Centre will have some initial phases of development that will begin to capture the ultimate vision for the Centre in terms of scale and mix of uses. All development should support the long-term goals and built-form objectives envisioned by this Secondary Plan; and, it is critical that a pedestrian-oriented Main Street and Urban Square be developed at the outset and through incorporation into the initial stages of construction.

To maintain long-term flexibility and ensure that this Secondary Plan can be implemented as envisioned, establishment of the road and block pattern in the first phases of development is crucial. Appropriate provision for the planned road network shall be a requirement of any approved development. As such, Maps 11.2.A and 11.2.B the development blocks and, subsequently, the comprehensive street pattern. The City may consider variations to this pattern subject to any specific development proposal’s ability to satisfy the design objectives and policies of this Secondary Plan.

Development throughout the Carrville Centre will be characterized by high quality urban design. The visual attractiveness and image of the Centre is of prime importance. Development related and scaled to the pedestrian is required on all lands within the Centre. This Secondary Plan is based on a Demonstration Plan (Appendix A) providing for development solutions that are comprehensive and integrate key planning and urban design considerations and requirements. Urban design requirements will be implemented through the policies of this Plan, the zoning by-law and through site plan control. A Community Improvement Plan may also be prepared to focus on the details of design for the public realm components of this Plan, as well as to establish a comprehensive financial incentives program that will assist private developers to achieve all of the City's objectives for the Carrville Centre.

## **11.2.2 Community Structure and Urban Form**

- 11.2.2.1. The Carrville Centre is intended to be the focus of the highest intensity land use within the Carrville community. Carrville Centre is generally located at the intersection of Rutherford Road and Dufferin Street, in order to be accessible to pedestrians, private vehicles and public transit services from the neighbourhoods in the surrounding Carrville community. However, given that the four quadrants are not equal in size and have dramatically different environmental and existing development contexts, the northwest quadrant will be the primary focus of the Carrville Centre.

## **11.2.3 Population and Employment**

- 11.2.3.1. The Carrville Centre is planned for, and shall be developed based on, a proposed development of approximately 5,400 residential units, in anticipation of a future population of approximately 11,000 to 12,000 residents. It is an objective of this Plan that up to 4,000 persons be employed in the Centre when it is fully developed.

## **11.2.4 Development Principles And Objectives**

Development of the Carrville Centre will implement the City's planned urban structure by establishing the Centre as the focal point of residential, commercial and business activity within the Carrville community, and by integrating the Centre with the surrounding Carrville community, contributing to its health and vitality.

The following principles express the fundamental premises for development within the Carrville Centre:

### **Compact Urban Form**

- 11.2.4.1. Development shall reflect a compact urban form, with a mix of residential, commercial, institutional, parks and open space, environmental and employment uses, concentrated and arranged in a manner which encourages travel on foot and by other modal alternatives to the car, and which fosters and facilitates public transit ridership. The objectives associated with this principle are:
- a. To provide a focus and sense of identification for the residential communities located primarily in the east part of the Vaughan urban area;
  - b. To facilitate the development and redevelopment of the Carrville Centre in a comprehensive manner;
  - c. To promote the evolution and growth of the Carrville Centre as a focal point of development according to Vaughan's existing and planned urban structure;
  - d. To provide opportunities for multi-use institutional and community facilities;
  - e. To promote and encourage convenient pedestrian accessibility throughout the Centre, and to parks and public facilities in particular; and,
  - f. To develop a pedestrian and bicycle network throughout the Centre, connecting parks, public buildings, stormwater management areas, utility and storm sewer easements and, where appropriate, local streets, within and adjacent to the Centre.

### **Urban Design**

- 11.2.4.2. The Centre will be developed in a manner that reflects the urban design policies of this Plan as they apply to the public realm and private lands, in order to create an attractive, safe and pedestrian-friendly environment. The objectives associated with this principle are:
- a. To promote high quality design within the area including streetscapes, open spaces, public buildings and infrastructure, and private buildings, in order to create a comfortable, usable, understandable and memorable neighbourhood / community;
  - b. To create a strong community image by enhancing the character of the built environment including building design and massing, signage, planting and streetscape;
  - c. To provide a sensitive transition between the concentration, mix and massing of uses and buildings within the Centre and the lower density community outside the Centre;

- d. To ensure that neighbouring developments within the Centre and adjoining it are physically compatible and complementary;
- e. To create a pedestrian-friendly environment through the layout of streets, configuration of lots, and siting of buildings and their entrances;
- f. To ensure that the retail commercial development is planned to support a street-related, pedestrian-friendly environment;
- g. To support the viability of transit by coordinating land use, transportation infrastructure and urban design in a mutually complementary manner supporting and encouraging pedestrian-oriented and transit-supportive development; and,
- h. To develop attractive streetscapes through attention to the design of the public realm, built form, and the relationship between buildings, streetscapes and other public areas, based on application of the following principles:
  - i. Ensure that development adjacent to the Main Street and Urban Square is designed to establish a comfortable, human-scale environment for pedestrians;
  - ii. Encourage pedestrian travel throughout the community through establishment of a grid network of streets, creation of a comfortable, safe and attractive walking environment, and connectivity to parks, public buildings and facilities and the commercial uses;
  - iii. Provide a consistent level of streetscape design, lighting, planting, signage, street furniture and other amenities;
  - iv. Ensure that all public and private areas are designed in a manner which is safe, secure and subject to informal surveillance, including walkways, building entrances and parking areas;
  - v. Establish a strong relationship between buildings and the street by minimizing setbacks and orienting main entrances to public sidewalks and on-street parking; and,
  - vi. Design service and parking facilities to complement the pedestrian system and enhance the attractiveness of the public realm.

### **Ecosystem**

- 11.2.4.3. Development shall occur in a manner consistent with Chapter 3 of the Official Plan and in conformity with the policies of the Oak Ridges Moraine Conservation Plan, which provides for the protection and conservation of the quality of environmental resources.

The objectives associated with this principle are:

- a. To protect the ecological health and integrity of the Oak Ridges Moraine, existing woodlots, and valley and stream corridors located within the Centre;
- b. To provide for a system of stormwater management for both water quality and quantity, through the application of state-of-the-art Stormwater Management Practices, designed to protect lands and receiving waters within and outside the Centre from detrimental effects such as erosion, flooding and sedimentation resulting from increased levels and rates of runoff;
- c. To ensure that environmental impacts are addressed in an ecologically responsible manner, consistent with the City's environmental policies;
- d. To encourage environmental remediation measures within new developments, where appropriate;
- e. To encourage the use of passive and active renewable energy sources;
- f. To encourage the use of 'green' technologies in the design of new buildings and infrastructure; and,
- g. To reduce vehicular dependency by encouraging residents to both live and work in the vicinity, and by providing easy access to public transit.

### **Range of Housing Types**

- 11.2.4.4. Development of a broad range of housing forms and types will be provided for in order to provide households of varied ages, incomes and demographic composition with choices and lifestyle opportunities. The objectives associated with this principle are:
- a. To create an adequate supply of housing, including a variety of low-rise, mid-rise and mixed-use housing options, unit types and sizes; and,
  - b. To encourage opportunities for residents to live and work in the Carrville community, through encouragement of appropriate commercial development within the Centre, and enabling home-based employment.

### **Range of Commercial Uses**

- 11.2.4.5. Development of a broad range of retail and office commercial uses will be encouraged in keeping with the Centre's function in the City's hierarchy of centres. This commercial development will provide increased opportunities for business competition, and provide a wide variety of commercial goods and services at competitive costs and prices to meet the needs of Centre residents and the residents of the surrounding Carrville

community. The objectives associated with this principle are:

- a. To promote the development of a mixed-use residential and commercial shopping district focused on the Main Street and Urban Square identified on Map 11.2.A. It is expected that development adjacent to Main Street and Urban Square will create an attractive shopping precinct at a human scale;
- b. To establish the mixed-use residential and commercial shopping district as an integral component of a pedestrian-friendly urban environment which is conveniently accessible by public transit, bicycle, and on foot from the surrounding community;
- c. To direct auto-oriented retail uses, such as service stations and drive-through businesses to locations outside the Centre;
- d. To discourage large format, single storey retail uses from locating within the Centre; and,
- e. To ensure that the Centre develops in a manner consistent with its 'planned function' in the City's hierarchy of retail commercial centres.

### **Parks and Open Space**

11.2.4.6. Parks and open space will be developed to satisfy the passive and active recreational needs of District Centre residents, to enhance the aesthetic appeal of the District Centre, to soften the transition between areas of lower intensity uses, and to connect to the parks and open space system in the surrounding community. The objectives associated with this principle are:

- a. To establish a parks and open space network where appropriate passive and active recreation opportunities are available to address the needs of the anticipated resident and worker population within the Carrville Centre;
- b. To plan and design open space linkages that facilitate continuous, uninterrupted pedestrian and cycling movement within the Carrville Centre, and to adjacent communities;
- c. To plan and design parks that provide central common green spaces within the Carrville Centre to address the recreational needs of residents, employees and visitors; and,
- d. To establish a hierarchy of parks to meet the varying social and recreational needs typical of an urban mixed-use centre. Their location and programming shall be determined by the City.

### **Institutional Opportunities**

- 11.2.4.7. The opportunity for a range of institutional services and facilities will be provided within Carrville Centre to address the needs of its residents and the surrounding Carrville community. The objectives associated with this principle are:
- a. To encourage and assist institutional users and public agencies in securing suitable sites and space to enable them to serve the community's social, cultural, educational, government, health care and religious needs;
  - b. To ensure that the institutional uses are compatible with and complementary to neighbouring uses and the surrounding community, and are designed to facilitate the achievement of the urban design and streetscape objectives of the Plan;
  - c. To encourage major institutional users to locate on sites conveniently accessible to pedestrians and via the public transit network;
  - d. To ensure convenient access for all residents to essential community services, parks, day care, recreation facilities, other institutional uses, and public transit services; and
  - e. To encourage establishment of social services, parks, public facilities and public transit services in locations that are conveniently accessible, particularly to individuals and households with more limited mobility, including seniors, families with young children and the physically challenged.

### **Efficient Transportation**

- 11.2.4.8. An efficient transportation network will be developed to serve the Carrville Centre and the surrounding Carrville community, which permits efficient vehicular movement while encouraging travel by non-auto modes, including on foot, bicycle and public transit. The objectives associated with this principle are:
- a. To develop a grid network of continuous and interconnected arterial, primary and local streets and laneways, and clearly defined development blocks, complemented by a public transit network, facilitating efficient movement by all modes of transportation;
  - b. To develop a walkable, pedestrian-friendly environment, with neighbourhoods that are well connected to public facilities, parks and commercial areas. To encourage walking through a contiguous, street-oriented central area that is safe, attractive, and human in scale in keeping with the urban design and streetscaping objectives of this Plan;
  - c. To encourage a significant increase in public transit ridership to reduce traffic

- congestion, energy costs and pollution, and foster greater efficiency in the operation of the transportation system;
- d. To encourage development to become increasingly transit supportive in terms of land use, density and urban design as the Carrville Centre evolves over time;
  - e. To provide on street parking on primary and local roads in the Centre and lay-by parking on the arterial roads and the Main Street to support street-related building entrances, and retail uses that are accessible to both pedestrians and drivers;
  - f. To encourage travel on foot and bicycle, by providing safe and convenient pedestrian and bike networks on public streets connected to parks, public facilities and the commercial area;
  - g. To design streets to enhance public safety and reduce traffic speeds;
  - h. To reduce the need for commuter travel by encouraging a closer relationship between the workplace and the home;
  - i. To acquire and protect road rights-of-way and transit rights-of-way that make up the grid network of primary and local roads and support the urban design and streetscaping policies of this Secondary Plan; and,
  - j. To support the provision of transit priority measures for transit services on Dufferin Street and Rutherford Road.

### **Anticipate the Growth**

11.2.4.9. Planning and development will proceed in a comprehensive and progressive manner that anticipates the needs of the forecast resident population. The objectives associated with this principle are:

- a. Where a development proposal does not achieve all of the development potential afforded by this Plan, the City shall require the preparation of a Development Concept Report and Phasing Plan that provides for the logical progression of development from its initial phase to a mature state reflecting the urban form envisioned by this Plan, and achieving certain minimum development objectives;
- b. To require all development to proceed on the basis of full services, logically extended in keeping with the availability of servicing capacity, and designed and built at a scale which ensures that the forecast population and employment growth resulting from this Plan can be accommodated
- c. To ensure that the municipality promotes and participates in the early development of the key public sector elements needed to achieve the vision for the Carrville Centre, including the establishment of:

- i. Public transit services, and associated infrastructure and facilities;
- ii. A grid network of public road rights-of-way, as identified on Map 11.2.B;
- iii. Lay-by parking on the Arterial Roads and Main Street, and on-street parking on primary and local roads in the Centre;
- iv. A pedestrian promenade with a high-quality streetscape treatment along the Main Street and Urban Square identified on Maps 11.2.A and 11.2.B; and,
- v. A system of enhanced urban squares for passive and active recreation;
- d. To plan well in advance, in cooperation with the Region of York, to ensure that infrastructure improvements at both the local and regional level are planned on a comprehensive basis, and built in time to address the needs of the Carrville Centre, in concert with market demand; and,
- e. To provide high quality development within the Centre by:
  - i. Encouraging and attracting a wide range of private investment within the Carrville Centre;
  - ii. Adding value to private lands served by the enhanced public realm;
  - iii. Encouraging a range and mix of residential and employment generating land uses, including a substantial retail focus;
  - iv. Establishing simple development rules in order to encourage, rather than discourage, development that meets the objectives of this Secondary Plan; and,
  - v. To ensure that the costs of private development are fully borne by, shared equitably among, and recovered from benefiting landowners.

### 11.2.5 General Land Use Policies

The Carrville Centre is intended to become a centre of commercial and residential activities within the Carrville Community. It is intended to become a highly desirable mixed-use area, focused on commercial and residential land uses. Its role as a focus of transit services will be enhanced over time with the establishment of a transit facility in the quadrant north of Rutherford Road and west of Dufferin Street. The policies of this Section of the Secondary Plan refer to Map 11.2.A, which identifies the applicable land use designations. The following outlines the requirements and planning parameters for housing, commercial, retail, social, open space/environmental and community facilities, which will meet the needs of the population.

- 11.2.5.1. Map 11.2.A illustrates the precise physical boundaries of the Carrville Centre. Map

11.2.A also identifies the location and distribution of the following land use designations:

- a. High-Rise Mixed-Use;
- b. Mid-Rise Mixed-Use;
- c. High-Rise Residential;
- d. Park;
- e. Natural Areas; and
- f. Infrastructure and Utility (Stormwater Management Area).

- 11.2.5.2. It is intended that the Carrville Centre will, upon full build out, accommodate approximately 11,000 to 12,000 people. It is expected, however, that the maximum residential development potential of this Centre may not be achieved until beyond the 20 year time horizon of this Secondary Plan.
- 11.2.5.3. It is the intent of this Secondary Plan to encourage mixed-use development throughout the Carrville Centre. Generally, for the purposes of this Secondary Plan, mixed-use development refers to buildings that include retail uses at grade, with residential and/or office uses above.
- 11.2.5.4. It is intended that the density of development within the Carrville Centre will be controlled through the use of a Floor Space Index. A Floor Space Index is a ratio of the amount of permitted development (measured as gross floor area) in relation to the size of the development lot. Buffer areas associated with valley and stream corridors and conveyed to the TRCA, may also be included for the purpose of calculating permitted development density (FSI) on adjacent lots.
- 11.2.5.5. Commercial Development, including retail and office uses, may be distributed among the **High-Rise Mixed-Use** and **Mid-Rise Mixed-Use** designations, subject to the following:
- a. Notwithstanding Policy 9.2.2.7.b.i of Volume 1 of this Plan, there shall be no limit on the Gross Leasable Floor Area for office commercial development;
  - b. Notwithstanding policy 9.2.2.7.c, of Volume 1 of this Plan, retail commercial development in stand-alone, single use retail buildings shall not exceed 26,000 square metres of Gross Leasable Floor Area within the Carrville Centre;
  - c. Notwithstanding those limitations on stand-alone retail facilities, any retail commercial space that is built as an integrated and ancillary component of a

mixed-use residential/retail, office/retail or office/residential/retail building shall not contribute toward the retail space cap that has been established for the Carrville Centre; and,

- d. Mixed-Use buildings that include a retail component are permitted in the **High-Rise Mixed-Use** and **Mid-Rise Mixed-Use** designations, as shown in Map 11.2.A.

11.2.5.5. A strong public presence is an important component of the success of an urban centre. The Carrville Centre will include a landmark quality building located within the **Mid-Rise Mixed-Use** designation, and identified as \*2 on Map 11.2.A. The site may include a library, daycare, arts and cultural facilities, higher density residential and/or professional offices. Retail facilities may also be provided. Retail commercial facilities provided within the potential community facility building shall not contribute toward the retail space cap that has been established for the Carrville Centre.

11.2.5.6. The Carrville Centre shall also include a public transit facility in the northwest quadrant of the Centre, west of Dufferin Street and north of Rutherford Road to coincide with the highest local concentration of potential ridership. It shall be designed and located to provide optimal access for pedestrians and transit vehicles. The City will work with York Region Transit and the affected landowner to identify and implement the Transit Facility within the District Centre based on policies 11.2.18.4 through 11.2.18.9.

11.2.5.7. The Carrville Centre includes the tributaries of the Upper East Don River, woodlots, ravines and an array of open space features. The Centre also includes parks, urban squares, gateways and smaller open space linkages. These features collectively form the linked greenway system within the District Centre.

## 11.2.6 High-Rise Mixed-Use

11.2.6.1. Notwithstanding policy 9.2.1.1, the following policies apply in the areas identified as **High-Rise Mixed-Use** on Map 11.2.A.

11.2.6.2. The **High-Rise Mixed-Use** designation represents the primary retail development areas within the Carrville Centre. It is the intent of this Secondary Plan to establish a lively and active mixed-use shopping district at these locations.

- 11.2.6.3. Development in these areas includes mixed-use developments characterized by high quality design standards. The visual attractiveness and consistent image of these areas is of prime importance. Development related and scaled to the pedestrian is required on all lands within this designation.
- 11.2.6.4. The amount of development potential within the **High-Rise Mixed-Use** designation provides the opportunity for the achievement of a substantive urban centre with market synergies existing among the various uses. In addition, the order of magnitude is sufficient to create an identity for this area that will set it apart from other areas in Vaughan.
- 11.2.6.5. Within the **High-Rise Mixed-Use** designation, development is planned to be mixed-use with commercial (retail and/or office) and residential / office uses in the same building, and with primarily retail uses, including eating establishments, located at grade. The following uses shall be permitted as part of mixed-use buildings:
- a. Residential units;
  - b. A broad variety of retail and service commercial activities, excluding:
    - i. individual retail uses in buildings with ground floor plates in excess of 5,750 square metres in size;
    - ii. drive-through facilities and/or gas bars/service station either stand alone or, associated with any permitted use; and/or,
    - iii. retail uses that require outdoor storage, with the exception of a seasonal garden centre ancillary to another permitted retail use;
  - c. Offices;
  - d. Public and institutional uses, public health facilities, libraries, places of worship and all types of special needs housing;
  - e. Cultural, entertainment and social facilities;
  - f. Non-profit clubs and organizations;
  - g. Community facilities such as day care facilities, public parking facilities, parks, squares and open space linkages; and,
  - h. Public utilities and public transit facilities.
- 11.2.6.6. The following building types are permitted in the High-Rise Mixed-Use designation, pursuant to Section 9.2.3:
- a. Townhouses;

- b. Stacked Townhouses;
- c. Low-Rise buildings;
- d. Mid-Rise buildings;
- e. High-Rise buildings; and,
- f. Public and Private Institutional buildings.

- 11.2.6.7. The following policies apply within the **High-Rise Mixed-Use** designation:
- a. Notwithstanding policy 9.2.1.6, mixed-use buildings shall be permitted to have a maximum Floor Space Index identified on Map 11.2.A;
  - b. Notwithstanding policy 9.2.1.5, the minimum and maximum heights for any building within the **High-Rise Mixed-Use** designation shall be as identified on Map 11.2.A;
  - c. Stand-alone, single use buildings are not permitted within the **High-Rise Mixed-Use** designation;
  - d. All buildings shall be sited close to the street right-of-way and create an attractive and interesting public realm;
  - e. Map 11.2.B identifies a Main Street and Urban Square. It is the intent of the City that this Main Street and Urban Square be developed with a combination of small-scale retail facilities, as well as higher density residential and office commercial uses. On the Main Street and Urban Square the High-Rise Mixed-Use designation also permits the following uses:
    - i. First and second storey retail, including office commercial on the second storey, located in the same building; and
    - ii. Live/work units;
  - f. It is anticipated that this Main Street and Urban Square will be anchored by the permitted larger format retail facility identified in subsection 11.2.6.8.d.v. below, and a building including a public institution;
  - g. To create the desired character and environment on lots abutting Main Street and Urban Square, buildings, and their main public pedestrian entrances, shall face the Main Street and Urban Square and shall be located close to the front lot line as specified in policy 11.2.14.13 of this Secondary Plan. All buildings on lots abutting the Main Street and Urban Square shall provide retail uses at grade. All individual retail uses in buildings on lots abutting the Main Street and Urban Square shall be limited in size to a maximum of 2,500 square metres of Gross Floor Area;
  - h. The City may consider a variety of development incentives. Qualification for any

incentive (or incentives package) for any individual development shall be tied to the achievement of below grade parking, and conformity with the urban design policies of this Plan and any additional urban design guidelines approved by the City; and,

- i. In reviewing any application proposing retail commercial development, that would result in more than the identified 40,000 square metre maximum Gross Leasable Floor Area within the District Centre, the City may require the submission of a market study or other form of analysis, prior to further consideration of the application. Such study or analysis shall be of a methodology satisfactory to the City.

11.2.6.8. The following exceptions to the permitted uses and policies of the **High-Rise Mixed-Use** designation are permitted:

- a. Notwithstanding the policies of 11.2.6.5 of this Secondary Plan, a gas bar may be permitted at the location identified by \*1 on Map 11.2.A. Specific urban design guidelines shall apply and the permitted gas bar site shall not exceed 0.3 of a hectare in site area.
- b. Notwithstanding the policies of 11.2.6 noted above, the property identified by \*2 on Map 11.2.A is intended to become the location for a landmark quality building that may include a public institution. This site, being prominently located on the north side of the open space/urban square, provides the opportunity to locate a highly visible building that will play an important role in attracting people to the District Centre and generating a high level of social activity. The design quality of the building and associated landscaping must set the standard for the Carrville Centre.
- c. The following uses are permitted on the **High-Rise Mixed-Use** site identified with \*2 on Map 11.2.A:
  - i. Office uses;
  - ii. High density residential uses;
  - iii. Small scale retail uses with individual stores having less than 2,500 square metres of Gross Floor Area;
  - iv. Cultural and social facilities;
  - v. Recreational facilities;
  - vi. Community facilities;
  - vii. Transit Facility; and,
  - viii. Public utilities.

- d. The following policies apply to the **High-Rise Mixed-Use** site identified with \*2 on Map 11.2.A:
- i. The maximum height for any building is identified on Map 11.2.A. Notwithstanding that height limitation, a height bonus, permitting development up to a maximum of 8 storeys may be considered if the proposed development includes a public institution, municipally-operated cultural, social, and/or recreational facility;
  - ii. The Floor Space Index shall not exceed 3.0;
  - iii. All development shall conform to policies 11.2.6.7.e and 11.2.6.7.f of this Secondary Plan;
  - iv. Notwithstanding the policies of Section 11.2.6 of this Secondary Plan, on the development block identified by \*3 on Map 11.2.A, one stand-alone; single-use food store building may be permitted with no restriction to maximum floor plate area, providing all of its required parking and landscaping are included on the same block. The food store shall have a minimum height of 10 metres, which may contain one storey plus an above grade mezzanine as an alternative to a two-storey building;
  - v. Notwithstanding the policies of Section 11.2.6 of this Secondary Plan, on the development block identified by \*4 on Map 11.2.A, one stand-alone; single-use retail building may be permitted. Further, the ground floor plate of that retail building shall be a maximum of 5,750 square metres in size. In addition, the minimum height of this building shall be 2 storeys or 6.7 metres;
  - vi. Notwithstanding the policies of Section 11.2.6 of this Plan, on the development block identified by \*5 on Map 11.2.A, one stand-alone; single-use retail building may be permitted. Further, the ground floor plate of that retail building shall be a maximum of 5,750 square metres in size; and,
  - vii. Notwithstanding the policies of Section 11.2.6 of this Plan, on the development block identified by \*6 on Schedule A, one stand alone drive through bank facility may be permitted, subject to design criteria and the requirement that the facilities for the drive-through component shall not face a public road. The bank building shall not be subject to a minimum building height on Map 11.2.A. Further, one additional stand alone building will be permitted at the north-east corner of the subject property, and this building shall not be subject to the minimum building height on Map 11.2.A.

## 11.2.7 Mid-Rise Mixed-Use Designation

- 11.2.7.1. Notwithstanding policy 9.2.1.1, the following policies apply in the areas identified as **Mid-Rise Mixed-Use** on Map 11.2.A.
- 11.2.7.2. The **Mid-Rise Mixed-Use** designation represents a lower scale, mixed-use area within the Carrville Centre. It is the intent of this Plan to establish the opportunity for live-work units and medium density house forms within this designation.
- 11.2.7.3. Development in this area includes single use residential buildings and mixed-use/live-work developments characterized by high design standards. The visual attractiveness and consistent image of the area is of prime importance. Development related and scaled to the pedestrian is required on all lands within this designation.
- 11.2.7.4. The **Mid-Rise Mixed-Use** designation is intended as a buffer between the larger scale uses of the **High-Rise Mixed-Use** designation and the adjacent residential neighbourhoods.
- 11.2.7.5. The **Mid-Rise Mixed-Use** designation permits:
- a. Residential and live-work units;
  - b. Small scale commercial uses, including individual retail establishments with less than 2,500 square metres of Gross Leasable Floor Area that are a contiguous component of a mixed-use building. All forms of drive-through facilities and uses requiring outdoor storage are specifically excluded from this designation;
  - c. Small scale office uses, that are a contiguous component of a mixed-use building;
  - d. Public and institutional uses, public health facilities, and places of worship, and all types of special needs housing;
  - e. Cultural and social facilities;
  - f. Community facilities such as day care facilities, parks and urban squares; and,
  - g. Public utilities.
- 11.2.7.6. The following building types are permitted in the Mid-Rise Mixed-Use designation, pursuant to Section 9.2.3:
- a. Townhouses;
  - b. Stacked Townhouses;
  - c. Low-Rise buildings;

- d. Mid-Rise buildings;
  - e. High-Rise buildings; and,
  - f. Public and Private Institutional buildings.
- 11.2.7.7. Notwithstanding policy 9.2.1.6, stand alone residential or mixed-use residential/retail or residential/office developments shall be permitted to have a maximum Floor Space Index as identified on Map 11.2.A.
- 11.2.7.8. Notwithstanding policy 9.2.1.5, the minimum and maximum heights for any building within the **Mid-Rise Mixed-Use** designation shall be as identified on Map 11.2.A.
- 11.2.7.9. The City may consider a variety of development incentives. Qualification for any incentive (or incentive package) for any individual development shall be tied to the achievement of below grade parking and conformity with the urban design policies of this Plan, and any additional urban design guidelines approved by the City.

## 11.2.8 High-Rise Residential Designation

- 11.2.8.1. Selected sites within the Carrville Centre are designated for **High-Rise Residential** uses. It is the intention of this Secondary Plan that this area shall be developed with high quality and a consistent level of urban design for the public and private realm.
- 11.2.8.2. Notwithstanding policy 9.2.1.1, the **High-Rise Residential** designation permits:
- a. Residential units;
  - b. Places of worship;
  - c. All types of special needs housing;
  - d. Community facilities such as day care facilities, parks and urban squares; and,
  - e. Public utilities.
- 11.2.8.3. The following building types are permitted in the High-Rise Residential designation, pursuant to Section 9.2.3:
- a. Townhouses;
  - b. Stacked Townhouses;
  - c. Low-Rise buildings;
  - d. Mid-Rise buildings;
  - e. High-Rise buildings; and,
  - f. Public and Private Institutional buildings.

- 11.2.8.4. Notwithstanding policy 9.2.1.6, all residential uses may be developed up to a maximum Floor Space Index, as identified on Map 11.2.A.
- 11.2.8.5. Notwithstanding policy 9.2.1.5, the minimum and maximum heights for any building within the **High-Rise Residential** designation shall be as identified on Map 11.2.A.
- 11.2.8.6. Structured parking garages are required within the **High-Rise Residential** designation. 90 percent of the required parking shall be provided in structures. A maximum of 10 percent of the required parking for any development may be provided at grade, subject to approval by the City.
- 11.2.8.7. Within the **High-Rise Residential** designation, abutting Rutherford Road, east of Dufferin Street, a landscaped greenway, 6.0 metres in width, shall be provided adjacent to the road allowance, providing a connection between two areas of valleylands/ woodlots.

### 11.2.9 Low-Rise Mixed-Use Designation

- 11.2.9.1. Medium density forms of housing are used in the Carrville Centre to provide an appropriate transition from the low density neighbourhoods surrounding the Centre to the higher intensity uses within it. It is the intention of this Secondary Plan that the medium density housing shall be high in quality, and supportive of the urban design objectives of the Carrville Centre Plan.
- 11.2.9.2. Notwithstanding policy 9.2.1.1 the **Low-Rise Mixed-Use** designation permits:
- a. Residential units;
  - b. Places of worship;
  - c. All types of special needs housing;
  - d. Cultural and social facilities;
  - e. Community facilities; and,
  - f. Public utilities.
- 11.2.9.3. The following building types are permitted in the Low-Rise Mixed-Use designation, pursuant to Section 9.2.3:
- a. Townhouses;
  - b. Stacked Townhouses; and,
  - c. Public and Private Institutional buildings.

- 11.2.9.4. Street, block and stacked townhouse units may be developed up to a maximum Floor Space Index of 2.0, as identified on Map 11.2.A.
- 11.2.9.5. The minimum and maximum heights for any building within the **Low-Rise Mixed-Use** designation shall be as identified on Map 11.2.A.
- 11.2.9.6. Access to parking garages from public laneways is preferred. Developments that provide access to garages facing a Primary Road shall not be permitted. Developments that provide garages facing a local road shall be discouraged.

### 11.2.10 Park Designation

- 11.2.10.1. The **Park** designation includes a variety of public park features connecting to the broader greenway, pedestrian and bicycle systems of the City. An urban centre is different than suburban neighbourhoods. It typically requires smaller parks, distributed strategically throughout the centre to enhance adjacent development. It is the intention of this Plan to promote parks as key aesthetic and functional components to complement the anticipated higher intensity forms of development.
- 11.2.10.2. Notwithstanding policy 9.2.1.1, the following policies apply to areas designated as Park on Map 11.2.A.
- 11.2.10.3. The **Park** designation permits:
  - a. Indoor and outdoor recreational facilities;
  - b. Cultural uses; and,
  - c. Public utilities, where required.
- 11.2.10.4. In recognition that the Carrville Centre is, by its intended urban character, different than adjacent suburban neighbourhoods, and the fact that traditional standards and requirements for parks and recreation planning may not be met within the Centre, the following policies apply within the **Park** designation:
  - a. The City may acquire lands anywhere within the Carrville Centre for the purposes of establishing parkland. The parkland designated by this Secondary Plan shall be shown on draft plans of subdivision. Their actual size, configuration and design will be subject to confirmation by the City during the development approval process;
  - b. There may be some minor adjustments to location and configuration of parks at

- the Draft Plan of Subdivision/Site Plan stage, subject to City Staff approval;
- c. Lands utilized for stormwater management facilities shall generally not be considered as any part of the parkland dedication requirement provided via the Planning Act;
  - d. All development applications should include a potential location for an urban square. Where the City has determined that the parkland dedication is not required on an individual development site, cash-in-lieu of parkland may be accepted by the City;
  - e. All parkland shall be designed and built to the satisfaction of the City. The functional design of each park component identified on Map 11.2.A will be appropriate for the nature and character of adjacent development;
  - f. A percentage (to be identified in the zoning by-law) of the required Amenity Space will be provided outdoors and on the ground level, to complement the public parks and open space system; and,
  - g. Urban squares are intended as formal spaces for passive recreation, in support of the adjacent higher density, mixed-use development. Urban squares should address the following design guidelines:
    - i. urban squares are to be planned as focal points throughout the District Centre;
    - ii. streetscapes that abut an urban square should be designed to reinforce a high quality formalized relationship between the open space and its adjacent land use;
    - iii. buildings should front onto an urban square to create built form edges to the public space;
    - iv. the landscape along the street frontage, including high canopy street trees, should be complementary on both sides of the street;
    - v. entry and access points should be located conveniently and incorporate civic design themes; and,
    - vi. hard and soft landscape elements and features shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.

### 11.2.11 Natural Areas Designation

- 11.2.11.1. It is intended that lands designated **Natural Areas** serve to protect and enhance the natural ecosystem and to maintain biological diversity within Vaughan. Furthermore,

most of the Centre is identified as a 'Settlement Area' within the Oak Ridges Moraine Conservation Plan, and shall be developed in a manner consistent with applicable Provincial legislation and in conformity with the policies of Chapter 3 of the Official Plan.

- 11.2.11.2. Notwithstanding policy 9.2.1.1, Natural Areas, identified on Map 11.2.A shall be subject to the following policies.
- 11.2.11.3. The **Natural Areas** designation shall contain one or more of the following natural features:
- a. Watercourses;
  - b. Valleylands;
  - c. Floodplains; and,
  - d. Woodlots.
- 11.2.11.4. The **Natural Areas** designation permits:
- a. Conservation areas;
  - b. Wildlife and fisheries management areas;
  - c. Essential public works including transportation, utility, watershed management and flood and erosion control facilities (but not stormwater management ponds), to the satisfaction of Toronto Region Conservation Authority;
  - d. Trails, walkways, and bicycle paths;
  - e. Natural interpretive centres;
  - f. Tableland woodlots; and,
  - g. Buildings accessory to other permitted uses.
- 11.2.11.5. Public works shall be located outside **Natural Areas**, unless it is demonstrated that there is no reasonable alternative and then only if such works meet the requirements of the Official Plan.
- 11.2.11.6. New or expanded permitted uses which are not subject to the Environmental Assessment Act, shall be permitted provided that, as a condition of development approval:
- a. An Environmental Impact Statement (EIS) is undertaken; and,
  - b. The use will not significantly impact any of the natural features or functions contained within the **Natural Areas** designation; and alternative methods and measures for minimizing impacts have been considered.

- 11.2.11.7. Natural features classified as areas where no development is permitted shall be designated as **Natural Areas**.
- 11.2.11.8. Development shall not be permitted on lands adjacent to a **Natural Areas** designation, or a natural feature described in this section, if it significantly impacts the integrity of the feature or the ecological functions for which the feature is identified.
- 11.2.11.9. The specific boundaries and appropriate buffers of a natural feature shall be identified through an Environmental Impact Statement, a natural features study or a Subwatershed Plan.
- 11.2.11.10. Notwithstanding any other policy in this Secondary Plan, a 10 metre ecological buffer shall be provided adjacent to valley and stream corridors. This buffer shall be located outside of the development block or lot; it shall be zoned an appropriate open space zone to prohibit the placement of buildings or structures; and shall form part of the public open space system. It shall be conveyed to the City or the TRCA with the adjoining valley and stream corridor. The buffer shall be 10 metres inland from the greater of the physical top of bank; the predicted long term stable slope line; the regulatory floodplain or the meander belt of the watercourse where there is no floodplain; or the drip line of significant vegetation contiguous to a valley and stream corridor.
- 11.2.11.11. The City shall cooperate with York Region, the Conservation Authority, local interest groups, and neighbouring municipalities in the creation of a continuous, interconnected trail system within the **Natural Areas**, 'Stormwater Management Area' and 'Parks' designations.
- 11.2.11.12. The City shall cooperate with York Region and the Conservation Authority to identify, designate, protect, conserve and remediate newly identified and already identified natural features.
- 11.2.11.13. The limits of all designated natural features should be staked and surveyed in the field in consultation with the TRCA and City of Vaughan. The establishment of the development constraint line should also take into account the buffer requirements and any special edge management measures necessary to protect the feature.

- 11.2.11.14. To maintain and enhance (where feasible) the existing groundwater regime, an appropriate level of hydrogeological investigation should be completed. This work should be completed in conjunction with stormwater management plans. Given the higher density development that is proposed within the Centre and the potential for groundwater interference, the hydrogeological investigation should identify appropriate mitigation measures to protect groundwater conditions and flow patterns. Measures to offset a reduction in groundwater recharge within higher density areas should also be addressed. Where soil conditions permit, recharge of clean runoff should be maximized to the extent feasible with the Centre. Various at-source, conveyance and end-of-pipe infiltration measures should be considered to achieve this objective.
- 11.2.11.15. A 5.0 to 10.0 metre edge management and grading adjustment zone should be established adjacent to all natural features. A 10.0 metre buffer shall be provided for valley and stream corridors. The purpose of this zone is to provide flexibility to achieve the following:
- a. match grading at staked development limits;
  - b. pedestrian walkways;
  - c. edge restoration/enhancement plantings;
  - d. edge/hazard tree management; and,
  - e. groundwater/stormwater management measures.
- 11.2.11.16. Suitable uses within buffer areas include passive open space amenity areas, the boulevard portion of street right-of-ways, stormwater/groundwater management facilities, pedestrian trails and naturalized landscaping. The buffer area shall form part of the Open Space System and shall be zoned as Open Space.

## **11.2.12 Infrastructure and Utilities Designation**

- 11.2.12.1. The **Infrastructure and Utilities** designation represents general locations for stormwater management facilities within the Carrville Centre.
- 11.2.12.2. Notwithstanding policy 9.2.1.1, the following policies apply to lands designated as Infrastructure and Utilities on Map 11.2.A.

- 11.2.12.3. The **Infrastructure and Utilities** designation permits:
- a. Stormwater management facilities and other essential public works including transportation, utility, watershed management and flood and erosion control facilities;
  - b. Wildlife and fisheries management areas;
  - c. Trails, walkways, and bicycle paths; and,
  - d. Buildings accessory to other permitted uses.
- 11.2.12.4. Notwithstanding the location and configuration of the lands designated **Infrastructure and Utilities**, stormwater management facilities shall be permitted in all land use designations on Map 11.2.A and shall be integrated with the open space and trail system.
- 11.2.12.5. The location and configuration of the stormwater management facilities shall be further refined through the Stormwater Management Plans for Blocks 11 and 18 and when plans of subdivision are prepared. Stormwater management facilities can be relocated without an Amendment to this Plan, subject to the agreement of the City and the Toronto and Region Conservation Authority, provided that alternative sites are consistent with the objectives and policies of this Plan and the Stormwater Management Plans for Block 11 and 18. Where identified pond blocks are relocated, resulting in land becoming available for development, such land may be redesignated for urban development without the need for an Amendment to this Plan, subject to the City's interpretation of the policies and objectives of this Plan.
- 11.2.12.6. In accordance with established policy, the stormwater drainage system will be designed to the satisfaction of the City, in consultation with the Toronto and Region Conservation Authority, the Ministry of Natural Resources and the Ministry of the Environment and Energy.
- 11.2.12.7. Stormwater facilities will be designed to maintain environmental and ecological integrity, and to provide a net benefit to the environment if feasible.
- 11.2.12.8. Stormwater facilities will be designed, wherever possible, as attractive community amenities and to provide for a functioning wildlife habitat.

### **11.2.13 Urban Design Plan**

- 11.2.13.1. This Secondary Plan is based on a Planning and Urban Design Study approved by Council. The study addressed land use and urban design considerations in a manner that enables the objectives of this Secondary Plan to be addressed through comprehensive, integrated planning solutions.
- 11.2.13.2. This Secondary Plan shall form the basis for subsequent submissions of implementing Development Concept Reports, draft plans of subdivision, zoning and site development applications. The City may also adopt a zoning by-law and urban design guidelines that further clarify the direction and intent of policies in this Secondary Plan.
- 11.2.13.3. To provide flexibility in the design process, other comparable design arrangements, which achieve the principles and objectives of this Secondary Plan satisfactory to the City, may be utilized without amendment to this Secondary Plan.

### **11.2.14 Built Form**

- 11.2.14.1. The following policies apply to built form throughout the Carrville Centre:
- 11.2.14.2. Buildings shall be sited and organized to create a street space scaled to the pedestrian, and organized to present an appropriate façade to all adjacent streets to provide interest and comfort at ground level for pedestrians.
- 11.2.14.3. Commercial uses and their main entrances shall, wherever possible, be oriented toward an adjacent public street to provide convenient access to pedestrians and public transit; buildings, and their main public entrances, shall be located close to the front property line, on-street parking, and public sidewalk.
- 11.2.14.4. This Secondary Plan and the zoning by-law shall establish build-within zones and will identify the minimum proportion of frontage within each block face, which shall be occupied by buildings.
- 11.2.14.5. Buildings shall be sited to ensure adequate sunlight, sky views, and wind conditions in streets, parks and open spaces.
- 11.2.14.6. Buildings shall be sited and organized so that streets and parks are overlooked by active building faces.

- 11.2.14.7. Buildings are to be generally sited parallel to the public street and along the edges of parks and open spaces. The public faces of these buildings are to align with neighbouring buildings in a manner that defines these spaces in a consistent building face lining the street.
- 11.2.14.8. The area between the building wall and the street serves as the transition zone between the public and private realms, and constitutes an important social and visual element of the street image.
- 11.2.14.9. In the case of residential development, this semi-public space creates a “buffer zone” between the public and the private domains, which enhances the visual appearance of the street edge and provides outdoor spaces for casual social interaction. Planted and constructed elements in the semi-public space -low hedges, trees, masonry and decorative metal fences and gates -should be designed to provide a transition from the public sidewalk to the finished floor level of adjacent residences.
- 11.2.14.10. For commercial and mixed-use developments, connections to the street -by proximity, by the location of windows and entranceways and the level of architectural detail -are fundamental to the animation of the streets and in achieving the desired urban character. As such, buildings shall address the street, through the provision of active façades that include windows, entry features and, where appropriate, outdoor cafés and restaurants.
- 11.2.14.11. In addition to providing a ‘connecting link’, a relatively consistent building edge is important to provide spatial definition and containment to the street. Build-within zones are recommended for all properties within the District Centre, requiring buildings to locate their front and exterior side walls within a defined zone on the lot. The build-within zones essentially set both a minimum and maximum setback.
- 11.2.14.12. All buildings, with the exception of Townhouse units, that are permitted within the Centre shall be developed with a substantial portion of their front and exterior side facades generally between 1.0 and 3.0 metres of the front lot line and the exterior side lot line.
- 11.2.14.13. It is understood that where a lot has three sides abutting a public road, the 1.0 to 3.0 metre setback may not be achieved on the third side.

- 11.2.14.14. Notwithstanding policy 9.2.3.2 Townhouses shall be built within 4.0 and 4.5 metres of the front property line, where front garages are not proposed, and shall be built to within 6.0 and 8.0 metres of the front property line where a front garage is proposed. In all cases, the exterior side build-within zone for street townhouses shall be between 3.5 and 4.5 metres.
- 11.2.14.15. To reflect the importance of corner properties and reduce the distance between buildings for pedestrians along Rutherford Road and Dufferin Street, the build-within zone abutting the regional road sight triangle will be between 0.6 and 2.0 metres.
- 11.2.14.16. A street wall or part of the building that is closest to the public street provides important spatial definition and a sense of enclosure for that street. This is critically important along the pedestrian oriented Main Street and Urban Square identified on Map 11.2.A. A minimum amount of building wall located within the build-within zone shall be required, as follows:
- a. On all lands adjacent to the Main Street and Urban Square identified on Map 11.2.A, the minimum built frontage shall be 75 percent of the block face of each block;
  - b. On all lands adjacent to Primary Roads or Local Roads that generally run north-south within the District Centre, the minimum built frontage shall be 75 percent of the block face of each block;
  - c. On all lands adjacent to Primary Roads or Local Roads that generally run east-west within the District Centre, the minimum built frontage shall be 50 percent of the block face of each block; and,
  - d. On all lands adjacent to Rutherford Road or Dufferin Street, the minimum built frontage shall be 50 percent of the block face to accommodate larger buildings and the potential for larger parking areas between buildings.
- 11.2.14.17. In order that appropriate spacing is achieved between buildings on the same block, light, view and privacy setbacks may be used to provide the appropriate relationship between building facing conditions.
- 11.2.14.18. The advantages of better visibility, light and view, make corner sites good locations for landmark buildings. Corner sites are consequently often occupied by prestigious buildings, or by buildings of community status. In the Carrville Centre, corner sites will

play a particularly significant role in defining landmarks. It is therefore important that the treatment of the corner sites be consistent throughout the Secondary Plan. The following policies apply to corner sites:

- a. Corner building designs shall articulate, define and enhance the intersection at which it is located by enhancing the building's presence at each corner;
- b. Corner buildings should not be lower than 2 storeys;
- c. Buildings should 'turn' the corner, i.e. they should have primary, articulated facades towards both streets and should be visually different from adjacent development; and,
- d. Corner buildings should have the highest level of architectural detailing and a distinct architectural appearance.

11.2.14.19. Architectural variety is crucial in creating a visually stimulating urban environment. Street walls composed of buildings of similar style and form can succeed through subtle variations in the façade treatment and building mass in projecting an image of architectural richness, variety, and building articulation. The following policies shall apply throughout the Carrville Centre:

- a. Large areas and continuous streets of monotonous and repetitive facades shall be avoided. A more textured architectural quality can be achieved by introducing variation in certain elements of the façade treatment;
- b. Variation in three-dimensional elements, such as balconies, bay windows and porches, cornices, window trim, entrances and the articulation of the building mass, shall be used to create a dynamic façade;
- c. An interesting architectural feature/treatment shall be added to all rooftops of High Density Residential buildings to prevent typical box shaped building forms;
- d. Variation and articulation in the building mass including horizontal and vertical setbacks, such as setbacks at the upper storeys, shall be established in the implementing zoning by-law; and,
- e. Site Plan applications in the Carrville Centre will be reviewed taking into consideration matters relating to exterior design, including the character, scale, appearance and design features of buildings and their sustainable design.

### **11.2.15 Pedestrian Realm**

11.2.15.1. A grid of arterial, primary and local streets and associated public open spaces shall provide the organizing framework for the development of the District Centre.

- 11.2.15.2. Pedestrian sidewalks shall be required on both sides of all streets and shall form a connected system of optional routes within the District Centre, and connect to pedestrian systems in surrounding communities.
- 11.2.15.3. A pedestrian and bicycle path system shall be developed in conjunction with the Streetscape and Master Landscape Plan.
- 11.2.15.4. Parking facilities, service access points and any visible mechanical equipment are to be located in a manner that has a minimal physical impact on public sidewalks and accessible open spaces. Shared driveways, service courts at the side and rear of buildings are encouraged to provide for these functions.
- 11.2.15.5. To promote the comfortable pedestrian use of streets, parks and open spaces, development is to provide:
- a. Well-designed, coordinated streetscapes with sidewalks and boulevards on important pedestrian and publicly accessible open spaces including walkways and setbacks adjacent to the public sidewalks that promote access, orientation and confidence of personal safety;
  - b. Appropriate landscape treatments shall be provided, including trees and pedestrian lighting throughout parking lots and along their edges. This is intended to improve their appearance and to contribute to the visual continuity of the street edge, while encouraging the safe use of these spaces;
  - c. High quality usable open spaces that are physically and visually linked to streets, parks and mid-block pedestrian routes;
  - d. A pedestrian weather protection system including awnings, canopies, colonnades, or front porches along the sidewalk edge of important pedestrian streets or edges throughout the District Centre, and adjacent to the urban squares and at entrances to buildings;
  - e. Buildings with primary windows and signage facing onto the street;
  - f. Barrier free design of buildings, streets and publicly accessible open spaces;
  - g. Street tree planting should form a continuous canopy along the street; tree species should be selected to reinforce the role of the various street hierarchies within the Carrville Centre and to visually and thematically distinguish the streets from one another;
  - h. Transformers and other above ground utilities, should be located within the building, or on private property located away from public view and appropriately

screened subject to the satisfaction of the City;

- i. Open space links should be planned and designed to facilitate continuous, uninterrupted movement through, and enhance the use of the open space systems within the Carrville Centre. These open space links should also be connected to the adjacent communities and may include but not be limited to: utility easements, greenway corridors, woodlots, parks, courtyards, valleys, storm ponds and expanded boulevards within the road right-of-way;
- j. Lighting should be designed to promote pedestrian comfort, safety and provide a high quality ambiance suitable for a District Centre. The design of lighting, as an urban design feature, helps to define the sense of place and pedestrian scale of the Carrville Centre. In addition, accent lighting is required to emphasize built form and landscape elements. Pedestrian scale lighting should be provided adjacent to streets, walkways, public squares, pedestrian routes and in parks and courtyards;
- k. Lighting guidelines will be required as part of the urban design/landscape master plan. The draft City of Vaughan Lighting Standards and Guidelines Study, Phase I, November 4, 2004, or any subsequent lighting document approved by Council, shall be referenced in the preparation of the lighting plan; and,
- l. Signage in the District Centre will conform to signage guidelines, developed prior to the approval of a Plan of Subdivision, which address the amount and type of illumination, size, materials, typography and design. Guidelines shall ensure that signage is incorporated into the building.

11.2.15.6. In order to reinforce streets as primary public spaces, the locations of parking, driveways and service entrances need to be carefully considered and coordinated with the locations for pedestrian entrances.

- a. Parking and servicing should have the least possible impact on the streetscape and public open spaces;
- b. Parking is encouraged to be provided below grade but, alternatively, may be provided in above grade structures faced with active uses, or in landscaped surface lots to the rear or side of buildings;
- c. On-street parking spaces provided on either the local, primary, or arterial roads within the District Centre may be credited towards the parking provided on-site;
- d. Entrances to below grade or structured parking and service areas should occur within the building;
- e. Large surface parking areas are generally discouraged and, in the long term,

parking is encouraged to be located below grade. Where surface parking must be provided, the visual impact of large surface lots shall be mitigated by a combination of setbacks, and significant landscaping including: pavement treatments, low walls or decorative fencing, landscape materials, trees and lighting throughout parking lots and along the edges; and,

- f. Surface parking lots or spaces should be set back 3.0 metres from the property line. The setback should be substantially landscaped with decorative fencing and coniferous and deciduous planting providing seasonal interest in order to continue to define the street edge and provide an enhanced environment for pedestrians and drivers alike.
- g. Access to parking and servicing areas should occur off local streets or service lanes and to the side or rear of buildings.

11.2.15.7. Loading areas are not permitted in any yard facing a street, unless they can be adequately screened from view, to the satisfaction of the City. The location of loading areas will be controlled in the zoning by-law.

11.2.15.8. No outdoor storage is permitted within the Centre, with the exception of seasonal garden centres established in conjunction with a permitted retail use.

11.2.15.9. Courtyards will be formed in many of the commercial, residential and mixed-use blocks. Their primary role will be to provide on-site, at-grade open space amenity for occupants of that block. The functional and spatial characteristics of the courtyard vary depending on building typology and size of the block. Residences with generous private yards might require less semi-private open space while multi-dwelling buildings would benefit from more ample courtyard spaces. The following policies apply throughout the Centre:

- a. During subsequent site plan review, development applications will be expected to present coordinated design concepts for courtyards that provide appropriate grading, pedestrian and landscape facilities, coordinated servicing and automobile access to maximize usable landscaped open space; and,
- b. To be most effective, courtyards should have a unified landscape design that ensures that gardens and play areas, on individual sites, can be shared with those living or working on the block as a whole.

- 11.2.15.10. In the Centre, the provision of community services, restaurants, cafés, stores and display windows at grade provides visual interest, encourages the use of sidewalks, promotes retail continuity and viability, and contributes to a safer and more vibrant pedestrian environment.
- a. Buildings shall, to the greatest extent possible, front onto public streets, be flush with grade and provide an active use at grade in order to promote pedestrian activity;
  - b. Within the **High-Rise Mixed-Use** and **Mid-Rise Mixed-Use** designations, ground floor uses will change over time to adapt to a variety of community needs. As a result, the floor-to-ceiling height of ground floors for all buildings should be 3.3 to 4.0 metres to be sufficient to adapt to all permitted uses;
  - c. Principal pedestrian entrances shall provide direct access to the public sidewalk; and
  - d. The City shall consider introducing provision in the zoning by-law to permit the operation of outdoor cafés.
- 11.2.15.11. The provision of publicly accessible, privately-owned, mid-block urban squares are encouraged on individual sites in order to complement the public open space system. These will be provided on an incremental basis as development occurs. Mid-block pedestrian connections should:
- a. Be provided within larger development parcels. These are intended to be designed as pedestrian landscaped mews and should be lit, landscaped and maintained for public use;
  - b. Provide a fine grain of pedestrian circulation and an important connection between two streets;
  - c. Lead to public destinations such as schools, parks and public transit; and
  - d. Provide an address to individual residential or business frontages along their lengths.

### 11.2.16 Road Allowance and Streetscaping

11.2.16.1. The arterial, primary and local roads in the District Centre identified in Map 11.2.B shall form a connected system to serve all parts of the District Centre and allow for multiple choices of routes for both pedestrians and motorists.

11.2.16.2. Within the District Centre, Rutherford Road and Dufferin Street shall be modified to

reflect their role and function as multi-purpose urban streets that are both transportation corridors and pedestrian oriented places. They shall be designed to accommodate a wide range of uses and users. The potential treatment of the road allowance is illustrated in Figure 11.2.A. The City of Vaughan will work with York Region towards the implementation of these enhanced streetscapes as the Carrville Centre develops over time.

- 11.2.16.3. The Primary Roads connect the District to the adjacent residential neighbourhoods. They shall be designed to accommodate a wide range of uses and users. Individual access points from a Primary Road shall be restricted.
- 11.2.16.4. Local Roads shall be designed to provide high levels of pedestrian amenity while providing adequate accommodating for cars and service vehicles. Map 11.2.B identifies a Main Street and Urban Square. It is intended that this road be the focus of the District Centre. This Main Street and Urban Square, which will provide direct access to a potential Transit Facility site, is planned as a 23.0 metre right-of-way to provide the opportunity for bus access and on-street parking.

### **11.2.17 Servicing Water, Wastewater and Stormwater Management Services**

- 11.2.17.1. Development within the District Centre shall be on the basis of full urban water, wastewater and stormwater management facilities.
- 11.2.17.2. A Master Servicing Strategy outlining preliminary functional water distribution, wastewater collection and stormwater management facilities, including designs and costs, will be prepared by proponents to the satisfaction of the City and the Region as a condition of approval of development applications.
- 11.2.17.3. The need for a variety of stormwater management ponds has been identified throughout the Centre. The precise operation, dimensions and design of the facilities will be determined and addressed as part of the Master Servicing Strategy. The design and function of the ponds will be in accordance with City policies. Every effort will be made to reduce the size of these ponds within the Centre.

- 11.2.17.4. Appropriate stormwater management practices shall be followed to the satisfaction of the City and the Toronto and Region Conservation Authority. Development shall provide for the management of stormwater runoff, and the promotion of water quality treatment on a comprehensive watershed basis. On-site storage of stormwater (e.g. parking lots and rooftop controls) will also be considered as an option for the treatment of stormwater within the District Centre.
- 11.2.17.5. New infrastructure or improvements to existing infrastructure, including roads, site grading, sanitary and stormwater management facilities, municipal water and electricity supplies, required to serve proposed development shall be provided concurrent with the rate of development, and shall not place a financial burden on the City.

## **11.2.18 Transportation General**

- 11.2.18.1. The intent of the transportation system is to provide an integrated, multi-modal transportation network for all residents and businesses that are safe, convenient, affordable, efficient and energy-conserving while minimizing environmental impacts.

### **Road Network**

- 11.2.18.2. The road network includes primary roads, local roads and laneways. Map 11.2.B identifies the road network. The right-of-way widths for the various roads are generally 23.0 metres for Primary Roads, 17.5 to 20.0 metres for Local Roads and 7.5 metres for Laneways. The following policies apply to the Road Network:
- a. The road allowance widths identified in this Plan are approximate. In certain cases, widening of existing roads to the ultimate road allowance width shown may not be required. In some cases the right-of-way may need to be wider to accommodate engineering and/or design requirements. Final right-of-way requirements and the provision of daylight triangles, and left turn lanes will be determined during the review of development applications;
  - b. Local road right-of-ways will range from 17.5 to 20.0 metres. Generally, local roads that serve primarily residential development shall be 17.5 metres;
  - c. Notwithstanding 12.1.14.8.b above, the north-south road identified as “Main Street” and the east-west road providing potential access to a transit facility shall have 23.0 metre and 20.0 metre rights-of-way, respectively, to accommodate buses and on-street parking;

- d. One road is identified as a 'Special Character Street' on Map 11.2.B. This road is an important pedestrian connection to open spaces and/or environmental features. It shall be designed to incorporate enhanced landscape features signifying their important contribution to the overall open space system. The enhanced landscape treatment may be incorporated within the boulevard or within a centre median, subject to the requirements of the City;
- e. It is a requirement of this Secondary Plan that all roads identified within the Centre shall be built and maintained to an operational standard satisfactory to the City, and shall provide for permanent public access for traffic through the Centre;
- f. The road network illustrated on Map 11.2.B is fundamental to the efficient functioning of the District Centre transportation network; laneways and local and primary road rights-of-way shall be identified in all approved plans within the Centre, and shall be conveyed to the municipality as a condition of approval of draft plan of subdivision, and at the time of plan registration. Alternatively, the City may in the subdivision agreement permit private roads designed and built to City standards with a public easement that conveys the road to the City at no cost at any time in the future that the City deems necessary;
- g. No plan shall be approved which permits the establishment of any structure on a local or primary road rights-of-way identified on Map 11.2.B. Local road allowances conveyed to the municipality may be used privately on a temporary or permanent basis for parking aisle purposes at the sole discretion of Council;
- h. The road network within the Centre shall recognize natural and built constraints and shall form a continuous and interconnected modified grid pattern, integrated with the road network planned or in place on surrounding lands bounding the Centre;
- i. Round-a-bouts, cul-de-sacs, hammerheads or other similar traffic calming and traffic control measures may be implemented subject to evaluation by the City and/or the Region of functional, operational and financial issues associated with their use;
- j. Two round-a-bouts, as identified on Map 11.2.B, shall be incorporated into the Centre to facilitate traffic movement at the intersections of the two north south primary roads and the northern most east-west primary road;
- k. Streetscape design for Arterial Roads, Primary Roads, and Local Roads shall be subject to the City's urban design requirements and policies articulated in this Secondary Plan;

- l. Individual direct access to any arterial road shall be discouraged. Building setbacks from arterial roads will be minimized and parking areas shall be directed to areas less visible from the street; and,
  - m. The decision to require the first east/west road north of Rutherford Road, in Block 11, will be examined in detail from a traffic and engineering perspective, at the Draft Plan of Subdivision stage, to the satisfaction of the City.
- 11.2.18.3. The Regional road structure (the Arterial Roads) of Rutherford Road and Dufferin Street will play an important role in the success of the Carrville Centre. A key component of making street related retail uses work, adjacent to the Arterial Roads, is the provision of lay-by parking. The City will work with York Region regarding the construction and detailed implementation of this feature in conjunction with the other streetscape improvements being recommended for the Arterial Roads. The detailed design of the Arterial Roads shall consider the incorporation of lay-by parking facilities.

### **Transit**

- 11.2.18.4. It is a major objective of the City to increase transit ridership. To achieve this objective, the transit network within the District Centre is encouraged to be integrated with local, regional and inter-regional transit routes and services, including:
- a. Connections to the local transit system along Arterial and Primary Roads;
  - b. Connections to the TTC Subway system via the Highway 7 corridor, Vaughan Corporate Centre (Spadina line) and Finch station (Yonge line); and,
  - c. Connections to the Maple and Rutherford GO rail stations at Major Mackenzie Drive and Rutherford Road and connections to commuter parking lots and GO bus operations along the Highway 400 corridor.
- 11.2.18.5. The City will take into account the requirements for a comprehensive and interconnected public transit network by ensuring that all development applications fully consider short and long-term transit opportunities.
- 11.2.18.6. The internal road network shall be continuous such that it is conducive to the efficient routing of public transit and provide the maximum possible service coverage.
- 11.2.18.7. The City shall encourage the increased use of public transit by requiring transit supportive urban design, retaining rights-of-way for off-street bus loops, terminals

and on-street bus bays as well as providing for bus shelters and bus stop locations. Improvements to the public transit network, based on the existing system of roads, should be initiated immediately and continue as necessary to meet the needs of development as it proceeds.

- 11.2.18.8. Local transit routes serving the surrounding community should converge within the District Centre, and transit stops/stations shall be established at appropriate locations.

### **Pedestrian and Bicycle System**

- 11.2.18.9. The development of all roads shall include safe, convenient and attractive facilities for pedestrians and cyclists.
- 11.2.18.10. A pedestrian walkway and bikeway system (developed primarily within the public road right-of-way) shall be required in the District Centre and integrated with the City's Pedestrian and Bicycle System Master Plan. Pedestrian and bicycle facilities will be designed to provide connections to bus stops, and linkages between buildings, adjacent sites, surrounding areas and public streets, particularly those with transit facilities.
- 11.2.18.11. An adequate supply of secure bicycle parking shall be provided near bus stops, in high activity areas and park areas.

### **Parking**

- 11.2.18.12. Sharing of parking in mixed-use developments will be encouraged within the District Centre, subject to evaluation by the City.
- 11.2.18.13. On-street parking shall be permitted on all streets fronting commercial development in the District Centre with the exception of Rutherford Road and Dufferin Street. Parking underground and in structures shall also be encouraged.
- 11.2.18.14. To assist with the reduction in large surface parking areas in the District Centre, a reduction to the current parking standards and maximum parking standards shall be applied to all surface and/or structured parking provided. The following parking standards will be used in calculating the required parking spaces for each development within the Carrville Centre:
  - a. For all retail commercial uses -a maximum of 4.25 spaces/100 square metres of Gross Floor Area;

- b. For all office uses -a maximum of 3.0 spaces/100 square metres of Gross Floor Area;
  - c. For all condominium-based residential uses – a maximum of 1.75 spaces per unit, inclusive of visitor parking; and,
  - d. For all freehold residential uses – 2.0 spaces per unit are required.
- 11.2.18.15. The City of Vaughan shall investigate the establishment of a public parking authority to provide public parking lots (surface lots and/or structured parking facilities) within the Carrville Centre. Should public parking be provided, the parking standards identified above shall be further reduced to balance the overall supply of parking with the permitted levels of development.
- 11.2.18.16. Cash-in-lieu of parking may be considered by the City. If considered, the minimum parking requirement shall be the benchmark for the calculation of required parking.
- 11.2.18.17. Parking requirements for any individual development do not necessarily need to be provided on the same lot, or on a lot contiguous to the development. Required parking for any development within the District Centre may be provided on any lot within 500 metres of the development that is being served by the parking facility, subject to approval by the City.
- 11.2.18.18. Notwithstanding 11.2.18.18 above, the approved plan for the major food store located on the west side of the Main Street and Urban Square shall fully accommodate all of the parking required for the food store use.

### **Traffic Demand Management**

- 11.2.18.19. The City will actively work with the Region and with developers, owners and tenants in the District Centre to develop, implement, facilitate and promote measures to increase the use of transit, cycling and walking, while reducing the use of low occupancy automobiles for trips, particularly trips to and from work. These measures may include:
- a. Promoting the use of public transit by employees;
  - b. Promoting measures to foster higher vehicle occupancy;
  - c. Assisting in organizing and promoting car pooling;
  - d. Giving priority parking space assignments and/or reduced rates for car pools;
  - e. Varying hours of work to reduce peak hour loads;
  - f. Participating in a Transportation Management Association; and,

- g. Giving priority parking space assignments or reduced rates for low and non-polluting motor vehicles such as electric cars, as they become available to the general market; and/or, other measures that may be identified.
- 11.2.18.20. Measures to support transit use, such as maximum parking standards, priority signaling, shared parking arrangements, and public-parking structures, may also be required.
- 11.2.18.21. Transportation Impact Studies submitted in support of development applications shall identify and assess the feasibility and impact of travel demand management measures. The recommendations of these studies shall include the implementation of a travel demand management program for the proposed development.
- 11.2.18.22. Development proposals for greater residential densities or for greater retail floor space than permitted by this Plan shall be required to provide a Traffic Impact Study and traffic certification by a qualified transportation consultant stating that the development meets with the following traffic criteria:
- a. The site layout provides adequately for the movement needs of pedestrians, automobiles and commercial vehicles without disrupting bordering streets and properties;
  - b. The development will not increase local residential road traffic so significantly as to produce appreciable new hazards, noise, dust and fumes for District Centre residents and nearby residential communities;
  - c. The project provides an appropriate level of parking and encourages the use of public transit, walking and cycling as alternatives to automobile use;
  - d. The traffic resulting from the proposed development does not significantly contribute to reducing the level of service of arterial roads and primary roads and their intersections to below a generally acceptable level; and,

### **Transportation Improvements**

- 11.2.18.23. The full achievement of the development proposed by the Carrville Centre Plan requires the following improvements to the transportation system servicing the Centre:
- a. The following Region of York road and transit improvements have been identified as necessary to support the full achievement of the development of the District Centre:
    - i. through the District Centre, construction of the redesigned Rutherford Road

- and Dufferin Street to include four through lanes;
  - ii. streetscaping improvements to Rutherford Road and Dufferin Street, including centre medians;
  - iii. a local transit network and associated transit infrastructure within the District Centre focused on the Transit Facility; and,
  - iv. a Transit Facility providing connections to the Highway 407 Transitway, transit links to the Spadina Subway and York University, and the GO rail commuter system, and local transit services focused on the Primary Roads.
- b. The following City of Vaughan road and transit improvements have been identified as necessary to support the full achievement of the development of the District Centre:
- i. the Primary and Local Road and Laneway network;
  - ii. the Main Street and Urban Square identified on Map 11.2.B;
  - iii. on-street and lay-by parking on all roads within the District Centre, in particular on both sides of the Main Street and Urban Square; and,
  - iv. a pedestrian and bicycle system linking to areas in the rest of the Carrville Community.

11.2.18.24. It is the policy of Council to assist the Region of York in protecting and obtaining lands required for the rights-of-way for the widening of Arterial Roads and the provision of public transit services through the development approval process.

11.2.18.25. Where lands have been identified as required for the construction of the road network within the Centre, and where such lands are the subject of a development application, it is the policy of Council to require the dedication of such lands before permitting the development of the site.

11.2.18.26. From time to time, at the discretion of Council, lands for planned road or transit improvements may be directly purchased or expropriated by the City, in order to foster the planned and orderly development of the Centre. It is Council's intention that the funds for such acquisition of land and for the construction of planned road or transit improvements be provided to the greatest extent practical through a charge against development in the Centre and/or the City and/or the Region under the provisions of the Development Charges Act, or by other means available to the City.

- 11.2.18.27. It is Council's intention that the development of the Centre proceed as expeditiously as possible, and that the planned transportation improvements should, therefore, be reflected in the capital works forecasts and any Development Charges By-law in such a fashion as to permit development without delay. As such, the City will urge other levels of government to proceed likewise in circumstances where the jurisdiction for a transportation network improvement is not with the City. The specific need and timing for these improvements will be determined based on the monitoring program set out in this Secondary Plan.
- 11.2.18.28. Until such time as the Regional Road improvements identified in this Secondary Plan have been undertaken, Council may phase development in the Centre until the required road capacity is available. Where the Arterial Roads have not been widened to the width set out in this Secondary Plan, development applications will be required to submit a traffic study to demonstrate that capacity is available.
- 11.2.18.29. The City, in partnership with the Region, shall establish a formal program to monitor the level of development and associated traffic conditions. The Monitoring Program will address:
- a. Traffic volumes on key routes and at key intersections, based on periodic traffic counts in the Centre area;
  - b. The amount of existing and proposed floor space for which subdivision, zoning or site plan approval has been granted, status of development approvals, completions and occupancy;
  - c. Travel characteristics and modal split;
  - d. Population and employment generated by existing development and projected for approved, but not yet occupied, development;
  - e. Evaluation of traffic volumes and transit ridership in the context of available capacity;
  - f. Evaluation of existing, planned and proposed development phases in order to allow for identification and planning of transportation improvements or to allow time for steps to be taken to control the pace of development; and,
  - g. The regular monitoring program will take place, as part of the periodic review of this Secondary Plan, at intervals of five years. Council will review the results of each cycle of the Monitoring Program at a public meeting. Sufficient detail will be made available to the public to enable the verification of results and conclusions.

### **11.2.19 Development Concept Report and Phasing Plan**

- 11.2.19.1. A Development Concept Report will be required for this Secondary Plan Area. In addition to the matters listed in policy 10.1.1.5. The Development Concept Report will discuss the achievement of the pedestrian oriented retail 'Main Street and Urban Square.' as identified on Map 11.2.A.
- 11.2.19.2. Within each quadrant of the Carrville Centre Secondary Plan development applications should co-ordinate neighbouring development proposals in a mutually complementary fashion. Non-participating lands shall be shown conceptually in the Development Concept Report and Phasing Plan.
- 11.2.19.3. Development shall be phased to provide for the orderly development of the Carrville Centre and to ensure the most efficient and economical use of existing and proposed infrastructure. The following phasing criteria shall be considered in the review of all development applications:
- a. The development contributes to, or can be appropriately integrated within, the logical sequence of construction of all required sewer, water, stormwater and transportation facilities.
  - b. The development satisfies all requirements regarding the provision of parkland and other facilities.
  - c. Traffic from the proposed development can be accommodated on the existing arterial road network.
  - d. Phasing may be addressed through the appropriate use of the holding (H) provisions of this Plan.
- 11.2.19.4. As a component of the Development Concept Report, development applications shall provide a Phasing Plan, which:
- a. Describes and illustrates how existing and proposed development can be incorporated into the site to achieve the full development potential of the site.
  - b. Identifies the public infrastructure and facilities required to serve the development, including water, sewer, stormwater, roads, transit, parks and open space facilities, and their proposed phased construction.
  - c. Describes the expected financial requirements for such public infrastructure, and the appropriate financial contributions from benefiting landowners.

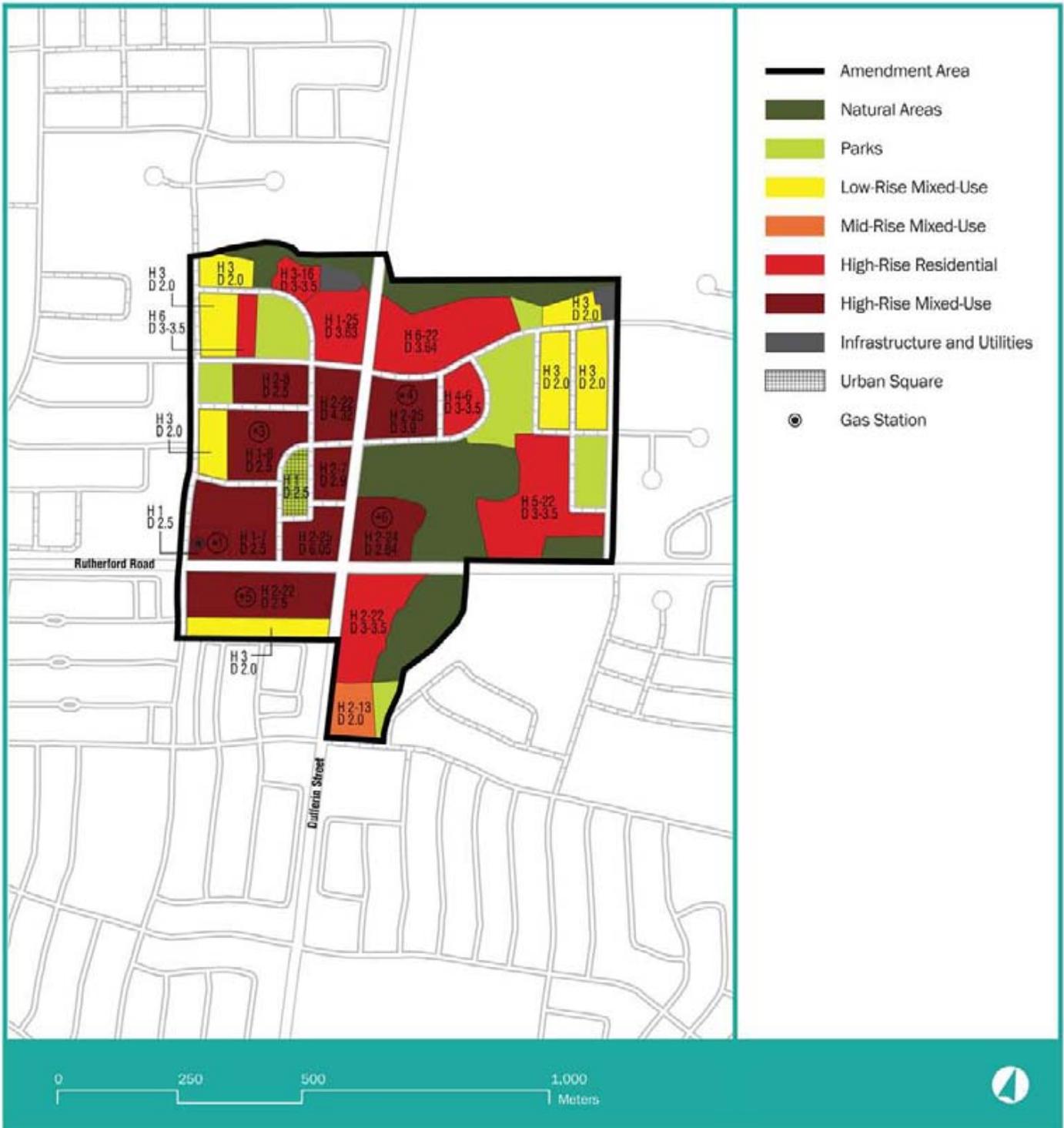
## **11.2.20 Community Improvement Policies**

- 11.2.20.1. The Carrville Centre is undeveloped. It is the intent of the City of Vaughan to facilitate the development of the Carrville Centre in conformity with the policies of this Plan. In order to achieve the planned function and intended image of the area as one of the primary urban centres of the City, it may be necessary for the City to implement a comprehensive public realm improvement strategy and/or to provide a financial incentive package to the private sector.
- 11.2.20.2. It is the intent of the City that the entire area of the Carrville Centre, which has been identified as a 'Community Improvement Area', may be further designated in whole or in part, by by-law, as a 'Community Improvement Project Area', for which a detailed Community Improvement Plan will be prepared.

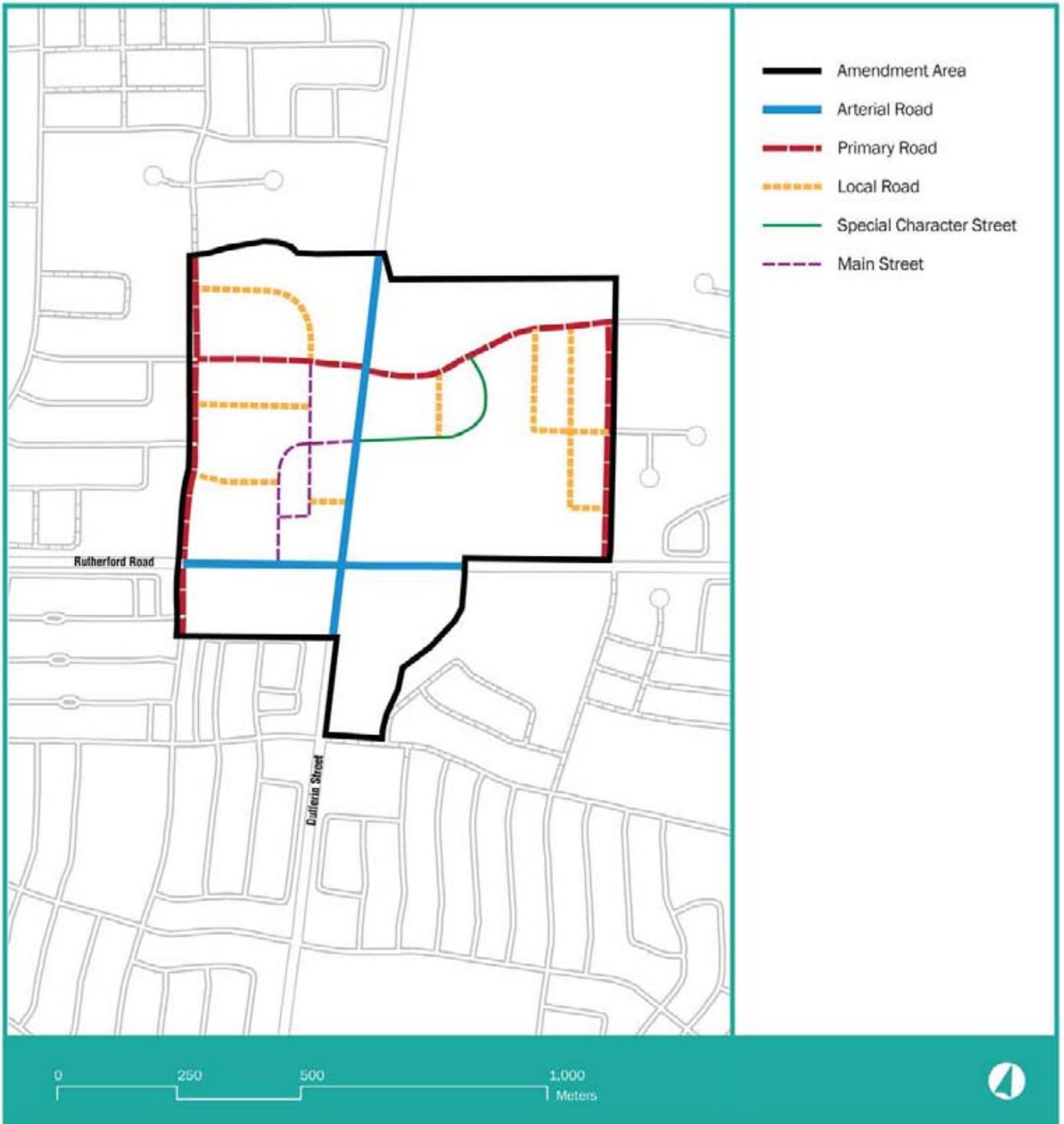
## **11.2.21 Interpretation**

- 11.2.21.1. This Secondary Plan is a statement of policy. It is intended to guide the development of the Carrville Centre Secondary Plan Area. Some flexibility in interpretation is permitted, provided the intent of the policies and principles of this Plan are maintained.
- 11.2.21.2. The designations identified on Map 11.2.A are intended to show general areas. Minor adjustments to the boundaries of the designations may occur through the City's approval process without Amendment to the Plan, except where the designations are established by fixed boundaries, such as road rights-of-way, or where specifically stated to be fixed in the policies of this Plan.
- 11.2.21.3. Where lists or examples of permitted uses are provided, they are intended to indicate the possible range and type of uses that are to be considered. Specific uses not listed, but considered by the City to be similar to the listed uses and to conform to the general intent of the applicable land use designation may be recognized as a permitted use and recognized in the implementing zoning by-law.
- 11.2.21.4. Minor variations from numerical requirements in the Plan may be permitted without an Official Plan Amendment provided that the general intent of the Plan is maintained. Minor variations, in this Secondary Plan, shall mean a maximum of 5 percent variation from the identified numerical requirement.

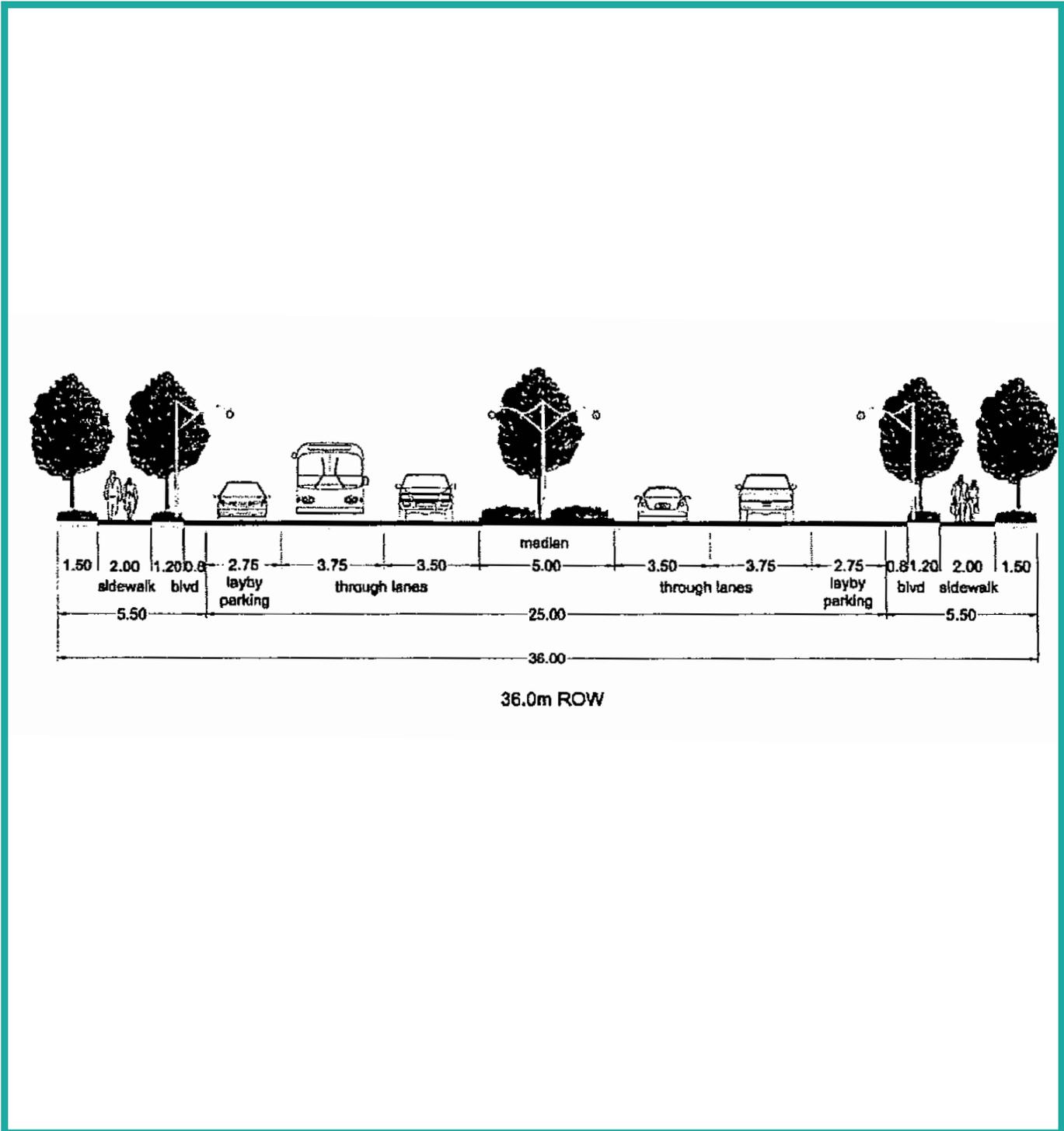
▼ Map 11.2.A:  
Carrville Centre Secondary Plan - Land Use



▼ Map 11.2.B:  
Carrville Centre Secondary Plan - Road Network



▼ Figure 11.2.A  
Arterial Road





## 11.3 Steeles West Secondary Plan

### 11.3.1 Introduction

This Secondary Plan recognizes that it will take a number of years for the Steeles Corridor: Jane to Keele (“the Corridor”) to reach its full potential. Therefore each phase of development will need to serve the Ultimate vision in terms of supporting the long-term goals and built-form objectives envisioned by this Secondary Plan.

It is vital that transit-supportive and pedestrian-oriented development be achieved at the outset. To ensure that this Secondary Plan can be implemented as envisioned, establishing the public road and block pattern of this Secondary Plan is important. Provision for the planned road network shall be a requirement of any approved development. As such, Map 11.3.B identifies the comprehensive public road pattern, including local streets, and, consequently, the pattern of development blocks.

Within the Corridor, the City’s objective is to establish a development block pattern based on a grid of new streets. The grid is comprised of three elements. The first is a continuous east-west primary road from Keele Street to Jane Street located within or at the southern edge of the Hydro Corridor. The second element is the structure of six north-south roads extending from the east-west primary road down to Steeles Avenue. The third will be an even finer network of local public roads to be developed within these blocks (as shown conceptually on Map 11.3.B).

The visual attractiveness and image of the Corridor is of prime importance, and development throughout the Corridor will be characterized by high quality urban design. Urban design requirements will be implemented through the policies of this Secondary Plan, the zoning by-law and through site plan control. Development related and scaled to the pedestrian is required on all lands within the Corridor.

- 11.3.1.1. The area within the heavy lines on Map 11.3.A shall be known as the Steeles Corridor: Jane to Keele, and is subject to the policies of this Section.

### 11.3.2 Community Structure

- 11.3.2.1. The planned Toronto-York Spadina Subway Extension will ultimately support enhanced service to this section of Steeles Avenue. To encourage the Corridor’s development as a public transit hub, a concentration of residents and workers to support and encourage this function will be essential. The Secondary Plan provides for higher densities around

the subway station site, which gradually lower with increased distance from the station. While Steeles Avenue and the municipal boundary define this area as a 'corridor', it is expected that the Secondary Plan Area will combine with York University to create a seamless development node of significance to the Greater Toronto Area.

### **Population and Employment**

- 11.3.2.2. The Secondary Plan Area is expected to accommodate approximately 5,000-5,500 residential units, and a residential population of approximately 10,000-11,000, at full build out. Approximately 100,000 - 120,000 square metres of Office/Commercial uses is planned, which would generate an estimated 4,000 - 5,000 employees.

### **11.3.3 Development Principles And Objectives**

- 11.3.3.1. The following principles express the fundamental premises for development within the Secondary Plan area. They will be considered in the review of all development applications and capital projects:
- a. A transit-supportive Plan: Support and capitalize on existing and planned transit investments.
    - i. To establish high density, mixed-use residential/commercial development that provides support to the existing bus transit and planned subway service.
    - ii. To establish a road and block structure that provides access for pedestrians, bicyclists, public transit and passenger vehicles.
    - iii. To develop a grid network of continuous and interconnected arterial, primary and local streets, and short development blocks, complemented by a public transit network, facilitating efficient movement by all modes.
    - iv. To establish, at as early a stage as possible, an east-west primary road connection between Jane Street and Keele Street to support bus and car movements and to provide relief to the Keele/Steeles and Jane/Steeles intersections.
    - v. To protect for transit-related facilities (including bus terminals, passenger pick-up and drop-off and station entrances) required for the Steeles West Station.
    - vi. To optimize the use of existing public lands within and surrounding the area for infrastructure and community amenities, thereby maximizing the potential of private lands for transit-supportive development.
  - b. A Plan to use infrastructure effectively: Manage development within the capacity of

existing and committed transportation and servicing infrastructure.

- i. To maximize land use densities within the capability of the infrastructure.
  - ii. To plan for infrastructure improvements and community facilities and services that will support further increases to permitted land use densities.
  - iii. To phase land uses so that they match with the implementation of the infrastructure improvements, meaning all public and/or private utilities (including but not exclusive of CATV, Hydro, Gas, Communications, Telecommunications, Canada Post, etc.), which are required to service a development.
  - iv. To require all development to proceed on the basis of full services, logically extended in keeping with the availability of servicing capacity, and designed and built at a scale which ensure that the ultimate forecast population and employment growth resulting from this Secondary Plan can be accommodated.
  - v. To ensure that the costs of private development are fully borne by, shared equitably among, and recovered from benefiting landowners.
  - vi. To plan well in advance, in cooperation with the Region of York, to ensure that infrastructure improvements at both the local and regional level are planned on a comprehensive basis, and built in time to address the needs of the Corridor, in concert with market demand.
- c. Create a compact, mixed-use precinct, concentrating worker and resident population within a 10-minute walking distance of the future Steeles West Station.
- i. To provide the opportunity for a full range of high density land uses.
  - ii. To support a mixing, rather than separation, of land uses.
  - iii. To provide an opportunity for the highest densities to be concentrated in the areas closest to the planned transit stops and station.
  - iv. To establish a concentration of residential and major office development close to the subway station that supports development of a vibrant urban area.
  - v. To provide for the achievement of a substantial commercial centre with market synergies among the various uses and which fosters increased public transit ridership.
  - vi. To provide retail development opportunities to address the needs of the employee and resident population in the Corridor, while avoiding creation of a retail centre serving broader City or Region wide needs.

- vii. To establish the mixed-use residential and commercial corridor as an integral component of a pedestrian-friendly urban environment which is conveniently accessible by public transit, bicycle, and on foot from York University and the surrounding community.
- viii. To ensure that the Corridor develops in a manner consistent with its 'planned function' in the City's hierarchy of transit-supportive centres and corridors.
- d. Complement and reinforce the academic function and campus environment of York University.
  - i. To provide for a range of land uses that support the continued evolution of York University.
  - ii. To recognize the core of the campus as the primary area for academic function, and provide support for other uses ancillary and complementary to that core academic function.
  - iii. To become a complementary component to the York University campus, rather than a separate corridor in another context.
  - iv. To make connections across Steeles Avenue and enhance pedestrian access between the north and south sides.
  - v. To visually connect land uses across Steeles Avenue from a built form and urban design perspective.
- e. Ensure early phases of new development establish the vision for the Corridor at the outset and provide the opportunity for further land use evolution.
  - i. To ensure that the first phase of development enables the City to acquire or secure the planned road rights-of-way which define the lot and block structure.
  - ii. To encourage and support as high a density as supported by this Secondary Plan at the earliest possible stage of development.
  - iii. To have all phases of development, including initial phases, reflect transit-supportive urban design.
  - iv. To require preparation of development phasing plans that provide for the logical progression of development from its initial phase to a mature phase reflecting the urban form envisioned by the Secondary Plan.
  - v. To ensure as early as possible, the development of the key public sector elements needed to achieve the vision for the Steeles Corridor Secondary Plan Area, including the establishment of:
    - A. public transit services, and associated infrastructure and facilities;

- B. a grid network of public road rights-of-way;
  - C. the continuous east-west primary road connection between Jane and Keele Streets;
  - D. a pedestrian promenade along Steeles Avenue with a high-quality streetscape;
  - E. appropriately sized and located park/neighbourhood square and school sites
- vi. To consider the compatibility of existing land uses in the early phases of new development.
  - vii. To provide an appropriate transition in scale from residential areas to surrounding employment areas.
  - viii. To protect against the displacement of employment from surrounding employment areas that are, in particular related to the CN Rail Yard and significant to the local, regional and national economy.
- f. Develop a consistent, attractive image and pedestrian environment along Steeles Avenue through landscaping and the placement and massing of buildings.
    - i. To promote the development of a mixed-use residential and commercial district focused along the Steeles Avenue frontage.
    - ii. To have development concentrated and massed along Steeles Avenue to create a physical presence and focus for the transit-riding pedestrian.
    - iii. To develop a walkable, pedestrian-friendly environment, with connections to the subway station, public facilities such as schools, parks, urban squares, and commercial areas.
    - iv. To encourage built form and streetscapes that support walking through a continuous, street-oriented urban area that is safe, attractive, and human in scale.
    - v. To encourage the grouping or clustering of utilities wherever possible in order to reduce or mitigate street clutter within the public or private realm. Wherever practical, the City encourages utilities to locate underground and/or within buildings.
  - g. Provide a balanced transportation system that is convenient, safe and comfortable for pedestrians, cyclists, public transit, and private vehicles.
    - i. To develop a pedestrian and bicycle network through the Corridor, connecting private amenity areas, parks, neighbourhood squares, public buildings, stormwater management areas, utility easements and, where

- appropriate, local streets, within and adjacent to the Corridor, and particularly to the subway station and York University.
- ii. To support on-street parking on the local north-south roads and east-west roads other than Steeles Avenue, to support street-related retail uses that are accessible to both pedestrians and drivers.
  - iii. To encourage the City of Toronto to consider, over the evolution of this Secondary Plan, the potential for on-street parking on Steeles Avenue.
  - iv. To encourage travel on foot and bicycle, by providing safe and convenient pedestrian and bike networks on public streets and connections to the subway station, parks, public facilities and publicly accessible private amenity areas.
  - v. To use appropriate building setbacks, lane widths, turning radii, etc. as traffic calming measures as appropriate, to reduce traffic speeds, enhance public safety, and create a more urban condition.
  - vi. To secure, acquire and/or protect for road rights-of-way and transit rights-of-way that make up the grid network of primary and local roads.
  - vii. To facilitate bus access to the subway station and automobile access to the passenger pick-up and drop-off and the commuter parking lot in the hydro corridor.
  - viii. To reduce the need for commuter travel by encouraging a closer relationship between the workplace and the home.
- h. Integrate transit facilities, roads and development as seamlessly as possible with surrounding development.
- i. To encourage a significant increase in public transit ridership to reduce traffic congestion, energy costs and pollution, and foster greater efficiency in the operation of the transportation system.
  - ii. To provide for a transition of land uses and density from the subway station to adjacent areas.
  - iii. To provide a network of north-south road rights-of-way connecting across to the south side of Steeles Avenue to York University that also facilitate pedestrian and bicycle crossings.
  - iv. To facilitate east-west movement through the corridor by pedestrians, cyclists and vehicles through the provision of linked, mid-block connections.
  - v. To continue to work closely with York Region, City of Toronto, York University and the various transit providers to implement the transit and road system

improvements complementing the development policies in this Secondary Plan.

- vi. To support additional longer-term road connections to areas beyond the Secondary Plan.
- i. Ensure future residential neighbourhoods are adequately served by community services and amenities such as schools, urban squares and parks, libraries, emergency services and recreational facilities.
  - i. To provide population and employment forecasts that support the planning and provision of community services.
  - ii. To provide opportunities for multi-use institutional and community facilities.
  - iii. To recognize the personal, social, economic and environmental benefits of recreation, urban parks and squares, and open spaces.
  - iv. To establish policies for securing public uses, urban parks and squares, and publicly accessible private amenity areas.
  - v. To create a convenient and continuous pedestrian and bicycle network throughout the Corridor, connecting to transit, parks, public facilities, and linking it to the surrounding community.
  - vi. To provide urban parks and squares that address the passive recreational needs of the anticipated households and employees within the Corridor.
  - vii. To encourage and assist institutional users and public agencies in securing suitable sites and space to enable them to serve the community's social, cultural, education, government, health care and religious needs.
  - viii. To ensure that the institutional uses are compatible with and complementary to neighbouring uses and the surrounding community, and are designed to facilitate the achievement of the urban design and streetscape objectives of the Secondary Plan.
  - ix. To encourage major institutional users to locate on sites conveniently accessible to pedestrians and via the public transit network.
- j. Maintain and support the primary function of the Hydro Corridor to transport electricity.
  - i. To continue to work with York Region Rapid Transit Plan, Ontario Realty Corporation and Hydro One on expediting the design details and approvals of the continuous east-west primary road right-of-way between Keele Street and Jane Street.
  - ii. To support a design of the east-west road right-of-way that protects the primary function of the Hydro Corridor.

- iii. To encourage as much of the east-west road right-of-way as is technically feasible to be located in the Hydro Corridor.
- iv. To protect the use of the Hydro Corridor for facilities that support the subway station and public uses but do not interfere with the primary function of the corridor.
- k. Encourage the urban design of future development to be of the highest quality.
  - i. To provide high quality development by:
    - A. encouraging and attracting private investment that will respond to the high order of public investment;
    - B. adding value to private lands served by the public realm;
    - C. encouraging a range and mix of high density residential, institutional and employment uses; and
    - D. establishing simple development “rules” in order to encourage, rather than discourage, development that meets the objectives of this Secondary Plan.
- l. Promote and demonstrate environmental sustainability.
  - i. To ensure a compact, mixed-use pattern of development and a network of streets and paths that encourage walking, cycling and transit use.
  - ii. To remediate contaminated land.
  - iii. To utilize best practices in watershed management.
  - iv. To achieve high levels of energy efficiency in buildings and utilities.
  - v. To encourage the use of renewable energy sources.
  - vi. To minimize the heat island effect from buildings and paved areas.
  - vii. To integrate stormwater management and water recycling facilities in the design of buildings, open spaces and parking areas.
  - viii. To protect the integrity of the Black Creek valley system.
  - ix. To support the development of district energy, heating and cooling systems.
  - x. To encourage “green” buildings designed to achieve high standards, for example, a high Leadership in Energy and Environmental Design (LEED) rating.
  - xi. To ensure trees line streets and populate parking areas, and are well maintained.

### 11.3.4 Land Use Designations And Densities – General

- 11.3.4.1. Map 11.3.A illustrates the precise physical boundaries of the Steeles Corridor - Jane to Keele Secondary Plan including the delineation of sub-areas based on their distance from the subway station.
- 11.3.4.2. The maximum heights and permitted densities, in terms of a floor space index, for the Secondary Plan Area are identified on Map 11.3.A and are based upon the following transition in scale from the subway station:
- |                       |                          |          |
|-----------------------|--------------------------|----------|
| a. Transit Core       | within 250 m of station  | FSI =4.0 |
| b. Transit Transition | 250 - 500 m from station | FSI =2.5 |
| c. Corridor           | 500m+ from station       | FSI =1.5 |
- 11.3.4.3. The floor space indices set out in Map 11.3.A are the targets to which future development should aspire. In accordance with Section 11.3.15, development applications will require conceptual Block and Phasing Plans that demonstrate how these floor space indices are to be achieved over time.
- 11.3.4.4. Notwithstanding the density and height policies of this Secondary Plan, the City may permit additional density and/or height under Section 37 of the Planning Act in accordance with policies 10.1.2.8-10.1.2.11.
- 11.3.4.5. Notwithstanding policy 9.2.1.1, the following uses are specifically permitted throughout the Secondary Plan Area:
- Residential units
  - Office uses
  - Research laboratory
  - Printing and publishing
  - Hotel
  - College or university
  - Commercial school
  - Financial institution
  - Data processing ancillary to a permitted use
  - Commercial uses in accordance with policy 11.3.4.6 below.

- 11.3.4.6. Commercial uses are to be provided at grade and only as part of a multi-storey, mixed-use residential, office, or institutional building. Retail commercial uses permitted on the ground floor of mixed-use buildings may include:
- a. Personal and business services
  - b. Business and professional offices
  - c. Retail stores
  - d. Restaurants
  - e. Places of entertainment
- 11.3.4.7. Public and Institutional uses are permitted in all land use designations without an amendment to this Secondary Plan. Public and Institutional uses include:
- a. parks
  - b. schools
  - c. community facilities such as community and civic centres, libraries, theatres, art and cultural centres, places of worship, daycare centres and day nurseries, police and ambulance stations and fire halls
- 11.3.4.8. The following buildings types are permitted in the Secondary Plan Area pursuant to the policies in Section 9.2.3:
- a. High-Rise buildings;
  - b. Mid-Rise buildings; and,
  - c. Public and Private Institutional buildings.
- 11.3.4.9. Non-transit-supportive land uses including: retail warehousing, service stations and gas bars, drive-through establishments of all types and uses, banquet halls, bowling alleys, motels, funeral homes, car brokerages, golf driving ranges, and miniature golf courses, are not permitted in this Secondary Plan.
- 11.3.4.10. Single use multi-unit residential, office, and institutional buildings may be permitted, but only as a component of a Development Concept Report and Phasing Plan as provided for in Section 11.3.15.
- 11.3.4.11. The ultimate location of the transit station(s) is yet to be confirmed and is the subject of an Environmental Assessment process. Any resulting major shift in this station location, or addition of a station in the Secondary Plan Area, will trigger a review of the location

of the designations in this Secondary Plan. Any minor shift in the transit station location may require a minor adjustment in the land use designations of this Secondary Plan which is permitted without the need for an amendment to the Secondary Plan.

- 11.3.4.12. The ORC/Hydro One corridor extending along the north side of this Secondary Plan is subject to the policies of the Parkway Belt West Plan. Within this corridor and in relation to the transit station exists the potential for a public parking facility that could accommodate up to 3,000 cars. This Secondary Plan permits the use of these lands for a parking facility in association with the transit station. However, there is also the potential for a large commuter parking facility to be provided in the Highway 407 corridor to serve a subway station there. The ultimate size of the parking facility in the hydro corridor should be determined based on a study of the projected demand for commuter parking associated with the subway station and the impacts of traffic from the future development anticipated by this Secondary Plan.
- 11.3.4.13. The area used for the calculation of permitted density shall include the land for the buildings, the land dedicated for the east-west road connecting Jane Street to Keele Street, private open spaces, landscaping, parking, servicing and driveway areas. The calculation of density shall not include the gross floor area of public transit uses, such as bus terminals and subway entrances, provided such uses are located within commercial or mixed-use buildings. In the event that more land is required for parks or surface stormwater facilities at-grade than is identified on Map 11.3.C then, without amendment to this Secondary Plan, consideration may be given to permitting the calculation of density to include the lands for such additional parks or stormwater ponds.
- 11.3.4.14. Development shall be undertaken on a comprehensive basis and provide an over-all design to achieve proper access, internal traffic circulation, adequate parking, and the desired urban design characteristics of the built form.
- 11.3.4.15. The needs of the transit rider and pedestrian shall be supported by providing street related commercial and public uses.
- 11.3.4.16. Public and private utility facilities shall be permitted in all land use designations and subject to detailed engineering designs to be approved by the City.

- 11.3.4.17. No outside storage of goods or materials shall be permitted in this Secondary Plan area.
- 11.3.4.18. The zoning by-law shall establish the applicable development standards.
- 11.3.4.19. This Secondary Plan establishes a framework for the designation of the Secondary Plan Area, or portions of it, for a Community Improvement Plan pursuant to policies 10.1.2.12 to 10.1.2.14 of Volume 1 of this Plan. The objective of the Community Improvement Plan will be to provide incentives for the development of transit-supportive land use densities.

### **11.3.5 Transit Core**

- 11.3.5.1. The purpose of the Transit Core area is to:
  - a. encourage and support high density residential and office uses to locate within 250 metres of the subway station and support the existing and planned transit system.
  - b. provide for the highest scale and density of residential and major office uses to surround the subway station and, if possible, be integrated with its development.
  - c. support the residential population and major office employees by providing opportunities for mixed-use development.
  - d. accommodate facilities that support the subway station and integrate them with development as efficiently as possible.
- 11.3.5.2. Office buildings are encouraged to locate at the subway station, on the corners of the Street 'C' and Steeles Avenue intersection as identified on Map 11.3.A.
- 11.3.5.3. Commercial uses oriented to public streets and plazas shall be provided on the ground floor of buildings fronting Steeles Avenue and New Street 'C' as identified on Map 11.3.A.
- 11.3.5.4. The maximum density in the Transit Core area shall be 4.0 FSI. The minimum density on a lot that is not a park shall be 2.0 FSI. Permitted density may be transferred from one lot to another within each of Block C and the portion of a Block D designated Transit Core, provided the minimum density of 2.0 FSI is achieved on all lots within Block C or the portion of the Block D, where the density transfer is occurring, and the total density on each of Block C and the portion of the Block D does not exceed 4.0 FSI.

- 11.3.5.5. Retail uses shall be ancillary to the main use and provided at grade, and only as part of a multi-storey, mixed-use high-density residential, office, or institutional building.

### **11.3.6 Transit Transition**

- 11.3.6.1. The purpose of the Transit Transition area is to:
- a. encourage and support high density residential uses and office uses to locate 250 metres to 500 metres from the subway station and support the existing and planned transit system.
  - b. provide for a transition in scale and density from the highest density uses surrounding the subway station to adjacent and surrounding areas.
  - c. support the residential population and employees by providing opportunities for mixed-use development.
- 11.3.6.2. The maximum density for residential, mixed-use and hotel developments in the Transit Transition area shall be 2.5 FSI. The minimum density for these uses on a lot that is not a park shall be 1.5 FSI. Permitted residential density may be transferred from one lot to another within each of Block B and the portions of Blocks D and E1 designated Transit Transition, provided the minimum density of 1.5 FSI is achieved on all lots within Block B or the portions of Blocks D and E1, where the density transfer is occurring, and the total density on each of Block B and the portions of Blocks D and E1 does not exceed 2.5 FSI. Densities for shall be in the range of 1.5 to 2.5 FSI.
- 11.3.6.3. The maximum density of office uses on any lot shall be 0.6 FSI.
- 11.3.6.4. Retail uses shall be ancillary to the main use and provided at grade, and only as part of a multi-storey, mixed-use high-density residential, office, or institutional building.

### **11.3.7 Corridor**

- 11.3.7.1. The purpose of the Corridor area is to:
- a. provide opportunities for high and medium density residential and office uses to locate more than 500 metres from the subway station and support the existing and planned transit system.
  - b. provide for a further transition in uses, scale and density from the highest density uses surrounding the subway station to adjacent and surrounding areas.
  - c. support the residential population and major office employees by providing opportunities for mixed-use development.

11.3.7.2. The maximum density for residential, office, mixed-use and hotel developments in the Corridor area shall be 1.6 FSI, except where the density bonus provided for in Policy 3.2.3.2(d) is utilized, in/which case the maximum density for all uses shall be 1.6 FSI plus 0.6 times the area of underground parking dedicated to non-residential uses, to a maximum of 1.75 FSI. Within Blocks A, E2, F and the portion of Block E1 designated Corridor the minimum density for these uses on a lot that is not a park shall be 0.8 FSI. Within Block G the minimum density for these uses on a lot that is not a park shall be defined by the Zoning By-law and may be less than 0.8 FSI. Permitted density may be transferred from one lot to another within each of Blocks A, E2, F and G and the portion of Block E1 designated Corridor, provided the minimum density is achieved on all lots within Blocks A, E2, F and G or the portion of Block E1, where the density transfer is occurring, and the total density on each of Blocks A, E2, F and G and the portion of Block E1 does not exceed 1.6 FSI.

11.3.7.3. On Block A and on Blocks E2, F and G combined, the maximum density of office uses shall be 0.6 times the area of the lot plus 0.6 times the area of underground parking dedicated to non-residential uses to be provided on the lot, to a maximum of 0.75 FSI.

11.3.7.4. Retail uses shall be ancillary to the main use and provided at grade, and only as part of a multi-storey, mixed-use high-density residential, office, or institutional building.

### **11.3.8 Policies For The Provision Of Public And Institutional Uses And Community Services**

11.3.8.1. The success of this Secondary Plan in reaching the population and employment targets and in supporting the major public investment in transit infrastructure also depends upon the ability to provide necessary community facilities and services.

11.3.8.2. The development of joint facilities for public and institutional uses will be strongly encouraged.

11.3.8.3. In the infill and redevelopment context of this Secondary Plan, the timing and phasing of new development, the mix of land uses, and the delivery of necessary community facilities and services are less predictable than with traditional 'greenfield' development. The City will assess the need for other community facilities and services to serve the Secondary Plan Area, based on the population and employment targets of this Secondary Plan. At various points of population and employment growth, the

delivery of services will need to be addressed through further study by the City and for Development Concept Reports and Phasing Plans submitted with Plans of Subdivision, as provided for in Section 11.3.15.

- 11.3.8.4. A monitoring, review, and report on the status of growth in the Secondary Plan Area and the delivery of services will be undertaken in conjunction with the monitoring policies in Section 10.1.1 of Volume 1 of this Plan.
- 11.3.8.5. Development of all Public or Institutional uses shall be in an urban form and scale that is sensitive to and supportive of the adjacent and highest density, transit-supportive land use designation and adhere to the Urban Design Policies contained in Section 11.3.10.
- 11.3.8.6. Without an amendment to this Secondary Plan, all Public and Institutional sites may be redesignated to permit an alternative use in accordance with the adjacent land use designations should they not be ultimately required for the original purpose.

### **Parks and Neighbourhood Squares**

- 11.3.8.7. Parks and public squares are to be provided in the Secondary Plan to provide central common spaces and key social gathering spaces for citizens. Public squares are smaller than typical neighbourhood parks, and are intended to address the passive recreation needs of residents and employees of the Secondary Plan Area.
- 11.3.8.8. A system of Parks shall be established in the Secondary Plan Area and integrated into the surrounding community's pedestrian and open space system. Map 11.3.C conceptually illustrates the general locations of Parks. The actual location, types and sizes of Parks will be determined by the City and may vary from Map 11.3.C without amendment to this Secondary Plan. Development Concept Reports shall identify the precise location and phasing of parks and public squares.
- 11.3.8.9. Parkland in this Secondary Plan area shall provide sufficient park space in the preferred locations to accommodate anticipated development and meet the needs of its future resident and worker population.
- 11.3.8.10. Suitably sized and located park and neighbourhood square sites shall be conveyed to the municipality as a condition of approval of draft plans of subdivision, to ensure

that the long term passive and active recreational needs of the Corridor residents and employees are accommodated.

- 11.3.8.11. Parks and public squares should be designed based on the following design guidelines:
- a. Parks and squares should be planned as focal points.
  - b. Streetscapes along the street right-of-way that abut a park should be designed to reinforce a high-quality, formalized relationship between the open space and its adjacent land uses.
  - c. Buildings should front onto the park to create built form edges to the public space.
  - d. The landscape along the street frontage should include high canopy street trees and be complementary on both sides of the street.
  - e. Entry/access points should be located conveniently and incorporate civic design themes.
  - f. Hard and soft landscape elements and features shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.
  - g. Parks and squares should be designed with a minimum of two (2) street or lane frontages.
  - h. Neighbourhood parks should be 0.8-2.5 hectares in size. Public squares will range in size from 0.4 to 0.8 hectares.
- 11.3.8.12. An urban plaza shall be located and provided in conjunction with the subway station. The plaza will support passive use of hard surfaces and include a high level of urban design detail and treatment.
- 11.3.8.13. The parks and public squares provided in this Secondary Plan area shall be shown on draft plans of subdivision, and shall be conveyed to the City at the time of plan registration.
- 11.3.8.14. A system of private open spaces should be provided to complement and interconnect parks.
- 11.3.8.15. Parks, private open spaces, and the public street system should be organized to connect to the Black Creek ravine system.
- 11.3.8.16. To complement the provision of public space to meet the needs of residents of the

Secondary Plan Area, Amenity Space will be provided. A percentage, to be identified in the zoning by-law, of the required Amenity Space shall be provided outdoors and on ground level to complement the public parks and open space system.

### **Schools**

- 11.3.8.17. The Secondary Plan area has been identified as a potential location for one (1) new elementary school. A location for the school is identified on Map 11.3.C. The location and site area for the school may change without amendment to this Secondary Plan.
- 11.3.8.18. In locating the school, the following criteria shall be considered:
- a. size and shape of the site
  - b. physical site conditions
  - c. the potential to establish itself as a focal point
  - d. accessible and visible from major pedestrian, transit and traffic routes
  - e. proximity to residential population
  - f. locations outside of the Transit Core and not fronting onto Steeles Avenue
  - g. the potential for future expansion.
- 11.3.8.19. The school may be a stand-alone building or part of a shared, mixed-use building or multi-use facility. Where the school is one part of a shared facility, the following issues in particular are to be considered:
- a. ready access to the public
  - b. self-contained with a separate entrance and services
  - c. a clearly visible and separate identity
  - d. a convenient service access which implements the urban design policies of this Secondary Plan.
- 11.3.8.20. The school site may be needed upon the population in the Corridor reaching a threshold of approximately 10,000 residents. The school boards will monitor population and student demand based on the land use approvals, Development Concept Reports, and Phasing Plans, and identify to the City in advance when, and where, a school site may need to be secured.
- 11.3.8.21. To provide an opportunity for a school facility to be developed as part of a mixed-use building, school boards will be circulated site-specific rezoning and site-plan applications.

## **Library**

- 11.3.8.22. The Secondary Plan area has been identified as a potential location for a new library.
- 11.3.8.23. The library could be of a “Neighbourhood Library” scale with floor space of approximately 560 square metres.
- 11.3.8.24. The selection of the library location should consider the following criteria:
- a. size and shape of the site
  - b. physical site conditions
  - c. the potential to establish itself as a focal point
  - d. accessible and visible from major pedestrian, transit and traffic routes
  - e. proximity to residential population
  - f. locations within the Transit Core and Transit Transition designations, and adjacent to Steeles Avenue are to be preferred
  - g. the potential for future expansion.
- 11.3.8.25. The library may be a stand-alone building or part of a shared, multi-use facility. Where the library is part of a shared multi-use facility, the following issues in particular are to be considered:
- a. ready access to the public
  - b. self-contained with a separate entrance and services
  - c. a clearly visible and separate identity
  - d. a convenient service access which implements the urban design policies of this Secondary Plan.
- 11.3.8.26. The library site may be needed upon the residential population reaching the planned population of approximately 10,000. The library board will monitor the population levels based on the land use approvals, Development Concept Reports, and Phasing Plans, and identify to the City in advance when, and where, a site or facility may need to be secured.

## **11.3.9 Noise And Vibration**

- 11.3.9.1. The purpose of this Secondary Plan is to:
- a. manage the evolution of land use in this area from the context of a low-intensity industrial employment area to that of a high density urban community with a mix

of residential and office uses served by high order transit. The most predominant noise source in relation to this Secondary Plan Area is the CN rail corridor and MacMillan Freight Classification Yard which are located north of the Hydro Corridor and Secondary Plan Area, The long-term protection of the CN Rail Yard function is important to the employment land base and economy of Vaughan; it is however, only one component of the City's overall growth management framework. While industrial and rail yard-related uses have been compatible with the historical function of the area, this function is changing, as expressed through the long-term vision and policies in the York Region Official Plan and The Vaughan Metropolitan Centre Secondary Plan. To address noise and vibration issues, this Secondary Plan requires identification of the limits and levels of the CN Rail Yard noise sources and mitigation against any adverse effect resulting from noise and vibration so that changes to accommodate sensitive land uses envisioned by this Secondary Plan can be incorporated into development.

- b. mitigate the impacts of noise and vibration from the subway and bus traffic.
- c. mitigate the noise and vibration impacts of truck traffic.

11.3.9.2. All residential development and other sensitive land uses within the Secondary Plan Area are required to undertake a noise and vibration study to the satisfaction of the City and CN Rail, to support the feasibility of the development: if feasible, the development proponent shall undertake appropriate measures to mitigate any adverse effects from the identified noise and vibration.

11.3.9.3. Prior to the development of sensitive land uses, including the residential land uses provided for by this Secondary Plan, a comprehensive City-initiated noise and vibration study, or in lieu of that, site specific noise and vibration studies in accordance with Ministry of the Environment Guidelines and in consultation with CN, will be required. This study will identify the CN Rail Freight Classification Yard and other noise sources, and control and mitigation measures, through enhanced building designs or acoustical building construction measures, for example, for development within 300 metres of CN facilities.

11.3.9.4. A portion of the sensitive residential land uses permitted by this Secondary Plan are planned to locate along the Steeles Avenue frontage. Prior to development approvals, Development Concept Reports and Phasing Plans, as provided for in Section 11.3.15,

are to address any potential phasing and integration of sensitive land uses with other non-sensitive land uses.

- 11.3.9.5. All proposed development adjacent to railways shall ensure that appropriate safety measures such as setbacks, berms and security fencing are provided, to the satisfaction of the City, in consultation with CN.
- 11.3.9.6. The City may require, development applicants to undertake, noise and vibration studies, prepared by qualified consultants, which address the potential impacts on development of the subway and associated bus terminal.
- 11.3.9.7. Where noise and vibration from development, including the development and operation of transit facilities, may have an adverse impact on Black Creek Pioneer Village, the City shall require the submission of noise and vibration studies prepared by qualified consultants and the implementation of appropriate mitigation measures.

### **11.3.10 Urban Design Policies**

- 11.3.10.1. In addition to the policies of this Secondary Plan, the City may require, with development applications, the submission of detailed Urban Design Guidelines and a Streetscape and Open Space Master Plan prepared to the satisfaction of the City, to further illustrate and articulate the policies.
- 11.3.10.2. To provide flexibility in the design process, other comparable design arrangements that achieve the principles and objectives of this Secondary Plan, and are satisfactory to the City, may be utilized without amendment to this Secondary Plan.
- 11.3.10.3. Any Development Concept Reports or Phasing Plans that are required under Section 11.3.15 of this Secondary Plan are to illustrate how the policies of this Secondary Plan and associated guidelines are being addressed and can be addressed in both current plans and future phases of development.
- 11.3.10.4. Site Plan applications within the lands subject to this Secondary Plan will be reviewed taking into consideration matters relating to exterior design, including the character, scale, appearance and design features of buildings and their sustainable design.

## **Built Form**

- 11.3.10.5. Buildings are to:
- a. define street edges, public spaces and, intersections;
  - b. face and have their main entrances onto a public street and sidewalk;
  - c. have their main entrance facing Steeles (where the buildings are located on Steeles Avenue);
  - d. be massed at an appropriate scale to ensure good sunlight, sky views, and wind conditions in streets, parks and open spaces, providing stepbacks as appropriate;
  - e. be sited and organized to achieve a harmonious relationship to the planned built form context through building massing and setbacks, roofline, profile, and scale;
  - f. be designed, where through-lots are provided, so that all elevations facing a street present active front elevations and fenestration;
  - g. address the street such that a continuous building façade along the street frontage and at corners is created;
  - h. be generally sited parallel to the public street and along the edges of parks and open spaces;
  - i. maximize ground floor coverage;
  - j. be sited and organized at-grade to enhance the public nature of streets, open spaces, and pedestrian routes, and so provide convenient access for pedestrians to public transit;
  - k. have their main entrance oriented and connected directly to the public sidewalk, flush with grade and where possible, located close to on-street parking; this applies in particular for each commercial use located at grade;
  - l. each building shall have its own lobby and entrance adjacent to the street;
  - m. be sited and organized so that principal windows and walls are separated to ensure adequate light, view and privacy; light, view and privacy setbacks should be proposed, to regulate the design of building facing conditions; and
  - n. overlook all streets, parks and publicly accessible open spaces with active building faces, in order to provide “eyes-on-the-street”.
- 11.3.10.6. All commercial and residential buildings shall have windows fronting onto the public street system. In recognition of the role that windows play in providing a visual connection between the building and the outside environment, it is important that appropriate glazing materials be used. Glazing materials shall generally allow for the transmission of light and views between the outside and inside of the building, and shall not be opaque.

- 11.3.10.7. The built form of development in the vicinity of Black Creek Pioneer Village shall be sensitive to its “sense of place” and views from the Village. The City will require view studies from strategic locations within the Village to determine if measures (in addition to the 6-storey height limit), such as setbacks, angular planes and landscaping, should be applied to development.

### **Heights, Setbacks, Build-to Lines and Built Frontage**

- 11.3.10.8. The following policies apply respecting the height of buildings:
- a. Maximum building heights shall be established in the zoning by-law and shall not exceed the height limits identified on Map 11.3.A.
  - b. Where additional levels of underground parking are provided for office uses beyond the one level required by Policy, 11.3.11.17.k, the maximum permitted height of the building containing the office use shall be increased by one storey for each additional level of underground parking.
  - c. The zoning by-law may establish maximum heights lower than those identified on Map 11.3.A for buildings or portions of buildings adjacent to parks and streets to address impacts related to shadows, wind and sky views.
  - d. The tallest buildings should be located on the corners closest to the subway station, and fronting onto Steeles Avenue.
  - e. Buildings fronting Steeles Avenue shall be a minimum of 13 metres in height (4 storeys).
  - f. Buildings greater than 6 storeys shall have a 3-6 storey podium.
- 11.3.10.9. The following policies apply respecting Setback Areas:
- a. The area between the building wall and the public right-of-way is to serve as the transition zone between the public and private realms, and constitute an important social and visual element of the street image.
  - b. In the case of residential development, the semi-public space creates a “buffer zone” between the public and the private domains, which enhances the visual appearance of the street edge and provides outdoor spaces for casual social interaction. Planted and constructed elements in the setback - low hedges, trees, masonry and decorative metal fences and gates - should be designed to provide a transition from the public sidewalk to the finished floor level of adjacent residences.
  - c. In the case of mixed-use retail/residential or retail/office buildings, the space between buildings and the street shall act as a connecting link between the public

realm and the private realm, and provide for generous pedestrian amenity space, and may include benches and outdoor cafes.

- d. Driveways and parking spaces are not permitted between buildings and the street.
- e. The minimum setback for residential buildings fronting the east-west road shall be 5 metres; the minimum setback for non-residential buildings fronting the east-west Road shall be 3 metres.

11.3.10.10. The following policies apply respecting Build-to Lines:

- a. In addition to providing a 'connecting link', a consistent building setback will be provided to give a sense of definition and containment to the street.
- b. Build-to lines shall be established in the zoning by-law for all buildings along Steeles Avenue and the north-south local roads.
- c. Build-to lines require buildings to locate at the build-to line by setting both a minimum and maximum setback.
- d. Buildings fronting Steeles Avenue, east of Murray Ross Parkway, and buildings fronting the north-south local roads shall have a build-to line a minimum of 3 metres and a maximum of 6 metres from the property line, or an equivalent identified distance from the curb of the travelled lanes of the street on which the building fronts, as illustrated in Figure 11.3.A.
- e. Buildings fronting Steeles Avenue, west of Murray Ross Parkway, shall have a build-to line a minimum of 9 metres and a maximum of 12 metres from the property line, or an equivalent identified distance from the curb of the travelled lanes of the street on which the building fronts.

11.3.10.11. The following policies apply respecting Minimum Built Frontage:

- a. A street wall or the part of the building that is closest to the public street provides important spatial definition and a sense of enclosure for that street. This is critically important along the pedestrian and transit oriented corridors of Steeles Avenue and the local north-south streets.
- b. The minimum built frontage requires a portion of each building façade and ultimately of each entire property to be located within the build-to line zone and will be further implemented through the zoning by-law.
- c. At least half (50%) of the property frontage along Steeles Avenue and the north-south streets shall have a building setback within the build-to zone.

## **Building Massing**

- 11.3.10.12. Angular planes, which require upper storeys of buildings to be set back further than the maximum setback or build-to line, may be established in the zoning by-law to ensure that direct sunlight reaches sidewalks and open spaces at most times of the year. Rear angular planes may also be established to provide a transition and mitigate impacts between high-rise and low-rise development.
- 11.3.10.13. In order that appropriate spacing is achieved between buildings on the same block, minimum light, view and privacy setbacks shall be established in the zoning by-law.
- 11.3.10.14. The dimension of new buildings generally should be limited to fifty (50) metres in either direction, in order to encourage street-related development with open courtyards in the center of the block, to reinforce pedestrian routes, mews and other forms of public open space, and to provide a fine grain of development. The façades of long buildings should be articulated with vertical recesses and architectural treatments for visual interest and to support a fine grain of development.
- 11.3.10.15. The advantages of better visibility, light and view, make corner sites good locations for landmark buildings. Corner sites are consequently often occupied by prestigious or civic buildings. Corner sites and intersections will play particularly significant roles in defining landmarks and activity nodes and in making improved connections at signalized intersections of Steeles Avenue across to York University. It is important that the treatment of corner sites be carefully considered throughout the Secondary Plan.
- a. Corner buildings should enhance intersections with well-articulated and prominent building facades that face both Steeles Avenue and the north south streets and which are visually different from adjacent development.
  - b. Corner buildings are ideal locations to meet the maximum density, height and massing provisions of this Secondary Plan; in the Transit Core and along Steeles Avenue, generally they should not be less than 6 storeys.
  - c. Corner buildings shall have their entrances at the corner.
- 11.3.10.16. Mechanical penthouses shall be integrated into the design of the building. Tall buildings should include “sculpted” roof tops to avoid box-shaped building forms and create visual interest. An interesting architectural feature/treatment shall be added to all rooftops of all buildings of eight storeys or more to avoid box-shaped building forms. Variation

and articulation in the building mass including horizontal and vertical setbacks, such as setbacks at the upper storeys, shall be established in the implementing zoning by-law.

### **11.3.11 Pedestrian Realm**

- 11.3.11.1. The grid of local streets and associated public or private open spaces shall provide the framework for the development of pedestrian realm in the Corridor.
- 11.3.11.2. The pedestrian realm as provided through streetscaping within the right-of-way is conceptually identified in Map 11.3.C.
- 11.3.11.3. Sidewalks on both sides of all streets shall form a connected system of optional routes to provide pedestrians access to the subway station.
- 11.3.11.4. The Steeles Avenue streetscape along with key buildings or facilities shall be designed to signal the importance of this urban corridor for pedestrians.
- 11.3.11.5. Parking, servicing, mechanical equipment and automobile drop offs are to be located in a manner that has a minimal physical impact on public sidewalks and accessible open spaces. Shared driveways and service courts at the side and rear of buildings are to be provided for these functions. Parking ramps to structured parking should occur within the building.
- 11.3.11.6. To promote the comfortable pedestrian use of streets. parks and open spaces, development shall provide:
  - a. well-designed, coordinated streetscape improvements, including trees, pedestrian-scale lighting, special paving and street furniture on sidewalks, boulevards and important pedestrian and publicly accessible open spaces;
  - b. appropriate landscape treatments, including trees and lighting, throughout parking lots and along their edges, in order to improve the appearance of the lots and along the edges, contribute to the visual continuity of the street edge, mitigate the heat island effect, and encourage the safe use of these spaces;
  - c. high-quality, usable open spaces which are physically and visually linked to streets, parks and mid-block pedestrian routes;
  - d. appropriate pedestrian weather protection at the transit station, along Street 'C', and in the Transit Core generally;

- e. buildings with primary entrances, transparent windows and signage facing the street; and,
- f. barrier-free design of buildings, streets and publicly accessible open spaces.

### **Courtyards**

- 11.3.11.7. Courtyards will be formed in many of the commercial, residential and mixed-use blocks. Their principal role will be to provide on-site, at-grade open space amenity for occupants of that block. The functional and spatial characteristics of the courtyard vary depending on building typology and size of the block.
- 11.3.11.8. Development applications and/or Development Concept Reports will be expected to present coordinated design concepts for courtyards that provide appropriate grading, pedestrian and landscape facilities, and coordinated servicing and automobile access to maximize usable landscaped open space.
- 11.3.11.9. To be most effective, courtyards should have a unified landscape design to ensure that gardens and play areas on individual sites can be shared with those living or working on the block as a whole.

### **Grade-related Uses**

- 11.3.11.10. In the entire Corridor, the provision of community services, restaurants, cafes, stores and display windows, etc. at grade are encouraged, as they provide visual interest, support the use of sidewalks and transit, promote retail continuity and viability, and contribute to a safer and more vibrant pedestrian and transit-supportive environment.
- 11.3.11.11. Buildings in the Transit Core and Transit Transition areas generally shall contain active uses along most of their street frontages, such as shops, restaurants, personal and business services, professional offices, front lobbies, and meeting, recreation, or activity rooms.
- 11.3.11.12. Ground-floor uses will over time change to adapt to a variety of urban community needs; as a result, the floor-to-ceiling height of ground floors for all buildings on Steeles Avenue and on Street 'C' in the Transit Core and other main north-south streets, should be a minimum of 4-5 metres.

- 11.3.11.13. The entrances to ground-floor commercial uses shall be flush with the public sidewalk.

### **Mid-Block Connections**

- 11.3.11.14. The provision of public, or publicly accessible privately-owned spaces are encouraged on individual sites in order to complement the public open space system. These will be provided on an incremental and coordinated basis as development occurs.

- 11.3.11.15. Development on each block identified on Map 11.3.A shall include continuous mid-block local streets, lanes, and/or pedestrian connections running east-west and north-south;

- 11.3.11.16. Other mid-block connections are encouraged that:
- a. are wide enough to safely serve the designed function;
  - b. are designed as pedestrian landscaped mews, and lit, landscaped and maintained for public use;
  - c. provide a fine grain of pedestrian circulation and an important connection between streets; and,
  - d. lead to the subway station and other public destinations such as the open space valley system to the west.

### **Parking, Service Entrances, Loading Areas, Utilities**

- 11.3.11.17. In order to reinforce streets as primary public spaces, the location of parking and service entrances need to be carefully considered.
- a. Parking and service entrances should have the least possible impact on the streetscape and public open spaces.
  - b. No parking, driveways, lanes, aisles or loading areas will be permitted between buildings and the public sidewalk along Steeles Avenue and the north-south local roads.
  - c. Large surface parking areas are generally discouraged.
  - d. Surface parking areas shall generally be located behind buildings fronting onto Steeles Avenue.
  - e. Limited side yard parking may be permitted.
  - f. Surface parking should be set back 3 metres from the property line or behind the building line along the local north-south road.
  - g. Setback areas should be substantially landscaped in order to continue to define the street edge and provide safety and amenity for pedestrians.

- h. Where surface parking is provided beside buildings located on Steeles Avenue and the north-south local roads, low walls and landscaping should be used to continue the visual street wall along the right-of-way.
- i. Where surface parking is provided, the visual impact shall be mitigated with significant landscaping and pavement treatments including landscape materials, trees and lighting throughout parking lots and along the edges. Attractive fences and generous landscaping between parking and adjacent residential uses should be provided.
- j. Parking is encouraged to be located below grade or in above-grade structures in particular where density is highest around the subway station.
- k. Office uses shall include a minimum of one level of underground parking.
- l. All parking for residents in apartment buildings shall be provided underground.
- m. Entrances to below grade or structured parking and service areas should occur within the building.
- n. Structured or below grade parking should be considered in the context of Development Concept Reports and Phasing Plans in terms of shared parking, parking ramps and loading bays.
- o. Above-grade parking garages in the Transit Core should be faced with active uses on the street, and all parking structures should be designed as buildings, with architectural treatments in keeping with design of adjacent buildings.
- p. Service entrances are not permitted on Steeles Avenue but should occur off of the local north-south streets, mid-block local streets or service lanes to the side or rear of buildings.
- q. Service entrances should be consolidated with adjacent development wherever possible.
- r. Loading areas are not permitted in any yard facing a street, unless they can be adequately screened from view, to the satisfaction of the City. The location of loading areas will be controlled in the zoning bylaw.

11.3.11.18. The following shall apply to utilities:

- a. When determining block layout, consideration should be given to the location of utilities away from and not within view of the public right-of-way.
- b. Utilities should be clustered to minimize visual impact, and located on or within buildings where possible.
- c. Utilities should generally be located within buildings or on the side of buildings and

where they are easily accessible by the utility service provider.

- d. Utilities shall not visually detract from the building treatment.
- e. Large, above-ground utility infrastructure should be located and designed to be compatible with its environment and innovative methods of containing utility services on, or within streetscape features, such as gateways, lamp posts, or transit shelters, should be considered when determining appropriate locations for large utility equipment and utility cluster sites.

### **Signage**

- 11.3.11.19. An area-specific sign by-law shall address the following:
  - a. Signage should provide a high level of clarity, visibility and visual interest with minimal visual clutter, and complement the building architecture in its scale, materials, consistency and design,
  - b. Building signage should be top-lit, or lit-letter; back-lit box signage is prohibited.
  - c. Pylon signage is prohibited; signage shall be incorporated onto the building.
  - d. Signage lighting should adhere to Vaughan's draft Exterior Lighting Guidelines.

### **Lighting**

- 11.3.11.20. The design and provision of lighting should adhere to the policies set out in the draft "City of Vaughan Exterior Lighting Guidelines" July 30, 2004 (or any other lighting policy document that supersedes this), with reference to the Local City Centre-Lighting District, Light Zones 1-4 and Lighting Quality Classes 3, 5 and 6.
- 11.3.11.21. The impacts of lighting from new development, particularly from commercial uses and parking lots, on adjacent properties and on Black Creek Pioneer Village in particular, shall be minimized.

### **Streetscaping**

- 11.3.11.22. High quality streetscaping is fundamental to the creation of attractive, pedestrian-friendly streets. The Secondary Plan Area is envisioned as an intensively developed urban place with a high concentration of residents and employees. To ensure that it can fully achieve the intended vision, a cohesive approach to the treatment of the streetscapes on all streets is crucial.
- 11.3.11.23. Steeles Avenue will play a particularly important role as the primary 'face' of

development, and will be subject to special treatment on both sides. Steeles Avenue is identified in the City of Toronto's draft Streetscape Manual as a Green Street/Scenic Street. To complement the intended treatment on the south side, a similar level of high quality streetscape treatment is required along the north side of Steeles Avenue, and in particular, on the frontage facing Black Creek Pioneer Village, as well as along Jane Street.

- 11.3.11.24. Other roads within the Secondary Plan Area also have important roles to play. The new major east-west road will become a major thoroughfare for private autos and transit vehicles. The parallel local east-west road will facilitate pedestrian and bicycle traffic within the Secondary Plan Area. The north-south roads will provide connecting links from the Secondary Plan Area to and across Steeles Avenue. The important functions of each of these roads will be facilitated and enhanced by appropriate high quality streetscape treatment.
- 11.3.11.25. The following streetscaping policies shall apply:
- a. Public realm conditions should support year-round and day and night activities.
  - b. Streetscape improvements, linked by a common theme and selection of materials, shall be made to both sides of each street to enhance the identity of place, pedestrian comfort and safety. A detailed streetscape plan will need to be developed in consultation with the City of Toronto and York University. Streetscape improvements shall include:
    - i. Paving and soft landscaping;
    - ii. Street furniture;
    - iii. Pedestrian-scale and street lighting; and
    - iv. Signage.
  - c. Mature trees within the public and private realm shall be preserved wherever possible. A tree preservation plan must be submitted with all development or redevelopment applications.
  - d. Soft landscaping shall be maximized and hard surfacing minimized, with the exception of well-designed and planted plazas, forecourts, patios, and streetscapes associated with Steeles Avenue.
  - e. Public spaces shall be designed to be inviting to the public, well lit, of a comfortable pedestrian scale, and complementary to the architecture of adjacent buildings.

- f. High-quality public realm elements such as railings, pedestrian lighting and tree pits shall be provided; and
- g. The pedestrian environment and connections both in the public realm (e.g., streets and sidewalks) and within the private realm shall be improved in the following ways:
  - i. Provide for public safety and comfort through a co-ordinated sidewalk and street furniture program, including adequate lighting (with reference to the City of Vaughan Draft Outdoor Lighting Design Guidelines), seating, wind and rain protection, bicycle parking, and enhanced and identifiable pedestrian crossing points;
  - ii. Provide additional planting of street trees and plantings on private lands;
  - iii. Ensure proper maintenance of public and private walkways; and
  - iv. Provide secure bicycle parking in commercial, public open space and public transit areas.

### 11.3.12 Transportation Policies

- 11.3.12.1. The intent of the transportation system is to provide an integrated, multi-modal transportation network for all residents and businesses that is safe, convenient, affordable, efficient, energy conserving, and minimizing in environmental impacts.
- 11.3.12.2. The arterial, primary and local right-of-ways in the Corridor shall form a connected system to serve all parts of the Corridor and allow pedestrians, public transit and private vehicles a multiple choice of routes.
- 11.3.12.3. The early establishment of the east-west primary road is critical to the implementation of this Secondary Plan.
- 11.3.12.4. The establishment of the TTC Spadina Subway Extension from Downsview Station through the Steeles Corridor is important to the ultimate achievement of the land use vision.

#### Transit

- 11.3.12.5. The Region of York has acquired the property on Schedule B at the northwest corner of Street C and Steeles Avenue, for the development of a transit terminal. Within the ORC/Hydro One corridor that extends along the north side of this plan exists the

potential for a parking facility that could accommodate up to 3,000 cars. York University is already a significant transit node served by many transit services and it is expected that the Corridor land uses will intensify as the transit service levels increase, and the road network is fully developed. The land uses provided for in this Secondary Plan are premised upon and support the existing and planned road and transit network.

- 11.3.12.6. The preferred location for the Steeles West Station has been identified through the EA process. An enlargement or minor shift in the transit station location and a corresponding adjustment in the land use designations of this Secondary Plan may occur without an amendment to the Secondary Plan.
- 11.3.12.7. It is a major objective of the City to increase transit ridership and the transit modal split within the entire City of Vaughan and, in particular within this Secondary Plan Area. To achieve this objective, the transit network within the Corridor will provide opportunities for regional and inter-regional transit routes and facilities, including connections to:
- a. the TTC subway and bus network, including potentially bus rapid transit facilities as a precursor to the subway extension;
  - b. the Vaughan Metropolitan Centre and the York Region Rapid Transit corridor in the Highway 7 corridor to the north, in the initial phase via prioritized 'Quick Start' bus service, evolving to a dedicated bus rapid transit route, and ultimately, to a subway.
  - c. the proposed Transitway stations within the Highway 407 corridor;
  - d. existing, proposed and potential GO rail station sites and GO bus stops; and,
  - e. to the local transit system along Steeles Avenue, Keele Street, and Jane Street.
- 11.3.12.8. The City will take into account the requirements for a comprehensive and interconnected public transit network by ensuring that all development applications fully consider short and long-term transit opportunities.
- 11.3.12.9. The internal road network shall be continuous such that it is conducive to the efficient routing of public transit and provide the maximum possible service coverage.
- 11.3.12.10. The City shall encourage the increased use of public transit by requiring transit supportive urban design, retaining rights-of-way for off-street bus loops, as well as providing for bus shelters and bus stop locations. Improvements to the public transit

network, based on the existing system of roads, should be initiated immediately, and continue as necessary to meet the needs of development as it proceeds.

- 11.3.12.11. Inter-regional, regional and local transit routes should converge in a coordinated manner within the Corridor, and transit stops shall be established at appropriate locations.
- 11.3.12.12. To promote transit use, optimize land use, create comfortable pedestrian environments and achieve other urban design objectives of this Secondary Plan, the integration of transit facilities with development is strongly encouraged. Direct connections between the subway station and adjacent development should be provided where feasible. Below-grade and surface transit facilities should be designed to permit commercial and/or residential development on the same site.

### **Road network and road allowances**

- 11.3.12.13. The grid network of local and primary road rights-of-way illustrated on Map 11.3.B is fundamental to the efficient functioning of the Corridor transportation network. Minor modifications to the alignment of primary roads on Map 11.3.B are permitted without amendment to this Secondary Plan, provided the principle of a grid network is maintained.
- 11.3.12.14. A more refined grid network of public streets than identified on Map 11.3.B will be considered in preparing and reviewing development applications and Development Concept Plans.
- 11.3.12.15. The road network is comprised of arterial roads (Steeles Avenue, Jane Street, Keele Street), the primary east-west road, six (6) local north-south roads, and laneways. Map 11.3.B identifies the major road network. The right-of-way widths for the various roads are generally as follows:
- a. east-west primary road - up to 26 metres;
  - b. local roads(with transit) - 23 metres;
  - c. local roads(without transit) - 20 metres;
  - d. mid-block local roads up to 17.5 metres; and,
  - e. laneways, where necessary - 7.5 to 8 metres.
- 11.3.12.16. The road allowance widths identified in this Secondary Plan are approximate and relate to the structural right-of-way width for a fully urbanized cross-section. In certain cases, widening of existing roads to the ultimate road allowance width shown may not

be required. In some cases the right-of-way may need to be wider to accommodate engineering and/or design requirements.

- 11.3.12.17. The Figures in this Secondary Plan illustrating the right-of-way and streetscape typologies are conceptual. Final right-of-way requirements and the design of road surfaces, utilities, sidewalks, daylight triangles, boulevards, etc., will be determined during the review of development applications.
- 11.3.12.18. When public transit routes have been finalized, they will be located on roads with a minimum right-of-way of 23 metres to accommodate transit vehicles, even if the road is identified as a 'local road'.
- 11.3.12.19. It is a requirement of this Secondary Plan that all roads identified within the Corridor shall be built and maintained to an operational standard satisfactory to the City, and shall provide for permanent public access for traffic through the Corridor at locations satisfactory to the pertinent authorities.
- 11.3.12.20. Local and primary road rights-of-way shall be identified in all approved plans within the Corridor, and shall be conveyed to the municipality as a condition of approval of draft plan of subdivision, prior to the registration of the plans. Alternatively, the City may in the subdivision agreement permit private roads designed and built to City standards with a public easement that conveys the road to the City at no cost, any time in the future the City deems necessary.
- 11.3.12.21. No plan shall be approved which permits the construction of any structure on a local or primary road right-of-way identified on Map 11.3.B. Road allowances reserved or conveyed to the municipality may be used on a temporary or permanent basis for parking purposes at the sole discretion of Council.
- 11.3.12.22. The road network within the Corridor shall recognize natural and built constraints and shall form a continuous and interconnected grid pattern, integrated with the planned road network on surrounding lands. The City will work with the Region of York, City of Toronto and York University in implementing the north-south local road network, so that the connections identified in the Secondary Plan align with pedestrian and vehicular connections to the south of Steeles Avenue.

- 11.3.12.23. The City may implement laneways or other similar traffic calming and traffic control measures subject to the evaluation of functional, operational and financial issues associated with their use.
- 11.3.12.24. Every street shall be designed to accommodate street trees to give streets a unity of form and shade for pedestrians.
- 11.3.12.25. Individual and direct vehicular access to Steeles Avenue shall not be permitted.
- 11.3.12.26. The use of cul-de-sacs shall generally be prohibited.
- 11.3.12.27. Development shall allow for the establishment of a continuous secondary east-west connection for pedestrians, cyclists and, through most blocks, vehicles, as conceptually shown on Map 11.3.B.

### **Arterial Road Network**

- 11.3.12.28. Steeles Avenue is under the jurisdiction of the City of Toronto, and is also identified in the York Regional Official Plan as a Regional Corridor. The treatment of the road allowance is extremely important to the overall character of the Secondary Plan area.
- 11.3.12.29. The City of Toronto with respect to Steeles Avenue, and The Region of York with respect to Jane Street and Keele Street, shall be encouraged to modify the role and function of the arterial roads from that of a single use transportation corridor, to that of a multipurpose urban street and pedestrian and transit supportive place.
- 11.3.12.30. To allow for pedestrian access across Steeles Avenue, signalized intersections are encouraged.
- 11.3.12.31. Arterial road widenings may also be required to implement the streetscaping elements identified in this Secondary Plan. Where necessary, these widenings shall be conveyed to the Region of York or City of Toronto as a condition of approval of draft plan of subdivision, prior to the registration of the plans. Alternatively, the Region or the City of Toronto may, in the subdivision agreement, permit streetscaping that is designed and built to these standards to be provided on a public easement that retains the streetscaping function on the private lands.

- 11.3.12.32. The City of Vaughan will work with the City of Toronto, York University, and York Region towards the implementation of an enhanced streetscape on Steeles Avenue, Jane Street and Keele Street as the Corridor develops over time.
- 11.3.12.33. Given its dominant length and impact on the function and character of the entire Corridor, the potential treatment of Steeles Avenue is illustrated below in Figure 11.3.A.

### **Primary East-West Road**

- 11.3.12.34. The continuous east-west primary road is a critical transportation connection to serve transit and road network needs, and therefore support access to and from the land uses in this Secondary Plan. The east-west primary road is to enable traffic flow through and around the Corridor and reduce the already considerable congestion on the Steeles/Jane and Steeles/Keele intersections. This Secondary Plan establishes the principle and process for securing the east-west primary road.
- 11.3.12.35. The east-west road is to be established as early as possible as a continuous road, and in the initial phase of all development.
- 11.3.12.36. The east-west primary road is to be located to the greatest extent practicable, in the lands owned by the Ontario Realty Corporation (ORC) and being leased to Hydro One. Figure 11.3.B illustrates the preferred option of using at least 10 metres of the Hydro Corridor for the east-west primary road right-of-way. The detailed location, design and geometries of this road will need to be addressed as part of an Environmental Assessment and will need the approval of ORC and Hydro One. The connections of this east-west road to Jane Street and Keele Street are to ultimately be fully signalized intersections.
- 11.3.12.37. Until the scale and precise location of the east-west primary road is approved by ORC, the City will secure a right-of-way, easement, or reserve of up to 26 metres wide as part of all development applications. The easement or reserve is to be located at the northern limit of the private lands abutting the Hydro Corridor. Once the necessary right-of-way in the Hydro Corridor is secured, all excess right-of-way will be returned to private landowners to further facilitate development in the area. Until the east-west road is established, the easement or reserve may be used for driving aisles and/or parking but not for the location of buildings.

- 11.3.12.38. Pedestrian streetscape amenities within the east-west primary road are to focus on the south side of the east-west primary road.
- 11.3.12.39. Connections should be made from the east-west primary road to the Community/Multi-use Recreational trail identified in the draft Pedestrian and Bicycle Master Plan Study (2004).
- 11.3.12.40. The potential treatments of the east-west primary road, both using and not using the corridor respectively, are illustrated below in Figure 11.3.B and Figure 11.3.C. Localized widening may be required to accommodate exclusive left-turn lanes and may be secured without amendment to this Secondary Plan.
- 11.3.12.41. Developers shall be financially responsible for the portion of the primary east-west road required on existing private lands that are subject to an application for plan of subdivision. Alternatively, the City may consider other arrangements that result in the costs of this portion of the road being distributed among multiple landowners, including public agencies.
- 11.3.12.42. Residential uses on the south side of the east-west primary road should provide a minimum 5 metre landscaping buffer.

### **Local North-South Road Network**

- 11.3.12.43. The alignments of the local north-south road connections are to establish logically sized and configured development blocks, appropriate signal spacing, and integration with pedestrian routes, roads, and/or driveways to the south of Steeles. Generally, the north-south local roads shown on Map 11.3.B are to align with signalized intersections and connect to the pedestrian and/or road system on York University's lands.
- 11.3.12.44. The roads shown on Map 11.3.B on the south side of Steeles Avenue are conceptual only. York University and the City of Toronto are encouraged to establish over time a grid network of streets on the south side of Steeles Avenue aligned with the planned grid on the north side, to service development and facilitate movement by all modes of transportation. If it is not practical or desirable to extend all of the north-south roads south of Steeles Avenue, the roads on the north side still will be required. Signalized pedestrian connections to the south side of Steeles- Avenue are to be implemented in accordance with this Secondary Plan. The City of Vaughan will work with the City of

Toronto and York University to coordinate the specific alignments and connections of the local north-south road connections to Steeles Avenue.

- 11.3.12.45. Local north-south roads shall be designed to provide high levels of pedestrian amenity and connections while providing adequate accommodation for cars and transit vehicles.
- 11.3.12.46. Street 'A' is to be aligned opposite Murray Ross Parkway to the south, and is to occur as part of the major redevelopment of lands to the north of Steeles Avenue, and not as part of the expansion of the existing use.
- 11.3.12.47. As part of the location of the Transit Station, Street 'C' is to be aligned opposite North West Gate to the south of Steeles Avenue in York University.
- 11.3.12.48. Founders Road shall be extended north to intersect with the new east-west road in conjunction with development of the adjacent lands or when required for the purposes of traffic management in the area.
- 11.3.12.49. The other road connections to Steeles Avenue (Streets 'B', 'D', 'F' and 'G') may shift slightly east or west from the locations identified on Map 11.3.B, and, an early determination of the precise locations of these streets is strongly supported by this Secondary Plan.
- 11.3.12.50. As a potential bus transit route, one of the local north-south Streets may need the ability to accommodate transit vehicles and therefore provide a right-of-way sufficient to accommodate transit vehicles. This Secondary Plan supports such a route and will accommodate the required right-of-way.
- 11.3.12.51. The potential treatments of the local roads, both with transit and without transit, are illustrated below in Figure 11.3.D and Figure 11.3.E.

### **Bicycle Network**

- 11.3.12.52. The development of all roads shall include safe, convenient and attractive facilities for cyclists.

- 11.3.12.53. The Pedestrian and Bicycle Master Plan has identified the potential need for a Community/Multi-use Recreational Trail extending from Jane Street to Keele Street in the Hydro Corridor and two pedestrian and bicycle north-south connections between Jane and Keele Streets. These links, shown on Map 11.3.B, will facilitate continuity of the City-wide pedestrian and bicycle network, north-south movements across Highway 407, and provide an alternative to the busy streets of Jane and Keele.
- 11.3.12.54. The Recreational Trail within the Hydro Corridor should:
- a. be developed as a dedicated corridor/bicycle lane as part of, or adjacent to, the final alignment of the east-west primary road right-of-way;
  - b. have convenient and direct access to the Transit Station through incorporation of a dedicated bicycle lane in Street 'C';
  - c. be linked to York University, potential bus stops, and sites in the Secondary Plan area via the north-south local streets and other midblock connections;
  - d. connect to the City of Toronto's bike route on Murray Ross Parkway, and potentially to a Steeles Avenue off-street bike route west of Keele Street.
- 11.3.12.55. An adequate supply of secure bicycle parking shall be provided at the subway station, near bus stops, in urban squares, and in other high activity areas.

### **Parking**

- 11.3.12.56. Transit-supportive parking standards shall be established for the Corridor through a City-initiated zoning by-law amendment based upon a City-initiated parking study, or on an application-specific basis with a supporting parking study. Reduced parking standards may be phased in over time as the target densities in the Secondary Plan are achieved. To encourage transit use and discourage an oversupply of parking, maximum parking standards will be established for the Corridor.
- 11.3.12.57. Sharing of parking in mixed-use developments will be encouraged within the Corridor, subject to evaluation by the City.
- 11.3.12.58. On-street parking may be permitted, and spaces provided on either the local, east-west primary or arterial roads may be credited towards the parking provided on-site as long as the peak roadway demands and functions are not significantly compromised.

## **Travel Demand Management**

- 11.3.12.59. The City will actively work with the Region, Smart Commute North Toronto, Vaughan, and with developers, owners and tenants in the Corridor to develop, implement, facilitate and promote measures to increase the use of transit, cycling and walking, and reduce the use of low occupancy automobiles for trips, particularly trips to and from work. These measures may include:
- a. promoting the use of public transit by employees;
  - b. promoting measures to foster higher vehicle occupancy;
  - c. assisting in organizing and promoting car pooling;
  - d. giving priority parking space assignments and/or reduced rates for car pool participants;
  - e. varying hours of work to reduce peak hour loads;
  - f. participating in a Transportation Management Association; and,
  - g. giving priority parking space assignments or reduced rates for low and non-polluting motor vehicles such as electric cars, as they become available to the general market; and/or, other measures that may be identified.
- 11.3.12.60. Measures to support transit use, such as maximum parking standards, priority signaling, shared parking arrangements, public parking structures, and “paid parking only” strategies, may also be required.
- 11.3.12.61. Traffic Impact Studies submitted in support of development applications shall identify and assess proposed Transportation Demand Management Strategies. The recommendations of these studies shall include the method of implementing the travel demand management program for the proposed development.
- 11.3.12.62. Traffic Impact Studies submitted in support of development applications are required to review background traffic growth generated by the York University campus.

## **Transportation Improvements**

- 11.3.12.63. Full implementation of this Secondary Plan requires the following improvements to the transportation system:
- a. Region of York:
    - i. construction of the road widenings for Keele Street and Jane Street
    - ii. construction of a transit station and dedicated bus rapid transit lanes connecting north to the Highway 7 corridor

- iii. construction of the subway station and connection to the Vaughan Metropolitan Centre and Highway 7 Rapid Transit corridor.
    - b. City of Toronto:
      - i. streetscaping improvements to Steeles Avenue including the centre median
    - c. City of Vaughan:
      - i. construction of the north-south road connection to Snidercroft Road
      - ii. construction of the easterly extension of the east-west primary road east of Keele Street and south to Steeles Avenue.
    - d. Region of York/City of Vaughan:
      - i. Securing and construction of the continuous east-west primary road connecting from Keele Street to Jane Street.
    - e. City of Vaughan/City of Toronto/York University
      - i. the alignment of the north-south local roads with driveways, roads or pedestrian connections on the south side of Steeles Avenue.
- 11.3.12.64. It is the policy of Council to assist the Region of York and City of Toronto in protecting and obtaining lands required for the rights-of-way and for the widening of roads for the provision of public transit services through the development approval process.
- 11.3.12.65. Where lands have been identified as required for the construction of the local and primary road network, and where such lands are the subject of a development application, it is the policy of Council to require the dedication of such lands before permitting the development of the site. Alternatively, lands may be provided in accordance with policies in Section 11.3.12.20.
- 11.3.12.66. From time to time, at the discretion of Council, lands for planned road or transit improvements may be directly purchased or expropriated by the City, in order to foster the planned and orderly development of the Corridor. It is Council's intention that the funds for such acquisition of land and for the construction of planned road or transit improvements be provided to the greatest extent practical through a charge against development in the Secondary Plan and/or the City and/or the Region under the provisions of the Development Charges Act, or by other means available to the City.
- 11.3.12.67. To enable the development of the Secondary Plan to proceed as expeditiously as possible, it is Council's intention that the planned transportation improvements be

reflected in the capital works forecasts and any Development Charges By-law, in such a fashion as to permit development without delay. As such, the City will urge other levels of government to proceed likewise in circumstances where the jurisdiction for a transportation network improvement is not with the City. The specific need and timing for these improvements will be determined based on the monitoring program set out in Section 11.3.16 of this Secondary Plan.

### **11.3.13 Environmental And Servicing Policies**

- 11.3.13.1. Development shall recognize the significance of the Black Creek valley system to the health of the local and regional environment. Impacts on the valley system and the larger Black Creek watershed shall be managed following best practices in sustainability. Impacts from development in the Secondary Plan Area on downstream water quality shall be positive or neutral.
- 11.3.13.2. The use of permeable materials for parking areas is encouraged.
- 11.3.13.3. The design of rooftops and parking areas should minimize the heat island effect, through rooftop gardens, green roofs and the planting of shade trees between parking aisles.
- 11.3.13.4. Streetscaping shall include irrigation systems for street trees where appropriate and feasible.
- 11.3.13.5. The City shall support and encourage the development of district energy, heating and cooling systems.
- 11.3.13.6. Solar power shall be considered for street lighting.
- 11.3.13.7. The City may require that development applications include a Sustainability Plan. Sustainability Plans shall consider the following techniques to reduce stormwater runoff, improve water quality and conserve energy:
  - a. rain barrels or cisterns to capture rainwater for reuse in landscape irrigation and other non-potable water applications;
  - b. vegetated swales to filter and detain stormwater;
  - c. porous surfaces for pathways, patios and parking lots to allow infiltration of stormwater;

- d. greywater systems that capture stormwater runoff and other greywater for reuse in toilets and industrial operations;
- e. the use of renewable energy sources for building systems and exterior lighting, such as solar, wind and geothermal;
- f. cogeneration, i.e., capturing and using heat from power generation;
- g. green roofs;
- h. other techniques encouraged by the policies of this Secondary Plan, and which may be identified by City staff.

### **Water, Wastewater And Stormwater Management Services**

- 11.3.13.8. Development within the Secondary Plan Area shall be on the basis of the full forecasts of development within this Secondary Plan.
- 11.3.13.9. A comprehensive approach to addressing the servicing requirements of the Secondary Plan Area is needed. As these requirements must be addressed prior to consideration of development approvals, completion of the necessary studies is a priority. The City will undertake a study to comprehensively address the Secondary Plan Area's storm water management requirements. A Master Environmental Servicing Plan outlining preliminary functional water distribution, wastewater collection and stormwater management facilities, including designs and costs, will be prepared by the City in consultation with the Region, Toronto and Region Conservation Authority or, alternatively, by proponents to the satisfaction of the City, the Region and the Toronto and Region Conservation Authority as a condition of approval of development applications. The MESP should include a comprehensive storm water management strategy identifying the development-related storage requirements and storage locations, including innovative, sustainable water management practices (e.g., 'green roof' storage, underground storage).
- 11.3.13.10. Development may be phased to coincide with the availability of all the necessary services being available for development, subject to the Phasing Policies of this Secondary Plan Plan.
- 11.3.13.11. Stormwater management practices shall be designed and implemented to the satisfaction of the City and the Toronto and Region Conservation Authority, based on

overall stormwater management criteria for Steeles Avenue within this corridor. The overall Master Servicing Plan should include a comprehensive stormwater management strategy that sets out criteria for the entire area.

- 11.3.13.12. The integration of stormwater management and water recycling facilities in the design of buildings, open spaces and parking areas is encouraged. Where public stormwater management facilities, in addition to those identified on Map 11.3.C, are required, they shall be designed as accessible, park-like open spaces.
- 11.3.13.13. Development shall provide for the management of stormwater runoff, and the promotion of water quality treatment on a comprehensive watershed basis. On-site storage of stormwater (e.g. parking lots and rooftop controls) will also be considered as an option for the treatment of stormwater.
- 11.3.13.14. New infrastructure or improvements to existing infrastructure, including roads, sanitary and storm sewers, stormwater management facilities, municipal water and electricity supplies, required to serve proposed development shall be provided and paid for by the developer.

### **Utilities And Servicing**

- 11.3.13.15. All utilities, including telecommunications, will be planned for and installed on a coordinated and integrated basis in initial common trenches, wherever possible, in order to be more efficient, cost effective and minimize disruption.
- 11.3.13.16. A comprehensive approach to address the servicing requirements of the Secondary Plan Area is needed. As these requirements must be addressed prior to consideration of development approvals, completion of the necessary studies is a priority. The City will undertake a Functional Master Servicing Strategy Report and a Traffic Impact Study to comprehensively address proposed development within the Secondary Plan Area in terms of the servicing and transportation infrastructure requirements for the Secondary Plan Area.

### **11.3.14 Administration**

- 11.3.14.1. The policies contained in this Secondary Plan shall apply to the lands shown on Map

11.3.A as the Steeles Corridor -Jane to Keele - Secondary Plan Area. Except as otherwise provided herein, the policies of this Secondary Plan shall supersede any other policies as contained in Chapters 1 through 10 and 12 of the Official Plan.

- 11.3.14.2. In respect of the lands located at the northeast corner of Jane Street and Steeles Avenue, which are subject to Amendment No. 454, as amended by OPA No. 481, the policies of Sections 4.2.3.f) (Regional modification No.4) and 4.2.3.g) (Regional modification No.5) of Official Plan Amendment No. 500 shall apply (With necessary modifications). In addition, the policies established by this Secondary Plan, as expressed in Section 3.2.3 and other relevant sections, also apply to the northeast corner of Jane Street and Steeles Avenue.

### **11.3.15 Implementation**

- 11.3.15.1. A Steeles Corridor Coordinating Committee, including staff from the City of Vaughan, the City of Toronto and York Region, York University Development Corporation, Black Creek Pioneer Village, Hydro One, CN Railway Properties, UPS, TRCA and the various transit providers will be established to oversee and facilitate the coordinated implementation of the plans north and south of Steeles Avenue and address, on an ongoing basis, specific issues related to road and pedestrian connections, land use and built form compatibility, transit facilities, community services, noise and streetscaping.
- 11.3.15.2. The initiation and completion of an Environmental Assessment for the proposed east-west road adjacent to, and potentially within, the Hydro Corridor is a critical early step in implementing this Secondary Plan. Upon approval of this Secondary Plan, the City will initiate the EA in cooperation with Ontario Realty Corporation, the Region of York and the transit providers.

### **Development Concept Report And Phasing Plan**

- 11.3.15.3. To provide a context for coordinated development, and to demonstrate conformity with the policies of this Secondary Plan, each development application, in particular those applications intended to develop over a number of phases, shall include a Development Concept Report, in accordance with policy 10.1.1.5, providing a detailed description of the proposed development, and the manner in which it addresses the policies of this Secondary Plan.

- 11.3.15.4. Within each block of the Secondary Plan, development applications should co-ordinate neighbouring development proposals in a mutually complementary fashion. Non-participating lands in the block shall be shown conceptually in the Development Concept Report and Phasing Plan.
- 11.3.15.5. Development shall be phased to provide for the orderly development of the Corridor and Secondary Plan Area, and to ensure the most efficient and economical use of existing and proposed infrastructure. The following phasing criteria shall be considered in the review of all development applications:
- a. the development contributes to, or can be appropriately integrated within the logical sequence of construction of all required sewer, water, stormwater and transportation facilities;
  - b. the development satisfies all requirements regarding the provision of parkland and other public facilities including streetscaping and landscaping;
  - c. traffic from the proposed development can be accommodated on the existing and planned road network, and,
  - d. phasing may be addressed through the appropriate use of the policies of this Secondary Plan respecting the application of the holding zone provisions of the zoning by-law.
- 11.3.15.6. As a component of the Development Concept Report, development applications shall provide a Phasing Plan, which:
- a. describes and illustrates how existing and proposed development can be incorporated into the site to achieve the full development potential of the site;
  - b. considers existing neighbouring uses and the potential need to buffer or stage particular uses;
  - c. identifies the public infrastructure and facilities required to serve the development, including water, sewer, stormwater management, roads, transit, parks and open spaces, and other community facilities and services, and their proposed phased construction; and .
  - d. describes the expected financial requirements for such public infrastructure, and the appropriate financial contributions from benefiting landowners.
- 11.3.15.7. Phases are to be based upon the existence of, or commitment to construct, the following infrastructure elements:

- a. Components of the local road network, including the continuous east west primary road;
- b. bus-rapid transit;
- c. the subway; and,
- d. public and community services.

### **Subdivision Control**

- 11.3.15.8. Subdivision Control encompasses draft plans of subdivision/condominium, consents and part lot control exemptions.
- 11.3.15.9. To secure the related infrastructure improvements required, all new development in the Secondary Plan area shall, as part of its initial development application process, proceed by way of the subdivision approval process that includes the full extent of property ownership, and includes a Development Concept Report and Phasing Plan. Plans of subdivision/condominium shall only be draft approved which:
  - a. conform with the policies and designations of this Secondary Plan;
  - b. can be provided with adequate services and facilities as required by the policies of this Secondary Plan; and,
  - c. are not premature and are in the best interest of the municipality.
- 11.3.15.10. Policy 11.3.15.9 does not apply to extensions or expansions to existing uses as provided for in policy 10.2.1.3.

### **Community Improvement**

- 11.3.15.11. It is the intent of the City that the area identified on Map 11.3.A of this Secondary Plan be identified as a Community Improvement Area under Section 28 of the Planning Act.

### **11.3.16 Monitoring**

- 11.3.16.1. The City, in partnership with the Region, is to establish a formal program to monitor and report on the level of development in the Secondary Plan.
- 11.3.16.2. The Monitoring Program will address:
  - a. traffic volumes on key routes and at key intersections, based on periodic traffic counts in the Corridor;
  - b. the amount of existing and proposed floor space for which subdivision, zoning

or site plan approval has been granted, status of development approvals, completions and occupancy;

- c. travel characteristics and modal split including if possible, trends in the number of pedestrians and cyclists;
- d. population and employment generated by existing development and projected for approved but not yet occupied development;
- e. evaluation of traffic volumes and transit ridership in the context of available capacity; and,
- f. evaluation of existing, planned and proposed Development Concept Reports, Phasing Plans, and the status of the York University Secondary Plan, in order to allow for identification and planning of transportation improvements, or to allow time to take steps to control the pace of development in relation to the provision of services.

- 11.3.16.3. The regular monitoring program is, at a minimum, to take place as part of the periodic review of this Secondary Plan, at intervals of five years, and at a public meeting of Council. Sufficient detail will be made available to the public to enable the verification of results and conclusions.

### **11.3.17 Interpretation**

- 11.3.17.1. This Secondary Plan is a statement of policy. It is intended to guide the development of the Steeles Corridor - Jane to Keele- Secondary Plan Area. Some flexibility in interpretation is permitted, provided the intent of the policies and principles of this Secondary Plan are maintained.
- 11.3.17.2. The designations identified on Map 11.3.A are intended to show general areas. Minor adjustments to the boundaries of the designations may occur through the City's approval process without amendment to this Secondary Plan, except where the designations are established by fixed boundaries, such as road rights-of-way, or where they are specifically stated as fixed in the policies of this Secondary Plan.
- 11.3.17.3. Where lists or examples of permitted uses are provided, they are intended to indicate the possible range and type of uses that are to be considered. Specific uses not listed, but considered by the City to be similar to the listed uses and to conform to the general intent of the applicable land use designation may be recognized as a permitted use and

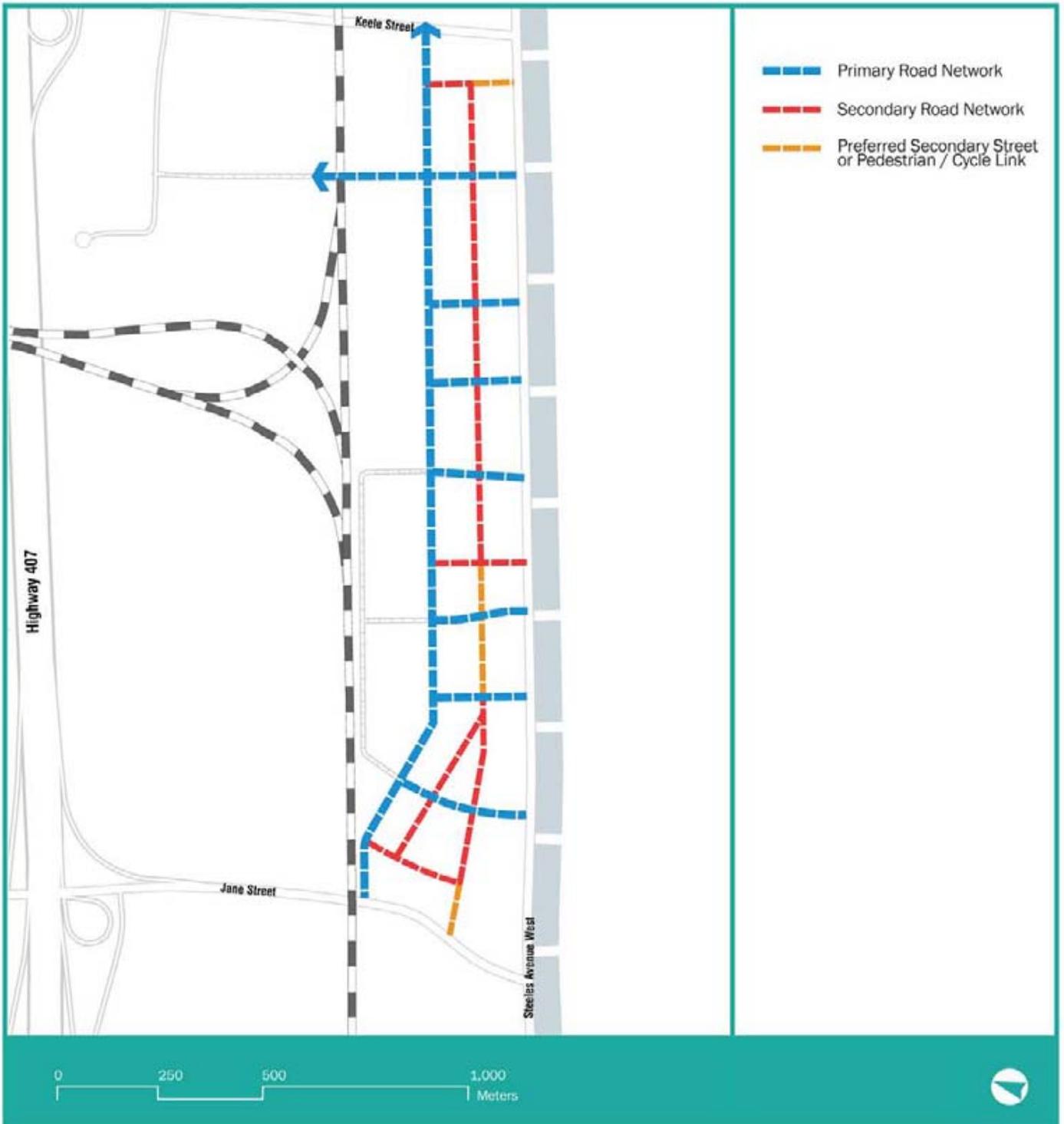
recognized in the implementing zoning by-law.

- 11.3.17.4. Minor variations from numerical requirements in the Secondary Plan may be permitted without an Official Plan Amendment provided that the general intent of the Secondary Plan is maintained.

▼ Map 11.3.A  
 Steeles West Secondary Plan - Land Use



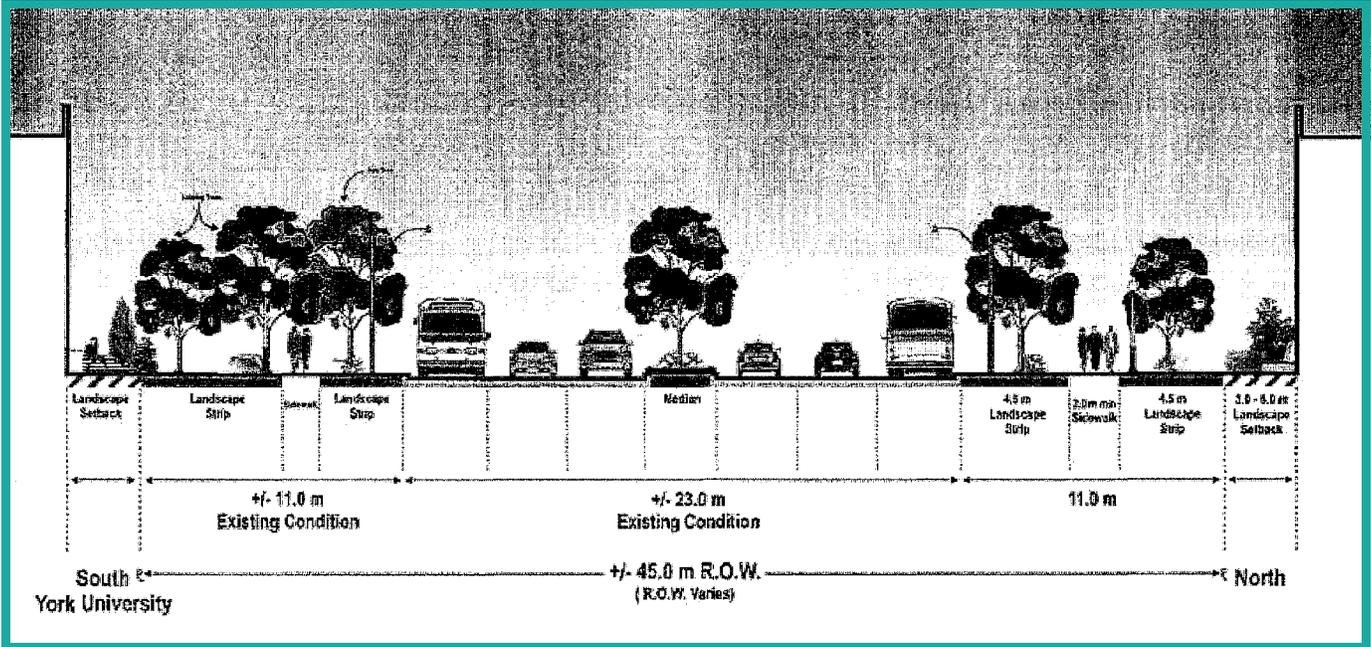
▼ Map 11.3.A  
Steeles West Secondary Plan - Transportation Network



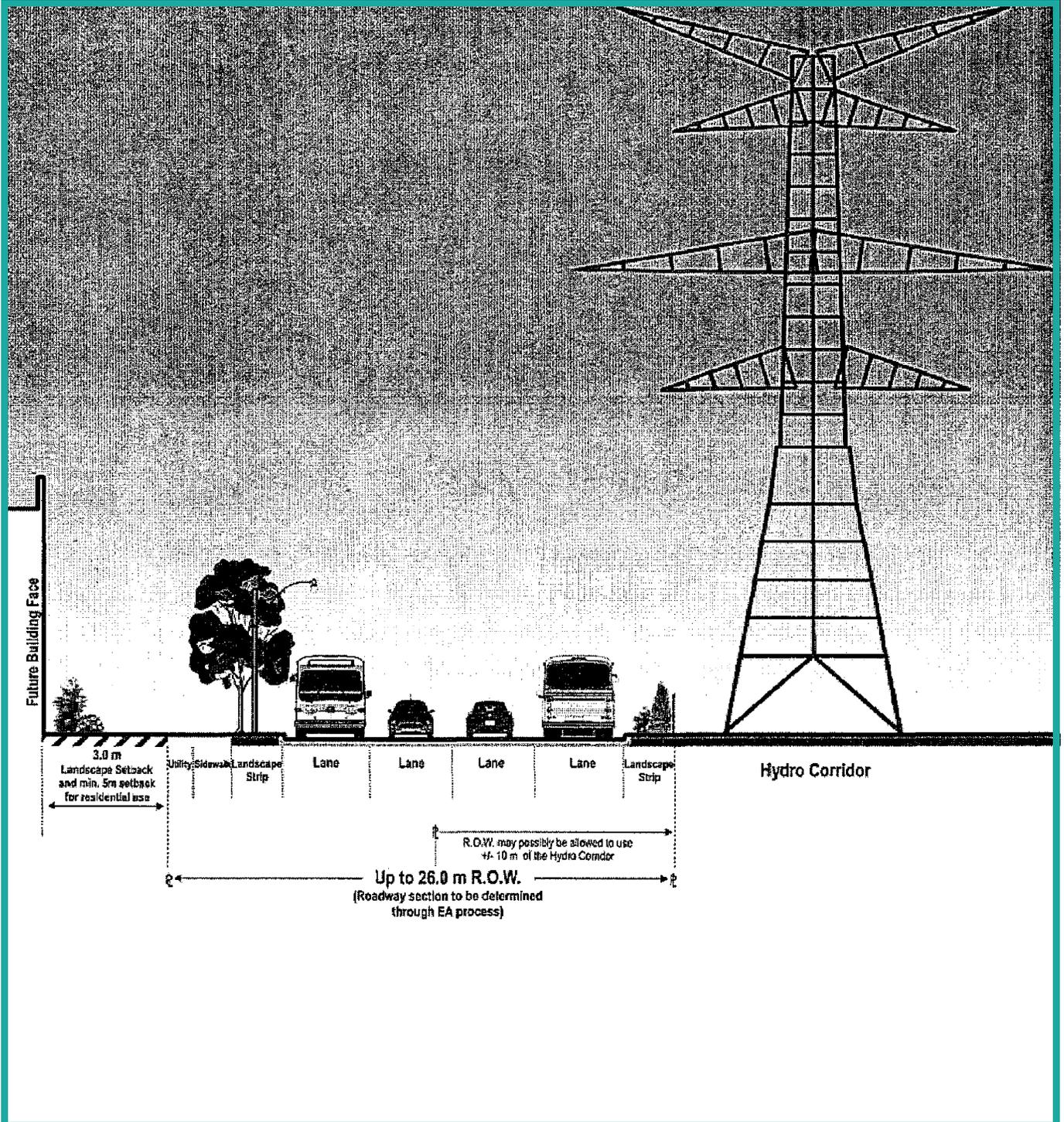
▼ Map 11.3.C  
 Steeles West Secondary Plan - Open Space Network



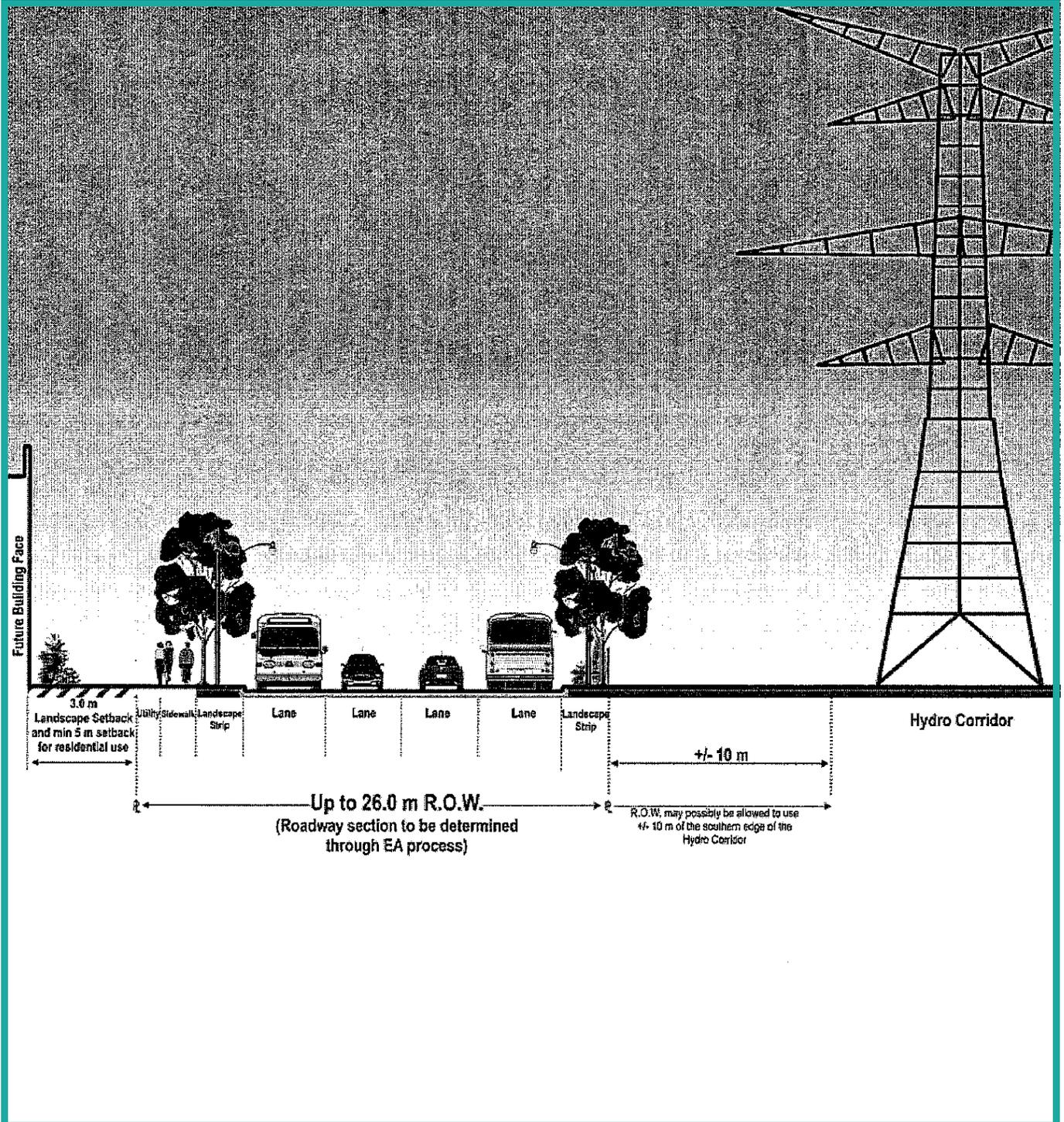
▼ Figure 11.3.A  
 Steeles Avenue Right-of-way and Streetscape



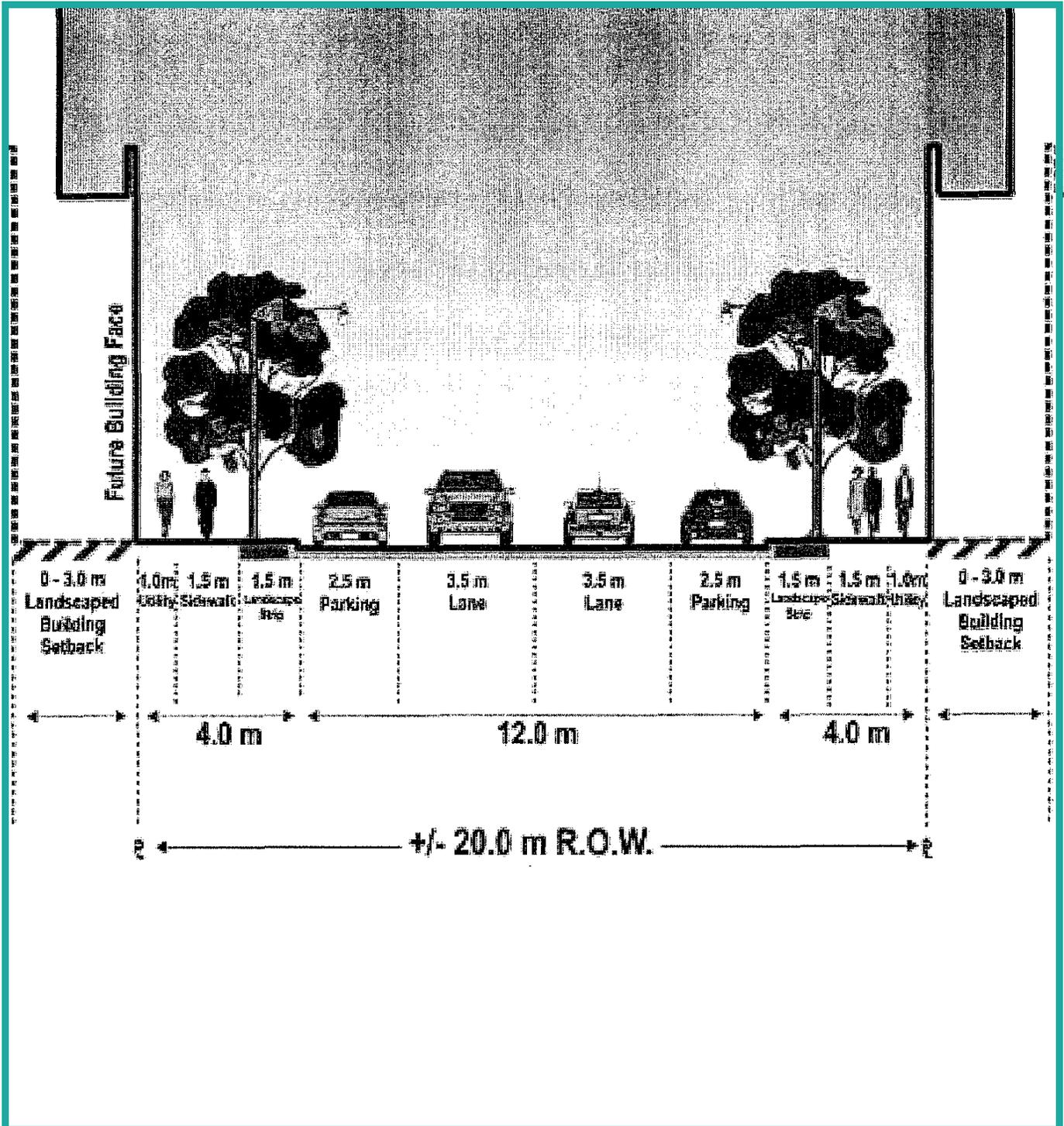
▼ Figure 11.3.B  
 East-West Primary Road and Streetscape (using the Hydro Corridor)



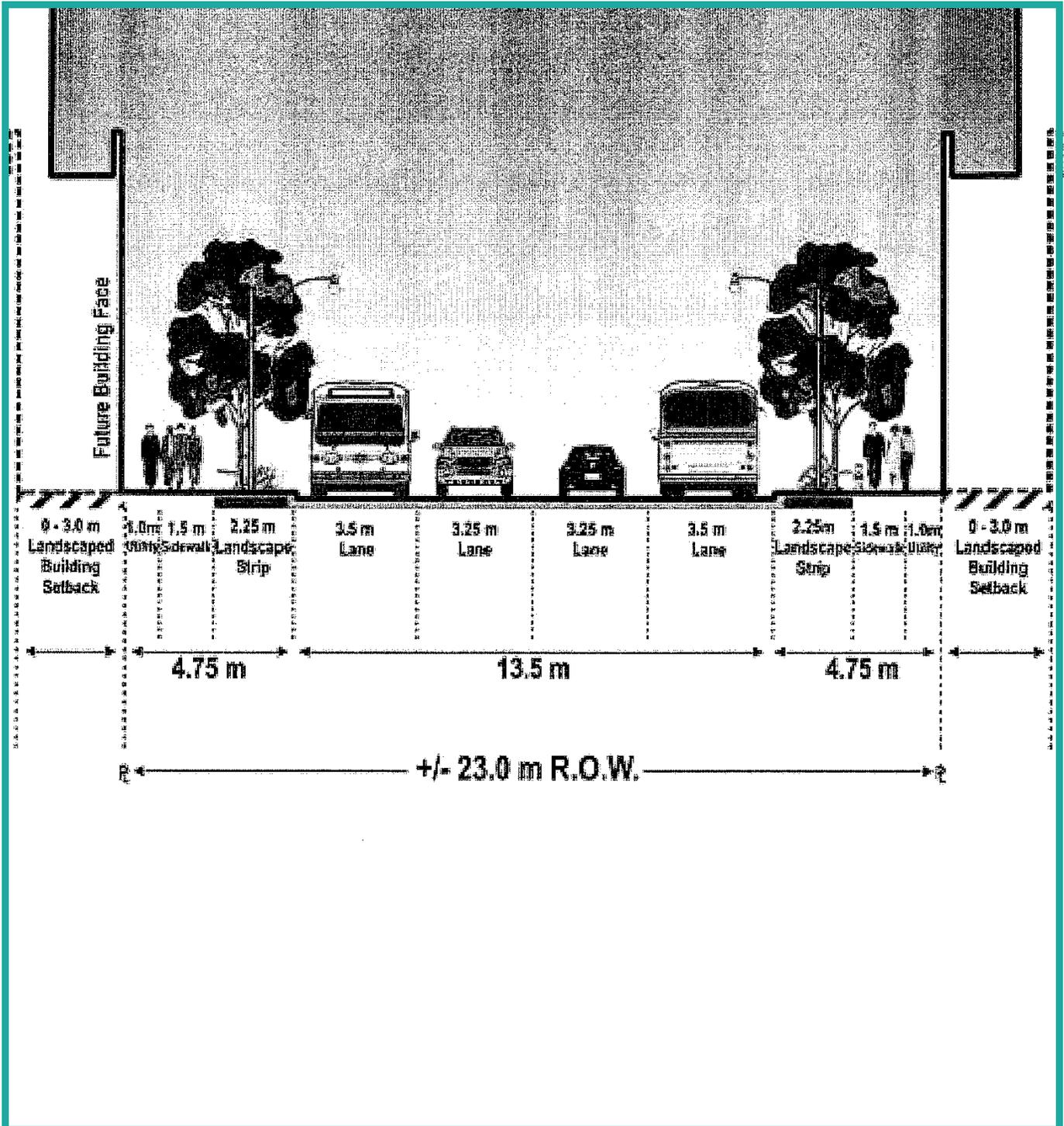
▼ Figure 11.3.C  
 East-West Primary Road and Streetscape (not using the Hydro Corridor)



▼ Figure 11.3.D  
Local road and Streetscape (without transit)



▼ Figure 11.3.E  
Local Road and Streetscape (with Transit)



## 11.4 Highway 400 North Employment Lands

### 11.4.1 General

- 11.4.1.1. Notwithstanding policy 9.2.1.1, the following shall apply in the Highway 400 North Employment Area, with the exception of the areas identified as within the Greenbelt Plan Area:
- a. Institutional uses including major educational facilities and hospital/health care facilities shall be permitted;
  - b. Retail warehousing uses are prohibited; and,
  - c. Where existing residential uses are to be maintained after the development of the Employment Area, consideration shall be given to the provision of buffering and other measures to mitigate impacts from adjacent employment uses on the existing residential use.

### 11.4.2 Land Use: Prestige Employment

- 11.4.2.1. Notwithstanding policy 9.2.1.1, the following policies apply to the area identified as **Prestige Employment** on Map 11.4.A.
- 11.4.2.2. The purpose of the **Prestige Employment** designation within the Secondary Plan area is to:
- a. provide locational opportunities for activities which require high visual exposure, good accessibility and an attractive working environment;
  - b. define areas where it is appropriate to restrict uses which, by nature of their operating characteristics, would not contribute to attractive streetscapes.
  - c. provide opportunities for employment intensive uses that will take advantage of and support the transit system.
- Development Policies**
- 11.4.2.3. **Prestige Employment** areas shall generally be developed with larger lots. Minimum unit sizes, or other measures may be required in order to support the prestige environment.
- 11.4.2.4. A wide range of industrial, office, business and civic uses shall be permitted. No outside storage of goods or materials shall be permitted in this designation. The zoning by-law shall establish the permitted uses and development standards.
- 11.4.2.5. Vehicular access to lots abutting arterial roads and provincial highways shall generally

be from the internal road network. Access to the provincial or arterial road systems shall be subject to the approval of the appropriate authority.

- 11.4.2.6. In instances where through lots are provided, buildings shall be designed so that all elevations facing a street present a “front” elevation. Loading areas are not considered appropriate in any yard facing a street. The location of loading areas will be controlled in the zoning by-law.
- 11.4.2.7. The following building types are permitted in **Prestige Employment** areas shown on Map 11.4.A pursuant to policies in Section 9.2.3:
- a. Employment/Industrial buildings;
  - b. Low-Rise buildings;
  - c. Mid-Rise buildings; and,
  - d. Gas Stations

### **11.4.3 Land Use: Prestige Employment - Office/Business Campus**

- 11.4.3.1. Notwithstanding policy 9.2.1.1, the following policies apply to the area identified as **Prestige Employment – Office/Business Campus** on Map 11.4.A.
- 11.4.3.2. **Prestige Employment - Office/Business Campus** areas are located adjacent to lands in the Natural System of the Provincial Greenbelt Plan, as well as to provincial highways and arterial roads.
- 11.4.3.3. To provide opportunities for high quality business and office employment uses and activities, including campus style development, which require good accessibility in an urban environment which is enhanced by a location adjacent to significant natural areas such as lands in the Natural System of the Provincial Greenbelt Plan.
- 11.4.3.4. To define areas where it is appropriate to restrict uses which, by the nature of their operating characteristics, would generally not be compatible with locations adjacent to the Natural System of the Greenbelt Plan, or attractive streetscapes.
- 11.4.3.5. To provide opportunities for employment intensive uses that will take advantage of and support the transit system.

## **Development Policies**

- 11.4.3.6. The predominant permitted uses shall be business/office and industrial development, including campus style developments, research facilities and public uses including a new Regional Park. The outside storage of goods or materials shall not be permitted. In addition, uses expected to involve chemical storage or to generate significant truck traffic, particularly stand alone warehouse operations, shall not be permitted.
- 11.4.3.7. Development shall be designed to complement the lands in the Natural System of the Greenbelt Plan, and be enhanced by its location in a natural setting. In particular, views and accessibility, both visually and physically, to lands in the Greenbelt shall be maximized. This will be accomplished through a range of different approaches, including requiring the use of single-loaded roads in key locations, the placement of employment buildings, and the location of parks or other public facilities.
- 11.4.3.8. The need for a Regional Park of approximately 30 hectares has been identified by the City in the Highway 400 corridor, west of the highway. Map 11.4.A identifies a conceptual location for the park. The precise size, location and configuration will be determined through the Block Plan process and the City's updated Parks and Recreation Master Plan.
- 11.4.3.9. **Prestige Employment-Office/Business Campus** areas shall generally be developed with larger lots. Minimum landscaping standards, and minimum and maximum unit and building sizes, heights and densities, or other measures, shall be implemented through the zoning by-law and urban design guidelines, to support the prestige environment, the enhanced relationship with the Natural System lands, and to ensure that the development is transit-supportive.
- 11.4.3.10. Development shall be designed to allow for future intensification; in particular, opportunities for additional building sites should be anticipated in the layout of buildings, parking areas, and primary site plan elements, such as service routes and significant landscaping.
- 11.4.3.11. In instances where lots front on Highway 400 or arterial roads, buildings shall be designed so that all elevations facing a street present a "front" elevation. The location of loading areas shall be controlled in the zoning by-law, and shall not be permitted in any yard facing a street.

- 11.4.3.12. Vehicular access to lots abutting arterial roads and provincial highways shall generally be from the internal road network, Access to the provincial or arterial road network systems shall be subject to the appropriate authority.
- 11.4.3.13. The lands between Highway 400 and the Greenbelt Natural System Area immediately north of Kirby Road will require special consideration given its proximity to the Natural System and Highway 400, and the limitations on access. In addition to the other permitted uses, consideration may be given to low intensity recreational or other similar uses in recognition of the unique attributes of this site.
- 11.4.3.14. The following building types are permitted in Prestige Employment- Office/Business Campus areas shown on Map 11.4.A pursuant to policies in Section 9.2.3:
- a. Employment/Industrial buildings;
  - b. Low-Rise buildings;
  - c. Mid-Rise buildings; and,
  - d. Gas Stations

#### **11.4.4 Land Use: General Employment**

- 11.4.4.1. Notwithstanding policy 9.2.1.1, the following policies apply to the area identified as **General Employment** on Map 11.4.A.
- 11.4.4.2. The **General Employment** area is located in the interior of the Secondary Plan area as shown on Map 11.4.A.
- 11.4.4.3. The purpose of the **General Employment** designation within the Secondary Plan area is to:
- a. accommodate uses that do not require higher profile locations;
  - b. provide locational opportunity for industrial development which may require outside storage or be undertaken outdoors; and,
  - c. provide opportunities for a broad range of lot sizes and a diversity of building forms in order meet the needs of any size business or industry.

#### **Development Policies**

- 11.4.4.4. In addition to uses that would be permitted in the **Prestige Employment** area pursuant to Policy 11.4.2.5, the **General Employment** area shall permit uses which require

outside storage or which would be undertaken outdoors. These uses would include the full range of processing, warehousing and storage operations and transportation and distribution facilities. The range of uses shall be specified in the zoning by-law.

- 11.4.4.5. The standard applicable to the outside storage of goods and materials or any other activity not contained within a wholly enclosed building shall be implemented in the zoning by-law. Storage areas, or other external activities, shall not be permitted abutting a street. Screening will be required and the percentage of lot area devoted to such uses will be specified. Site specific zoning exceptions, in respect of such standards will be evaluated on their merits, in accordance with the policies of this plan.
- 11.4.4.6. The following building types are permitted in **General Employment** areas shown on Map 11.4.A pursuant to policies in Section 9.2.3:
- a. Employment/Industrial buildings;
  - b. Low-Rise buildings;
  - c. Mid-Rise buildings; and,
  - d. Gas Stations.

### 11.4.5 Service Nodes

- 11.4.5.1. Service nodes are to be located at the intersections of arterial and or collector roads.
- 11.4.5.2. The purpose of Service Nodes is to:
- a. provide for the day to day convenience and service needs of businesses, industries and their employees;
  - b. ensure that service opportunities are provided at convenient, easily accessible locations throughout the Employment Area; and ,
  - c. ensure that these facilities are comprehensively designed in order maximize functional efficiency and visual amenity.

### Development Policies

- 11.4.5.3. Service nodes are not shown on Map 11.4.A to this plan. Acceptable sites will be identified and zoned either through site specific zoning amendment applications or through the Block Plan process.
- 11.4.5.4. Service nodes shall generally be located at intersections of arterial and or collector

roads. Other areas that are conveniently located or are predominantly devoted to another use, such as an office complex, or a hotel may also be considered.

- 11.4.5.5. Prior to approving an application to permit a service node, the approval of an urban design plan and traffic impact study may be required.
- 11.4.5.6. The maximum area of a service node shall be approximately 1.2 ha. A service node may exceed 1.2 ha if the site is to be developed in conjunction with a predominant use such as an office complex, or hotel.
- 11.4.5.7. Uses permitted shall provide for the day to day convenience and service needs of businesses, industries and their employees. Retail uses which service the convenience needs of employees (ie. convenience retail store or pharmacy) and the needs of business (ie. business supplies) are considered to conform. The detailed range of uses shall be established in the zoning by-law.
- 11.4.5.8. Notwithstanding the foregoing, in the Highway 400 North Employment Area, service uses shall be encouraged to locate in the Employment Area Activity Centre along Kirby Road, or in the Employment/ Commercial Mixed-Use Area in the northwest quadrant of Jane Street and Teston Road. However, consideration may be given, subject to the policies of this section, to permitting some limited service uses which will generally be located in mixed-use buildings, where feasible. Service stations may be located as part of a Service Node, however they will be limited to one per arterial road intersection.

#### **11.4.6 Employment Area Activity Centre**

- 11.4.6.1. Employment Area Activity Centres are located both east and west of Highway 400, and east and west of the lands in the Greenbelt Natural System Area, on lands fronting on Kirby Road.
- 11.4.6.2. To permit the development of a concentration of uses designed to provide services to the surrounding employment area and adjacent areas, in addition to the uses permitted in the underlying land use designations.
- 11.4.6.3. To provide opportunities for private development and a public realm which contribute to the creation of an attractive, urban environment with a strong pedestrian orientation.

## **Development Policies**

- 11.4.6.4. In addition to the permitted land uses in the underlying land use designations, a broad variety of uses including retail and service commercial uses, hotels, public, non-profit and institutional uses, cultural, and entertainment and social facilities shall be permitted where such uses provide a service to the surrounding employment area and contribute to the creation of an urban environment with a strong pedestrian orientation. However, the following uses shall not be permitted:
- a. individual retail uses in buildings with ground floor plates in excess of 929 square metres in size;
  - b. drive-through facilities, gas bars/service stations, either stand alone or associated with other permitted uses; and,
  - c. any uses which require outside storage of goods and materials.
- 11.4.6.5. The additional uses permitted in the Activity Centre shall generally be located as part of mixed-use buildings which generally also accommodate uses permitted by the underlying land use designations. Mixed-use buildings are a minimum of two storeys in height. Consideration may be given to the location of the permitted uses in single-use buildings, provided that such buildings contribute to the creation of a strong pedestrian orientation, including a building height equivalent to a minimum of two storeys. However, where single-use buildings are for retail or service commercial uses, they shall not occupy more than 25% of the frontage of anyone side of Kirby Road in the Activity Centre designation.
- 11.4.6.6. To enhance the pedestrian orientation of permitted development in this area, the zoning by-law will establish:
- a. minimum and maximum setbacks, heights and densities, limitations on the location of parking areas and other standards to ensure that buildings and their primary entrances are designed to be located close to and fronting on Kirby Road, and to provide interest and comfort at ground level for pedestrians;
  - b. parking regulations which establish maximum parking requirements and permit on-street parking or municipal parking to be utilized to meet parking standards; and,
  - c. height and density bonuses for the use of decked or underground parking or contributions to the creation of municipal parking spaces.
- 11.4.6.7. The urban design guidelines for the area will reinforce the direction with respect to the

creation of a pedestrian-oriented environment including guidelines with respect to the design of building facades facing Kirby Road; the size, location, and design of parking areas; and the location and design of loading and garbage facilities. In addition, the urban design guidelines will provide designs for Kirby Road which will recognize its role and function as a multi-purpose street that is both a transportation corridor, with the potential for an interchange at Highway 400, and a pedestrian-oriented place, including wide sidewalks on both sides of the road, buildings with active facades, including primary windows to provide visibility to and from the street, enhanced street trees, and other landscaping, on-street parking and pedestrian lighting.

#### **11.4.7 Land Use - Commercial/Employment Mixed-Use Areas**

11.4.7.1. **Commercial/Employment Mixed-Use** areas are located at the intersection of arterial roads adjacent to residential areas, or recognize existing commercial areas.

11.4.7.2. To permit a range of large-scale uses, such as institutional uses, places of worship, hotels and banquet halls, as well as a range of small scale commercial facilities, in addition to employment uses, to be located at a development node which serves the employment area as well as through traffic and adjacent residential areas.

#### **Development Policies**

11.4.7.3. In addition to the permitted land uses in the **Prestige Employment** designation, the **Commercial/Employment Mixed-Use** designation permits a range of small scale retail and service commercial uses designed to serve through traffic, as well as the surrounding area, such as restaurants, convenience stores, pharmacies and business supply uses. However, the following uses shall not be permitted:

- a. individual retail uses in buildings with ground floor plates in excess of 929 square metres in size; and/or,
- b. any uses which require outside storage of goods and materials.

11.4.7.4. The permitted uses shall generally be located as part of mixed-use buildings. However, single-use buildings shall be permitted on the lands fronting on Highway 400. Consideration may also be given to the location of the permitted uses in single-use buildings in the lands at the intersection of Jane Street and Teston Road, and King-Vaughan Road and Jane Street. However, where single-use buildings are for retail or service commercial uses, they shall not occupy more than 25% of the frontage of the lands in this designation on either Jane Street, Teston Road or King-Vaughan Road.

- 11.4.7.5. The zoning by-law will establish minimum and maximum setbacks, heights and densities, and other standards to ensure that buildings and their primary entrances are designed to be located close to and to front on Jane Street, Teston Road and King-Vaughan Road to provide interest and comfort at ground level for pedestrians. Buildings shall have active facades including primary windows to provide visibility to and from the street.
- 11.4.7.6. The lands designated **Commercial/Employment Mixed-Use** at the northwest corner of Jane Street and Teston Road, and at the south-west corner of Jane Street and King-Vaughan Road, may permit a food store within the designation without amendment to the Plan, subject to a land use study at a future date which determines the form and character of development east of Jane Street and demonstrates the need and appropriateness of the food store.
- 11.4.7.7. Notwithstanding the uses permitted in the **Commercial/Employment Mixed-Use** designation, no other use other than the existing service station uses on the west side of Highway 400 and extensions thereto, shall be permitted on the site. The existing uses and extensions of such uses shall be recognized in the zoning by-law.

#### **11.4.8 Urban Design**

- 11.4.8.1. Areas of urban design priority shall include the 400 series Highways, Prestige Employment areas, and Service Nodes, as well as Employment Area Activity Centres and Employment/Commercial Mixed-Use areas. These areas will serve as focal points and their design treatments shall reflect their importance in the Secondary Plan Area. Development abutting Highway 400 and the arterial roads will be required to respond in a design sense to the importance of these strategic locations in order to create a prominent City image. Development in the Prestige Employment areas, given their linear nature will focus on securing attractive streetscapes, which serve to connect to more prominent nodes. The design of the General Employment area lands will be directed toward creating attractive working environments while accommodating a wide range of uses. The design of the Prestige Employment- Office/Business Campus areas focuses on the enhancement of the relationship between the development and the lands in the Natural System of the Greenbelt Plan, as well as the creation of attractive private development and public streetscapes. Development in the Employment Area Activity Centre and the Employment/Commercial Mixed-Use Areas will reflect their role as focal points for the Highway 400 North Employment Area.

The lands along Highway 400 in the Highway 400 North Employment Area serve as the major northern Gateway to the City. Urban design guidelines will be prepared for this Area to ensure that development is designed in a manner which enhances the City's image and which reflects the prestige nature of the Employment Area. In particular, the guidelines will address the following:

### **Significant Enhanced Landscape Area**

- a. Where development fronts onto Highway 400, a "Significant Enhanced Landscape Area" has been designated on Map 11.4.A. The urban design guidelines will provide detailed direction on the extent and form of the required landscape, topographical, built form and architectural relationships to Highway 400.

### **Significant Interface Area**

- b. The Significant Interface Area designations on Map 11.4.A, identify significant intersections, which serve as "gateways" into the employment area. The guidelines will address not only the design parameters for the lands in the Employment Area, but also their relationship with lands at the intersection outside the Employment Area. The permitted uses in Significant Interface Areas shall be those in the underlying land use designations.

### **Character Road**

- c. Kirby Road is identified as a "Character Road" on Map 11.4.A. This reflects the fact that it is intended to be a multi-purpose street that is both a transportation corridor, with the potential for an interchange at Highway 400, and a pedestrian-oriented place. The urban design guidelines will provide designs for Kirby Road which will recognize its dual role including wide sidewalks, enhanced street trees and other landscaping, on-street parking and pedestrian lighting.

## **11.4.9 Environment**

11.4.9.1. Planning for new development and redevelopment in the Highway 400 North Employment Area, with respect to the environment, shall be carried out in accordance with the policies in Chapter 3, recognizing the following:

- a. The Potential Valley and Stream Corridor and Potential Environmental Feature designations on Map 11.4.A reflect the results of the environmental analysis

carried out as part of the background study prepared as a basis for the preparation of the Secondary Plan Area Land Use plan. The detailed studies to be carried out for the Block Plans will determine the actual extent of any environmental features and the requirements for their protection. No amendment shall be required to this Plan if changes are made to these designations arising from the Block Plan process. Where a valley, stream or feature is relocated or eliminated through the Block Plan process, the underlying land use designation shall apply.

- b. The Greenbelt Natural System Area reflects lands included in the Provincial Greenbelt Plan. Lands in this designation shall be subject to the policies in Section 3.4.2.

#### **11.4.10 Transportation**

- 11.4.10.1. Should the Province indicate that an East-West Corridor is Provincial policy, then provision for it could be provided in the Highway 400 North Employment Area through an amendment by the Region of York.
- 11.4.10.2. The need for at least one new interchange on Highway 400 has been identified as necessary to support the full development of the Highway 400 North Employment Area. The location of the interchange(s) will be identified through an Environmental Assessment. The “Interchange Study Area” on Map 11.4.A identifies areas where an interchange potentially could be located. Any development of lands which are subject to this designation will be limited to uses such as agricultural uses which will not restrict the location of a future interchange, until such time as the City is satisfied that sufficient information is available to determine more specifically the location of the interchange(s). In particular, new buildings and structures, other than expansions of existing buildings and structures, and new accessory buildings and structures, will be prohibited. Upon approval of the Highway 400 North Employment Area Plan, the City will immediately initiate discussions with the Ministry of Transportation and the Region of York regarding completion of an Environmental Assessment to provide additional interchanges with Highway 400 within the Highway 400 North Employment Area.

#### **Pedestrian and Bicycle System**

- 11.4.10.3. In the Highway 400 North Employment Area, the intent is to provide an integrated, multi-modal transportation network. A key component of this will be a comprehensive

pedestrian and bicycle system as follows:

- a. A pedestrian walkway and bikeway system (developed primarily within the public road right-of-way), and related facilities such as bicycle parking, shall be required in the Highway 400 North Employment Area on Map 11.4.A and integrated with the City's Pedestrian and Bicycle System Master Plan. Pedestrian and bicycle facilities will be designed to provide connections to bus stops, and linkages between buildings, adjacent sites, surrounding areas and public streets, and particularly to transit facilities.
- b. A key part of the pedestrian walkway and bikeway system shall be a continuous, interconnected trail system within the Greenbelt Natural System Area. Where possible, recognizing the barrier created by Highway 400, this trail system will link to other environmental areas in the Employment Area, The City will cooperate with York Region, the Toronto and Region Conservation Authority, local interest groups, and private land owners to create this trail.

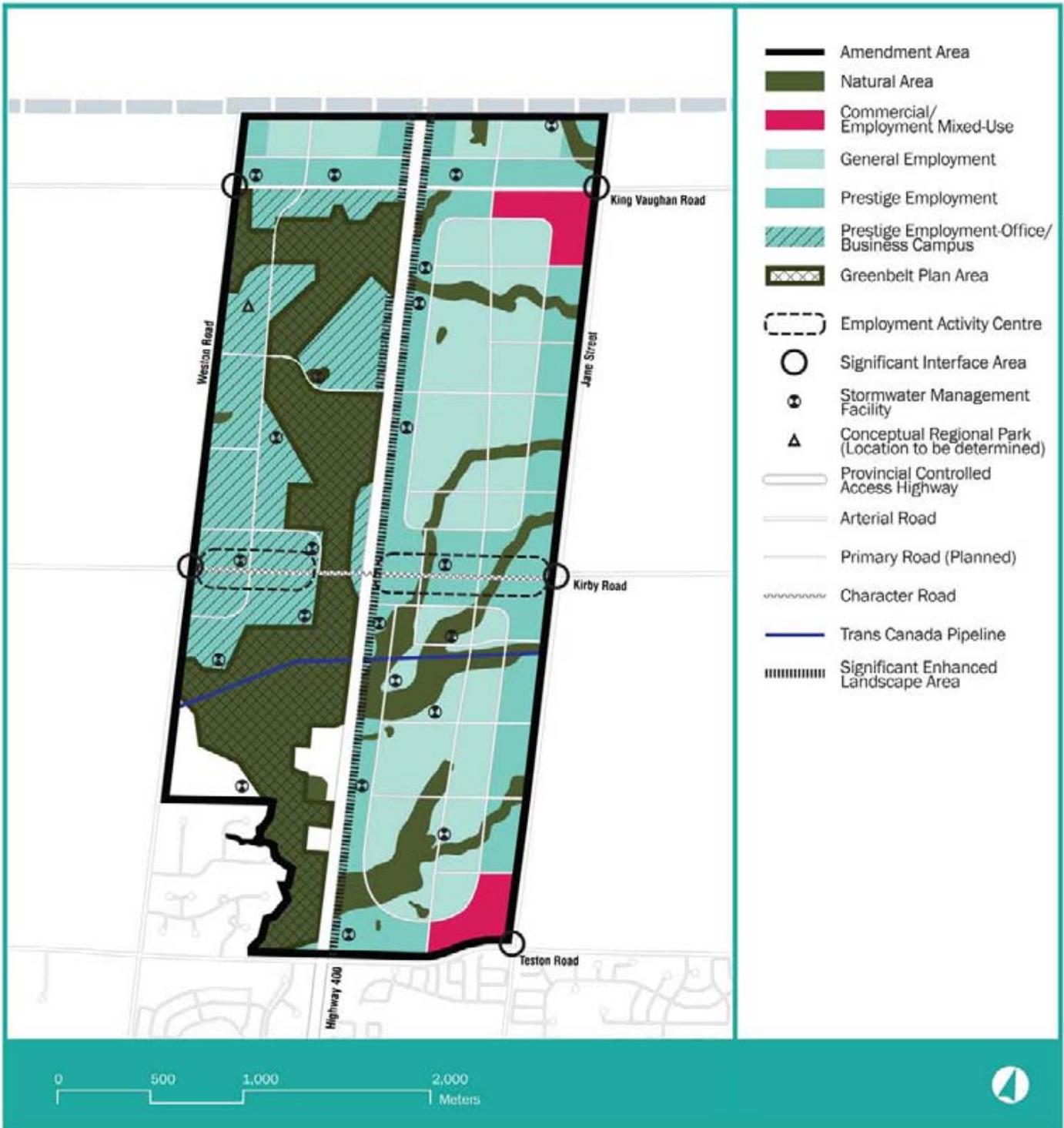
### **Public Transit**

- 11.4.10.4. In the Highway 400 North Employment Area, the intent is to provide an integrated, multi-modal transportation network. In support of that objective, in addition to the development of a pedestrian walkway and bikeway system, the City will actively work with the Region, developers, owners and tenants to develop and implement a traffic demand management plan including measures such as the promotion of public transit to employees, measures which support transit such as priority signalling and maximum parking standards, and assisting in organizing and promoting car pooling.

### **11.4.11 Stormwater Management**

- 11.4.11.1. The Stormwater Management Facility designation on Map 11.4.A represents general locations for stormwater management facilities within the Highway 400 North Employment Area. Notwithstanding the location of these designations, such facilities shall be permitted in all designations, will be located and designed to the satisfaction of the City and TRCA, and should be integrated with the open space and trail system.

▼ Map 11.4.A  
 Highway 400 Employment Lands



## 11.5 Kipling Avenue Corridor

### 11.5.1 Development Principles and Objectives

The following principles support the Vision, and represent the fundamental objectives to be considered in the decision-making related to all public and private sector initiatives within the Kipling Avenue Corridor Area:

#### **Principle:**

#### **To foster a sense of place.**

- 11.5.1.1. Development shall contribute to a defined identity and a 'sense of place' for the Kipling Avenue Corridor Area, and the area should be planned as a destination in its own right, defined by a strong pedestrian realm and a healthy mix of uses. The following are the objectives to foster a sense of place:
- a. To design a consistent streetscape and a cohesive approach to architecture that draws upon existing assets, including heritage structures and landscapes.
  - b. To develop an attractive, pedestrian-oriented street that enhances the public realm with intimately-scaled open spaces, active at-grade uses, gathering spaces, and safe and well-connected boulevards and pathways, resulting in a safe, animated and walkable environment.
  - c. To ensure building design is appropriately scaled to frame public spaces and create a comfortable pedestrian environment.
  - d. To fill in the gaps in the urban fabric to create a continuous, attractive pedestrian environment and an appropriately-scaled urban form, which enhances the character of the street.

#### **To protect heritage resources.**

- 11.5.1.2. All new development shall respect Kipling's heritage assets and contribute to its heritage character. The following are the objectives to protect heritage resources:
- a. To ensure that the scale and massing of new development contributes to the heritage character and attributes of the area.
  - b. To ensure that open spaces, green corridors and trail connections are considered part of the area's heritage resources.

#### **To create a supportive transportation network.**

- 11.5.1.3. The design of the transportation network should support a range of users, including pedestrians, cyclists, public transit and private vehicles. The following are the objectives to create a supportive transportation network:

- a. To develop a street network which is well-connected and is supported by public transit to facilitate vehicular movement which is safe and efficient and reduces traffic congestion.
- b. To ensure that new development supports investment in public transit through increased densities and efficient design.
- c. To design streets and the public realm to ensure safe relationships between pedestrians, cyclists and vehicles.
- d. To provide a safe and well-connected pedestrian and cycling network that facilitates movement throughout the area as well as to surrounding areas in Vaughan.
- e. To mitigate areas of transportation conflict, such as at the railway crossings and along railway corridors, to facilitate improved circulation throughout the area.

**To provide a mix of uses.**

11.5.1.4. The Kipling Corridor area shall accommodate and encourage a mix of uses to support a vibrant community and healthy economy. The following are the objectives to provide a mix of uses:

- a. To establish a mixed-use environment which includes compatible residential, commercial and institutional uses to encourage residents to live and work in the area.
- b. To ensure an appropriate transition between uses and different building types.
- c. To provide a variety of housing types to accommodate a broad demographic population, including a complimentary range of heights, unit types and sizes.

**To enhance a green environment and open space system.**

11.5.1.5. The Kipling Avenue Corridor Area shall be made “green”, including public and private open spaces, streets, pathways and trails. The following are the objectives to enhance a green environment and open space system:

- a. To ensure that green open space corridors (i.e. the Humber River and the Rainbow Creek Corridors) are conserved, enhanced and well-connected.
- b. To ensure that development will be located outside of hazard lands and that the risk to life and property will be minimized to the furthest extent possible.
- c. To provide trails/paths throughout the green corridors, public parks and neighbouring communities that are continuous, accessible, well-linked, and facilitate movement throughout the Kipling Corridor area.

- d. To create a park and open space network that supports an array of both passive and active recreational uses, as well as varying community needs.
- e. To ensure the Fairgrounds remain a key open space amenity for the community, are well-connected to surrounding neighbourhoods, and are designed to encourage year-round activities, while respecting their heritage significance.

**To achieve a critical mass.**

- 11.5.1.6. The population density on Kipling Avenue should support its role and function as a vibrant neighbourhood within the City of Vaughan. The following is the objective to achieve a critical mass:
  - a. To plan and design the Kipling Avenue area to support intensification and compact urban form, in order to provide residents with a variety of uses and activities within walking distance, support local amenities, increase transit options, and ensure a vibrant street life.

**11.5.2 Urban Form - Precincts**

This section identifies the desired future character and function of the various distinct precincts within the Kipling Avenue Corridor Area. The definition of the precincts is a result of both the vision generated for Kipling Avenue Corridor, and the analysis generated through the Woodbridge Heritage Conservation District Study.

The character identified for each precinct is prescriptive. The intent is to guide future development and public investments, in a way that serves to: a) conserve significant elements of the existing character; and b) to reinforce the future vision. The precincts are shown on Schedule 11.5.B.

**Kipling Avenue**

- 11.5.2.1. Kipling Avenue should be:
  - a. A road that maintains a significant function as part of the public realm in the region, providing pedestrian access and views to significant public destinations and amenities, including: Woodbridge Core, the Fairgrounds, and the open spaces associated with the Humber River and the Rainbow Creek.
  - b. A picturesque heritage Avenue, with a significant tree canopy and buildings that front directly onto Kipling. There should be active at-grade uses and buildings should be setback, offering a landscaped front yard.

- c. A pedestrian friendly street, that provides pedestrian connections to a range of parks, open spaces, trails and walkways, and to commercial and residential areas, throughout the area.
- d. A residential character and scale, with buildings that average 2-3 storeys, and include some commercial and live-work uses.
- e. An area of Vaughan that displays and conserves significant heritage buildings, structures, and landscapes.

### **Woodbridge and Kilpling Avenue Intersection**

11.5.2.2. Woodbridge Avenue and Kipling Avenue Intersection should be:

- a. A main Street that extends along Woodbridge Avenue, east to Islington Avenue and west to Kipling Avenue, with a mix of at grade pedestrian oriented uses such as retail, offices and restaurants and a variety of other uses above grade, mostly residential.
- b. A street wall of buildings between 4 and 6 storeys at the intersection.
- c. A welcoming environment where small-scaled storefronts open directly onto the sidewalk and provide pedestrians with a variety of storefronts, which change every few steps.

### **Fairgrounds Area**

11.5.2.3. The Fairground Area should be:

- a. An area to be maintained as a significant open space for the community.
- b. A place that conserves existing significant greenery and tree canopy, that is part of the rural character, and should extend to the surrounding context.
- c. An enhanced open space as a destination to several important pedestrian connections and trails.
- d. Buildings located on site are maximum three storey structures that are characteristic of a rural character and setting.
- e. A centre programmed throughout the year with fairs, events, and functions, (e.g. a farmer's market) that attract residents and visitors from Vaughan and beyond.
- f. Enhanced as an important connection and contributing forest landscape to the natural conservation area directly to the east.
- g. Porter Avenue, as well as new public connections around the perimeter, are designed gateways and treated as significant elements in the overall open space system of the area.

### **Rainbow Creek Neighbourhood North**

11.5.2.4. Rainbow Creek Neighbourhood North should be:

- a. A new neighbourhood that is intimately connected to both Kipling Avenue and the open spaces surrounding Rainbow Creek, through a network of pedestrian connections and streets.
- b. An area of mostly residential uses.
- c. A neighbourhood that can accommodate a significant residential population, through higher density developments that are adjacent to significant green areas.
- d. An area in which all streets are welcoming to pedestrians, with active grade related uses, and ample landscaping.

### **Rainbow Creek Neighbourhood South - Central and South West Areas**

11.5.2.5. Rainbow Creek Neighbourhood South provides a transition in building scale and height towards the lower scale residences on Woodbridge Avenue and Kipling Avenue, with mid-density development.

### **Rainbow Creek Neighbourhood South - South East Area**

The concept plan for these lands contemplates a block townhouse development with a minimum 8 metre central pedestrian sidewalk and landscaped area.

11.5.2.6. The following are general minimum policy requirements for the development of these lands:

- a. The lands must develop comprehensively, considering the best future development scenario for the easterly adjacent properties along Kipling Avenue and the westerly industrial properties along the river corridor, providing opportunities for increased north/south and east/west pedestrian circulation, and connections to Kipling Avenue, Woodbridge Avenue and the river valley.
- b. Notwithstanding policy 9.2.1.1, the permitted residential uses for these lands, with the exception of the contributing heritage industrial building, include block townhouses, stacked townhouses, street townhouses, and semi-detached dwellings, with a maximum FSI of 0.70.
- c. The development of these lands must be respectful of the contributing heritage industrial building at the end of Burton's Lane, as identified in Map 11.5.O. The recommended use for the heritage building include small scale, grade related

- d. neighbourhood amenity uses, such as daycare, service commercial, or art studios.
- e. The development of these lands must allow for a public pedestrian connection that is visually and physically accessible from Woodbridge Avenue and the Rainbow Creek Neighbourhood South, to Parkside Drive and the Rainbow Creek Neighbourhood North. The pedestrian connection should take the form of a generous landscaped promenade between the townhouse developments, as a
- f. shared neighbourhood amenity and public “greenway”. Any proposed townhouse development must front the promenade.
- g. The central promenade, where it is located between residential development, should include a continuous minimum 1.5 metre sidewalk and landscaped area which together should form the minimum 8 metre promenade.
- h. The minimum 1.5 metre sidewalk must be constructed within the first phase of the Low Density Residential Neighbourhood development.
- i. The promenade and heritage building should remain clearly visible from Woodbridge Avenue, and a historic recognition of the heritage building’s significance and the old C.P. Rail line should be considered as part of the promenade open space fronting Woodbridge.
- j. Vehicular access to the residential units must be located at the rear, through a rear lane access.

### **Phasing of Development for the Rainbow Creek Neighbourhood**

Currently, the industrial properties within the study area are considered stable, and development of the Rainbow Creek precinct, as envisioned in the plan, provide the best opportunity within the Study Area to create a complete neighbourhood. Build-out of the industrial lands, therefore, may be a long term phased scenario and dependent on future residential development pressures.

- 11.5.2.7. Phasing of Development for the Rainbow Creek Neighbourhood is subject to the following policies:
- a. Partial build-out of the neighbourhood north and south is expected and should be achieved through a comprehensive planning process in order to ensure that the character and function of this precinct is achieved.
  - b. The successful build-out of this neighbourhood should happen as a phased scenario and is dependent on the collaboration of all landowners in terms of implementing the necessary infrastructure, such as roads and parks, in a form that

serves to benefit the overall plan in the short and long term and does not restrict future development opportunities.

- c. Issues related to road access and connections for example, that are key to good future development should be resolved prior to site plan approval.
- d. Land owners may be required to enter into Developer Group Agreements prior to having a development application considered. This will ensure that the front ending costs and land dedication for desirable infrastructure such as key road connections and parks are achieved and landowners of early phase development can be compensated in the future.

### **11.5.3 Block Pattern, Street Network and Linkages**

11.5.3.1. The block pattern and street network of Map 11.5.C and Map 11.5.D defines a pattern, grid and hierarchy of streets, and linkages to guide the siting and orientation of future developments and development blocks. For the most part, the plan reinforces the existing structure of blocks and streets. It is mainly along the western side, between the Rainbow Creek and the railway that the need for a new block pattern and street network is identified. As development opportunities arise within the Corridor, achieving permeable block patterns and a fine network of vehicular and pedestrian connections is essential. The pattern, grid and hierarchy of streets, as illustrated in Map 11.5.D, should be implemented through the development process

- 11.5.3.2. The City encourages the consolidation of land parcels to facilitate comprehensive development particularly in the Rainbow Creek neighbourhoods. In particular the City shall be satisfied that:
- a. good block patterns, street connections and pedestrian linkages are established to achieve the goals of the vision;
  - b. development proposals consider the comprehensive requirements of the neighbourhood and adjacent lands in establishing road and pedestrian connections;
  - c. streets and blocks are configured to provide building frontages and addresses to the street and encourage parking access at the rear via a rear lane;
  - d. opportunities to increase public access and linkages to parks and open space amenities are achieved as part of good block development;
  - e. private streets and laneways are discouraged, but if allowed, development should provide public pedestrian access, connect to other streets or laneways and be

visually appealing and pedestrian friendly; and,

- f. to consolidate open space to create a more substantial public amenity where this opportunity arises.

- 11.5.3.3. The following sections describe the recommended character, function and hierarchy of new and existing streets within the study area. Further detail and description is given for each street category within the Corridor in The Appendix: Detailed Streetscape Guidelines.

### **Primary Streets**

- 11.5.3.4. Primary Streets are mixed-use commercial/residential streets and are considered main access roads within the street network.
- 11.5.3.5. Primary streets should function as two way connections with R.O.W.'s from 20 metres to 26 metres, accommodate various modes of transportation such as on street parking, transit and bicycles, and be provided with a higher quality streetscape treatment.
- 11.5.3.6. The following policies apply to Kipling Avenue North and South:
  - a. Kipling Avenue North and South should function as the main primary street within the Corridor;
  - b. A 26 metre R.O.W., should be maintained for Kipling Avenue north and south. The protection of this R.O.W. width is beneficial for the Avenue in terms of providing opportunities to increase the street tree canopy, and enforce the “green character” prescribed for Kipling Avenue and the Corridor in general;
  - c. The R.O.W. width should accommodate a double row of street trees and a generous pedestrian zone within the boulevard, along the majority of Kipling Avenue;
  - d. The 3.0 metre building setback beyond the R.O.W. should allow for future built form to be more in keeping with the existing heritage building setbacks and provides an additional opportunity for private landscape enhancements;
  - e. The heritage character of this street should be emphasized and enhanced, and the streetscape designed to attract a greater pedestrian presence, as per policy 11.5.2.1; and,
  - f. Additional landscape enhancements such as double street tree planting, may not be feasible at pinch points where existing heritage buildings fall within the 26 metre R.O.W.

- 11.5.3.7. The following policies apply to Kipling Avenue South:
- a. Kipling Avenue, south of Woodbridge Avenue, should have a wider roadway width, with additional travel lanes to accommodate the volume of traffic entering the avenue from Highway 7.
  - b. Bike lanes should continue to run south along Kipling Avenue to Highway 7.
  - c. The east bike lane is envisioned to have a dedicated curb edge or rumble strip to the adjacent travel lane to make peak hour on-street parking possible on the east side of the avenue, while protecting the bike lane.
  - d. The widened roadway width will allow for only a single row of street tree planting within a 4.5m boulevard.
- 11.5.3.8. The following policies apply to Woodbridge Avenue East:
- a. Woodbridge Avenue, east of Kipling Avenue, should function in the same way as Kipling Avenue in terms of creating an attractive pedestrian oriented environment, and supporting an enhanced commercial presence and character.
  - b. Woodbridge Avenue, between Kipling Avenue and Islington, should maintain a 20m R.O.W. because of the location of existing heritage contributing buildings. There are a significant amount of buildings along this avenue that are considered contributing to the heritage character of the avenue and the area, many of which have minimal or zero setbacks to the 20 metre R.O.W. These buildings should remain in situ, which affects the feasibility of the 26 metre R.O.W. allowance.
  - c. The recommendations of the Woodbridge HCD Study is such that, Woodbridge Avenue at the commercial core should reflect the tight, pedestrian oriented character of the existing 20 metre R.O.W. street cross section.
  - d. Although the stretch of Woodbridge Avenue, from Islington to Kipling Avenue is envisioned as a future marked bike route in the City of Vaughan Bike Master Plan, incorporating bike lanes would affect having on-street parking as part of the Woodbridge Avenue cross section.
  - e. On-street parking should be encouraged along Woodbridge Avenue as a key element to enable existing and future businesses to flourish and is necessary to the success of the commercial context of the area.
  - f. In terms of providing bike access, Woodbridge Avenue, from Islington to Kipling Avenue, should be considered under the “Class 3 Facility: Signed Only Routes” category as identified in the Bike Master Plan, which defines an on-street signed bicycle route used to “form a connection to the bike network”, (see section 4.4.3

of the City of Vaughan's Pedestrian and Bicycle Master Plan). The existing and future traffic volumes for Woodbridge Avenue are considered to be moderate and is envisioned to have slower moving traffic, as it is viewed as a pedestrian priority area. This would make a signed bike route manageable along Woodbridge Avenue.

- 11.5.3.9. The following policies apply to Woodbridge Avenue West:
- a. The protected 26 metre R.O.W is feasible on Woodbridge Avenue, west of Kipling Avenue and should be maintained, which allows for enhanced streetscaping within the boulevard, and should function in the same way as Kipling Avenue in terms of creating an attractive pedestrian oriented environment.

### **Secondary Streets**

- 11.5.3.10. Secondary Streets should function similarly to primary streets, but are mainly residential in character. These streets should also function as two way connections with a minimum 20 metre R.O.W.
- 11.5.3.11. Secondary Streets should be given a higher quality streetscape treatment and allow for bike lanes and on-street parking.
- 11.5.3.12. The following policies apply to Parkside Drive:
- a. Parkside Drive is envisioned as a new secondary north/south street that runs parallel to Kipling Avenue, mainly servicing the proposed Rainbow Creek neighbourhood north. Parkside Drive enables the creation of new frontage to both the Rainbow Creek Valley public open space system, and to new development blocks that support a High-Rise Residential neighbourhood.
  - b. This street should function as the key vehicular spine for the new neighbourhood and the main connection to Kipling Avenue and the river valley.
  - c. This should be treated as a grand street, with lush greenery, and ample space for pedestrian interaction.
  - d. As many points of access to Parkside Drive from Kipling Avenue should be achieved to increase the development potential for the industrial lands, as well as provide opportunities to redevelop existing adjacent properties.
  - e. The design of Parkside Drive shall include a high-quality pedestrian environment that links buildings, amenities and open spaces and provides east-west

connections to the river valley and Kipling Avenue;

- f. The design of Parkside Drive shall include wider sidewalks (2.5 metre) on both sides, that can accommodate additional street furniture, lighting, and public amenities associated with the proposed park;
- g. The design of Parkside Drive shall include a 3 metre setbacks to accommodate additional landscaping and front yards for grade related residential units; and
- h. The design of Parkside Drive shall include parking on both sides of the street, for visitors and park users.

### **Local Connector Streets**

- 11.5.3.13. The Local Connector Streets provide key east/west connections to Kipling Avenue and to the new Rainbow Creek neighbourhood.
- 11.5.3.14. These streets may vary in character depending on the allowable right-of-way, but should function typically in the same way as secondary streets.
- 11.5.3.15. The following policies apply to Rainbow Creek Road and Industry Avenue:
  - a. “Rainbow Creek Road” and “Industry Avenue” are two new east/west local street connectors that link Kipling Avenue to Parkside Drive.
  - b. Rainbow Creek Road, which crosses the rail corridor, should function as a key northern gateway to the new Rainbow Creek neighbourhood and as a connection to Meeting House Road. It is envisioned as a two way 20 metre R.O.W. with the same characteristics as Parkside Drive.
  - c. Industry Avenue, which is south of the rail corridor, should provide a two way connection to Kipling Avenue and the new neighbourhood. The right-of-way for this new street is restricted to 17.5 metre because of its adjacency to a heritage building, and provides only a single sided sidewalk condition.
  - d. The City may consider reduced pavement widths for this local road to accommodate additional sidewalk facilities, provided they are satisfied that vehicular accessibility to the new neighbourhood and the location of utilities are not compromised.

### **Special Character Streets**

- 11.5.3.16. The Special Character Street exists along two east-west connections that lead off from the intersection of Kipling Avenue and the railway.

- 11.5.3.17. The following polices apply to both Porter Avenue East and West:
- a. Porter Avenue East should continue to provide east bound access to the Fairgrounds.
  - b. Porter Avenue West is a new westbound street that should provide new connection to the Rainbow Creek open space system and residential neighbourhood.
  - c. The Special Character Street should be characterized by high quality landscaping, an emphasis on the pedestrian environment, and its function as a gateway to significant urban spaces.
  - d. Porter Avenue East should signalize and celebrate an entrance to the Fairgrounds.
  - e. Porter Avenue West should function as the main pedestrian draw towards new public amenities, open spaces, and residences that are part of the Rainbow Creek neighbourhood development.
  - f. Key aspects of the design include:
    - i. a double row of trees for Porter Avenue East, and enhanced landscaping on both sides of the street;
    - ii. a double row of street trees where the R.O.W. allows, along Porter Avenue West;
    - iii. wide sidewalks, with enhanced paving treatment;
    - iv. additional street furnishing and lighting;
    - v. designed gateway elements on Kipling Avenue; and,
    - vi. clear open views, to the valleys east and west.
- 11.5.3.18. The following policies apply to Porter Avenue East as a Special Character Street:
- a. Porter Avenue East is a local connector and should function as the main gateway to the Fairgrounds.
  - b. This is a two way street with a 20 metre R.O.W. and should be primarily pedestrian oriented, providing opportunities for on street parking and new building frontages that support a pedestrian priority environment.
  - c. This street should provide additional parking opportunities for the Fairgrounds.
  - d. This a Special Character Street and should be given a high quality streetscape treatment.
- 11.5.3.19. The following polices apply to Porter Avenue West as a Special Character Street:
- a. Porter Avenue West is considered a local connector street within the street network although it can only function as a one way out to Kipling Avenue due to C.P. Railway setback and sight-line restrictions at that location.

- b. Even though this street serves as a minor connector, Porter Avenue West is key to the realization of the “Kipling Avenue Junction”, and should function as a major east/west pedestrian and bicycle link from Rainbow Creek to the Fairgrounds and beyond.
- c. This is a Special Character Street that should be given the same high quality streetscape treatment as Porter Avenue East.

### **Residential Streets**

- 11.5.3.20. Residential Streets within the study area are neighbourhood streets that carry lower volumes of vehicular traffic.
- 11.5.3.21. These streets should be pedestrian oriented in character and should cater primarily to the safe movement of pedestrians as a priority.

### **Laneways**

- 11.5.3.22. Laneways should mainly provide vehicular access to parking and services.
- 11.5.3.23. They should be organized as internal connections within blocks that link to main streets, and should also be designed as opportunities for increased pedestrian linkages throughout the Corridor.
- 11.5.3.24. The following policies apply to Public Laneways:
  - a. Public Laneways should function primarily as vehicular connections, but should also be considered as opportunities for pedestrian linkages to Kipling Avenue and to new areas of development.
  - b. They should be designed with a special or enhanced streetscape treatment.
  - c. Historically Burton’s Lane, just north of Woodbridge Avenue, was the main connection to the Toronto, Grey and Bruce Railway Station, and to some of the first industrial buildings, such as the old Woodbridge Farmer’s Co-operative Company.
  - d. Burton’s Lane should function as a key public access lane from Kipling Avenue to the Rainbow Creek neighbourhood.
  - e. Burton’s Lane should be enhanced with higher quality materials and streetscaping to signify its heritage significance.

- 11.5.3.25. The following policies apply to Private Laneways:
- a. Private laneways are considered contributing to the overall street network as they provide pedestrian linkages throughout the area and should be designed to be pedestrian friendly, accessible, visible, and safe.
  - b. Private laneways should be located at the rear of buildings only as a means of accessing and connecting to parking facilities and services.
  - c. Furthermore, they should provide a continuous pedestrian connection to adjacent streets or open spaces.

### **The Pedestrian Priority Nodes**

- 11.5.3.26. The Pedestrian Priority Nodes are the key intersections along Kipling Avenue and should function as a focus for pedestrian activity.
- 11.5.3.27. In terms of street character and transportation hierarchy, pedestrians take priority within these nodes.
- 11.5.3.28. A high quality streetscape treatment should be given to these key intersections including special paving, lighting, street configuration, landscaping and street furniture.
- 11.5.3.29. The following policies apply to Woodbridge Avenue and Kipling Avenue Intersection:
- a. This is the main intersection along Kipling Avenue and should function as the main gateway to the Corridor.
  - b. Special consideration should be given to achieving a pedestrian oriented environment by reducing lane widths, providing on-street parking and bike lanes, increasing the opportunity for additional landscaping, and art; and integrating adjacent public open spaces.
- 11.5.3.30. The following policies apply to Porter Avenue East and West - The Junction:
- a. These streets, together, should create a pedestrian node, referred to as the “Junction”, which is a key east/west pedestrian connection along Kipling Avenue, and at the centre of the Corridor.
  - b. The same special consideration should be given to this node as the Woodbridge/ Kipling node in terms of achieving a pedestrian oriented environment.

#### 11.5.4 Pedestrian Priority Zones

Various areas along Kipling Avenue have been identified as places where the pedestrian and cyclists are considered to be a critical part of the infrastructure and place making, and where a balance between the role of the automobile and the requirements of pedestrians and cyclists, particularly in terms of traffic movement, should be established. The Pedestrian Priority Zones, shown on Map 11.5.D are established at key areas along Kipling Avenue in order to provide safe and comfortable walking and cycling environments. These zones are areas where higher levels of pedestrian activity and amenity are anticipated, such as the Woodbridge/Kipling Avenue Gateway or the Rail Junction Public Square; or where street crossing and safe pedestrian and bicycle movement across Kipling Avenue is a priority. The Pedestrian Priority Zone establishes a first preference to pedestrians, second preference to cyclists and third preference to automobiles.

- 11.5.4.1. In general, Kipling Avenue should no longer function only as a through street. The integration of the Pedestrian Priority Zones should be used as a method of street calming and traffic management.
- 11.5.4.2. Areas where the Pedestrian Priority Zone has been established, priority must be given to the pedestrian in terms of access, circulation, capacity and amenity.
- 11.5.4.3. Transportation infrastructure within these zones should accommodate transit and bicycle facilities and amenities where possible such as transit stops, shelters, bike racks, and safe passenger drop-off areas.
- 11.5.4.4. The street crossings should have a high level of design consideration in terms of traffic calming measures such as: signalization, clearly defined boundaries, marked crosswalks and signage, and special paving materials.
- 11.5.4.5. Traffic calming measures such as pedestrian signalization should be considered at the Meeting House Road intersection. Truck and pedestrian conflicts are currently a problem at this intersection.
- 11.5.4.6. Within these zones, the location of vehicular service entrances, and parking access and egress should be located away from the pedestrian realm to avoid interfering with pedestrian circulation and activity.

- 11.5.4.7. Special design considerations must be given to the ease of access and circulation for the handicapped within these zones.
- 11.5.4.8. Porter Avenue and Porter Avenue West are special pedestrian oriented streets within the Pedestrian Priority Zones and should be designed with a higher emphasis on landscaping and pedestrian amenity, such as special paving materials, roll curb edges to extend the boundaries of the public realm, pedestrian scaled lighting and special signage.
- 11.5.4.9. Open Space areas within these zones should be used for amenities that cater to the pedestrian and cyclist and should not be used for surface parking.
- 11.5.4.10. A strategy to replace the surface parking of the United Church site should be considered to extend the boundaries of the Rail Junction Public Square and include the church within the public square context. A future below grade shared parking scenario should be considered with adjacent properties as development around the church occurs.
- 11.5.4.11. Surface parking should not be permitted within, or directly adjacent to the Pedestrian Priority Zones, but opportunities for on-street parking should be encouraged and identified.

### **11.5.5 Heritage Conservation**

- 11.5.5.1. All new development shall respect Kipling's heritage assets and contribute to its heritage character. The objectives for the conservation of Kipling's heritage assets are:
  - a. To ensure that the scale and massing of new development contributes to the heritage character and attributes of the area.
  - b. To ensure that open spaces, green corridors and trail connections are considered part of the area's heritage resources.

### **11.5.6 Parks and Open Space**

The park and open space framework defines the desired future character and function of the various components that form the open space system, including parks, enhanced streets, trails, bike paths and civic uses. The intent of this framework is to guide private developments and public investments towards the creation of a consistent and inter-connected system of public and private open spaces and to identify opportunities to

maximize the amount of open space available. Parks are shown on Schedule 11.5.F.

- 11.5.6.1. The total park and open space requirement in relation to total proposed development is based on:
- a. 1022 units.
  - b. Park requirement based on 1ha/300units = 3.4 ha (8.4 acres)
  - c. Total proposed park space achieved in the Vision = 2.0 ha (5.0 acres), see Map 11.5.F: Proposed Parks, Parkettes and Public Squares.
- 11.5.6.2. Additional open space opportunities include:
- a. Trails
  - b. Fairgrounds, heritage landscape
  - c. Neighbourhood parks
  - d. Neighbourhood school open space amenities
  - e. TRCA lands - River Corridors and Conservation Lands
  - f. Public realm enhancements - streets and nodes
- 11.5.6.3. The total amount of parkland required in accordance with the proposed Vision is approximately 3.0 ha (7 acres). Currently, there are no opportunities along Kipling Avenue to achieve parks of a neighbourhood park scale or greater. The existing Fire Hall site would permit a small neighbourhood park at the south end of the corridor, and the industrial lands. The industrial lands provide the best opportunity to achieve a significant open space amenity and one that is central to the corridor.
- 11.5.6.4. There are additional open space opportunities that will not offset the parkland requirement of 1ha/300units, however, they should be considered as opportunities to increase the overall open space amenity for the Corridor. The development of a significant and connected trail system throughout the Corridor and better use of the river corridors provide significant open space opportunities. The City, in conjunction with the TRCA, should consider locations within the conservation lands where additional recreational uses can be accommodated without detriment to the natural system.
- 11.5.6.5. As well, there are opportunities to use a portion of the Fairgrounds land for recreational purposes, especially at the northern end where trail connections to the east and west river corridors can be integrated. Other recreational opportunities include the utilization

of the neighbouring schools such as the Christian District High School, which have significant open space and play facilities. The existing parks and community centres are also within a five to ten minute walking distance from Kipling Avenue. These open space opportunities are described in further detail in the following sections.

### 11.5.7 Open Spaces

The Kipling Avenue Corridor is characteristically defined by three major open space systems that constitute the larger mass of the open space network: the Rainbow Creek, the Humber River Corridor, and the Fairgrounds. These larger open space systems should play a major role in achieving a connected, accessible open space network as well as in achieving a connected neighbourhood fabric. The neighbourhood parks, parkettes, plazas and public squares should provide a fine grain layering of open spaces that support and enhance the larger system. Kipling Avenue should function as the central north-south spine that connects all aspects of the open space network.

11.5.7.1. Open spaces are shown on Map 11.5.E.

11.5.7.2. The following policies apply to Rainbow Creek and Humber River:

- a. Publicly accessible areas and trails within the valley corridors should be easily accessible, connected and clearly defined in order to protect the natural flora and fauna of the conservation lands.
- b. The trail system within the valleys should be well connected throughout, providing opportunities to partake in views, key features and destinations within the system as well as providing opportunities to commute to the various adjacent neighbourhoods.
- c. The industrial lands located within the TRCA floodplain boundaries shall form part of the Rainbow Creek public open space, allowing for increased opportunity for new trails and additional open space amenity as described in the following guideline.
- d. New open spaces within the industrial lands should be considered and enhanced as opportunities to increase the publicly accessible neighbourhood park amenities with compatible recreational uses such as a children's play area connected to the trails.
- e. The existing mature tree canopy should be protected and enhanced at every

opportunity, especially along Rainbow Creek where residential development is being proposed. Other significant opportunities for enhancement exist around the Fairgrounds and should be considered in conjunction with enhancing the trail system. Any form of enhancement to the natural system within TRCA jurisdiction must be undertaken in accordance with the TRCA Act.

- f. Trail heads and access points to the valley corridors should be clearly defined and signed from Kipling Avenue.
- g. Bike parking racks should be provided in the new parks, at the entrance to the valley trails, and along Kipling Avenue where possible.

11.5.7.3. The following policies apply to Kipling Avenue:

- a. Kipling Avenue should be the focus for beautification and enhancement of the public realm, with key pedestrian nodes such as the Woodbridge/Kipling Avenue intersection and the Kipling Avenue Junction as the main focus of social activity. The Avenue should be defined by a cross section that is organized to accommodate at a minimum, enhanced landscaping and street furniture, a comfortable pedestrian walking zone and a zone for retail and café spill-over.
- b. This would include a defined palette of materials that contribute to creating an overall cohesiveness and character for the Corridor such as street signage, benches, garbage and recycling, and pedestrian scaled lighting.
- c. Materials used along the Corridor should be of high quality to provide longevity to the streetscape and should allow ease of maintenance and servicing. The style of materials should strengthen the identity for the Corridor and complement the existing heritage fabric.
- d. Where possible, new streetscaping should integrate any existing mature trees within the streetscape master plan and enhance all heritage landscape frontages with additional planting.

11.5.7.4. The following policies apply to The Fairgrounds:

- a. A new landscape design should be applied to the Fairgrounds that maintains the existing open field, enhances the existing uses, and accommodates additional programmed activity.
- b. A new landscape should also define the racetrack boundaries and the Fairgrounds edges, enhance the surrounding existing mature canopy and conservation lands, and focus views to the river valley and to the eastern Woodbridge

neighbourhoods.

- c. A continuous trail connection should be accommodated around the perimeter of the Fairgrounds and connect to the surrounding neighbourhoods and to Kipling Avenue.
- d. Porter Avenue, which is the “gateway” entrance to the Fairgrounds, should be clearly defined by a double alleé of street trees and enhanced landscaping including pedestrian scaled lighting and signage. This streetscape should also be applied to the new “Porter Avenue West” to create a continuous east/west landscaped connection of open spaces.

- 11.5.7.5. The following policies apply to Neighbourhood Parks, Parkettes and Public Squares:
- a. The proposed and existing neighbourhood parks, parkettes, and public squares within the Corridor should be designed, programmed and furnished to meet the day-to-day open space needs of the community
  - b. Existing parks, parkettes, and public squares should be enhanced where possible, with additional trees, landscaping and furnishings that are in keeping with the overall vision and palate of materials for the Corridor.
  - c. There should be a hierarchy of use and programming established for the parks in order to ensure a range of uses to meet the needs of a variety of users and age groups.
  - d. New neighbourhood parks, parkettes, and public squares should be planned with enough flexibility to accommodate special or additional programming, such as community gatherings, and special events.
  - e. New neighbourhood parks, and parkettes, should be designed with high quality materials, innovative design, and the newest technology in play facilities.
  - f. Any park along Kipling Avenue should be visible and publicly accessible with clear views in and throughout and have an open face to Kipling Avenue.
  - g. Parks should have adequate lighting and signage for ease of use and access.
  - h. The public squares should be designed to support social events, street related events and activities and day-to-day gathering. These spaces should be the forum for art related activity and art display.
  - i. To ensure maximum utilization and presence in the community, the public squares should be fronted by animated uses with a high level of transparency, such as
  - j. restaurants, cafés and market venues.
  - k. Extending the pavement treatment from the public square onto the street can give

the space further prominence while delineating an extended space that can be occasionally utilized for large scale events.

- 11.5.7.6. The following policies apply to Pedestrian Nodes:
- a. There are two key pedestrian nodes along Kipling Avenue: the Kipling/Woodbridge Avenue Intersection and the “Kipling Avenue Junction”.
  - b. Pedestrian nodes occur at key junctures along Kipling Avenue and function as gateways to other surrounding open spaces and to the neighbourhoods east and west of Kipling Avenue.
  - c. Pedestrian nodes provide places to meet, congregate and increase social activity and pedestrian amenities. The nodes provide opportunities to celebrate the heritage character of the Corridor and are the focus for high quality enhanced streetscaping treatments.

## **11.5.8 Open Space Connections**

11.5.8.1. Open Space Connections are shown on Map 11.5.G.

11.5.8.2. A connected system of parks and open space should service all aspects of the Corridor and the surrounding neighbourhoods and should be made publicly accessible by a completed trail network.

### **Enhanced Streetscapes**

11.5.8.3. All new and existing neighbourhood streets should provide continuous streetscaping to contribute to the overall tree canopy.

11.5.8.4. Special enhanced landscape treatments should be applied to streets that function as gateways and/or are identified as pedestrian priority zones, such as Porter Road and an enhanced Porter Road West.

### **Pedestrian Connections**

11.5.8.5. Pedestrian connections should be provided within the public realm to ensure ease of access and permeability throughout the Corridor, such as midway connections through blocks and through all public parks and open spaces.

### **Pedestrian Crosswalks**

- 11.5.8.6. Safety measures should be applied at key pedestrian crossings along Kipling Avenue and Parkside Drive, including signal lights and enhanced streetscape treatments and road paving materials.
- 11.5.8.7. Pedestrian crosswalk materials and design should be in keeping with the overall Streetscape Master Plan Vision for Kipling Avenue.
- 11.5.8.8. Recommended locations for pedestrian crosswalks are at the intersections of Kipling Avenue and Meeting House Road, Kipling Avenue and Porter Avenue, Porter Avenue West and the new Parkside Drive, and Woodbridge Avenue and Kipling Avenue.

### **Bike and Pedestrian Trails**

- 11.5.8.9. All development shall implement the requirements of the City of Vaughan's Pedestrian and Bicycle Master Plan.
- 11.5.8.10. The new trail system proposed in the Vision should expand and enhance the Pedestrian and Bicycle Master Plan.
- 11.5.8.11. All existing and proposed parks and public open spaces within the Corridor should be connected to, and made accessible by, the new trail network.
- 11.5.8.12. The bike trail along Kipling Avenue should be incorporated in the Streetscape Master Plan for Kipling Avenue in the form of bike lanes, as proposed in the City of Vaughan's Pedestrian and Bicycle Master Plan.
- 11.5.8.13. Where possible, Neighbourhood and Valley trails should be accessible from Kipling Avenue and access points should be demarcated with trail signage that is in keeping with the overall vision for signage and wayfinding.

### **11.5.9 Landmark Sites, Gateways and Nodes**

Landmarks, nodes and gateways, as shown on Map 11.5.H, are the special places and features within the Corridor and are meant to define and enhance the character and identity of the Corridor as well as provide specific functions at strategic locations.

## **Landmark Sites / Signature Architecture**

- 11.5.9.1. The Landmark Sites, as identified in Map 11.5.H include both, locations for signature architecture and open spaces. Collectively, these sites should help define the Kipling Avenue Corridor as a distinct and special place within Woodbridge.
- 11.5.9.2. Obtaining LEED building standards and incorporating green building technologies for new developments/buildings will also contribute to the area as a distinct and special place.
- 11.5.9.3. The following policies apply to Open Space Landmarks:
- a. Most of the open space landmark sites are heritage features and are dispersed along the Corridor. These sites constitute a variety of forms from memorial sites at a small scale to the Fairgrounds and Rainbow Creek at a much larger scale. Most of the sites are not immediately visible from Kipling Avenue and provide an element of surprise and discovery along the Corridor. The accessibility to, and visual presence of, such sites should be enhanced through measures such as discrete signage, trail connections, enhanced landscaping, visible art elements, and mapping of the sites posted at the nodes along Kipling Avenue.
  - b. The open space landmark sites should be given special treatment in terms of quality of landscaping, materials and design that are unique to each open space.
  - c. Opportunities to enhance the distinct characteristics of the existing landmark sites should be considered as a first priority such as, enhanced landscaping and additional tree planting for the mature canopy of the northern gateway landmark site, which can then be augmented by an architectural feature within the landscape as a terminus to the Avenue.
- 11.5.9.4. The following policies apply to Signature Architecture Landmarks:
- a. Most of the significant built form landmarks are focused around the new neighbourhood development west of Kipling Avenue and the entrance gateway buildings at Porter Avenue West; as well as the Woodbridge/Kipling Gateway.
  - b. The design of the buildings should reinforce the distinct character of the area and showcase a high quality of architecture and materials.
  - c. The collective of new buildings proposed within the new Parkside Drive neighbourhood should together, display a landmark quality and stature that will contribute to the enhancement of the river valley and define the scenic character

of Parkside Drive.

- d. The built-form should demonstrate high quality materials and innovative design that allows for physical and visual integration and interplay of the valley landscape and that provides a human scale comfort for pedestrians.
- e. Built-form elements should include having numerous openings and mid-block pedestrian connections to the valley; visually accessible courtyards and gardens; as well as terraces, balconies, and green roofs as outdoor amenity space and as opportunities to landscape at each above grade level.
- f. The same should apply for the buildings proposed for the Woodbridge/Kipling Gateway, which collectively, should display a language of architectural features that defines and demarcates the node as a key focal point.
- g. Buildings should also work individually to provide a diversity and interest in architectural form that establishes a pleasing and comfortable built form and public realm.
- h. The buildings at the Woodbridge/Kipling Gateway should also contribute to the visual interest as seen from the Woodbridge Core with architectural elements and function to orient people towards the Core.
- i. Individually, each building should contribute to the architectural diversity and interest along Parkside Drive, and to the visual interest of the valley corridor and public open spaces, with architectural features that provide identifiable visual landmarks from the neighbourhood west.
- j. All buildings along the valley edge should therefore front the street as well as the valley and back building uses such as servicing and parking entrances should be visually hidden from the valley, parks and pedestrian trails.
- k. Buildings at the new Porter Avenue West gateway should display a corner articulation as a focal point to the entrance of Parkside Drive and to frame views west toward the river valley.
- l. Buildings at the “Rail Junction Public Square” should enhance the existing heritage United Church building and tower and other heritage properties, and should contribute with visually prominent elements to demarcate the public square along the Avenue and to frame views toward the river valley west.
- m. Buildings around the public square should have architectural features that enhance the quality of public areas, reinforce neighbourhood and district identity, and provide a sense of enclosure and orientation for the public realm.

- 11.5.9.5. The following policies apply to Primary Gateways:
- a. The primary gateways are located at the Woodbridge and Kipling Avenue Intersection, at the “Rail Junction Public Square”, and at Kipling Avenue North at the bend of the road. The primary gateways serve to define the boundaries of the Corridor in terms of establishing an identifiable character, place, and quality. Each gateway is considered as a distinct node with unique characteristics.
  - b. Woodbridge Avenue and Kipling Avenue Gateway: A framing of higher density development, signature architectural buildings, at-grade pedestrian activity, and a new pedestrian public square define this Gateway. Buildings should be articulated at the corners with primary entrances facing the street, with a high level of streetscaping to identify the gateway as a distinct place and to organize pedestrian and vehicular movement.
  - c. The Kipling Avenue Junction: This gateway celebrates the entrance to the new Parkside Drive neighbourhood and highlights the presence of two major open space systems within the Kipling Avenue Corridor. A new landscaped public square with flexible space to service the new neighbourhood and existing community, an enhanced streetscape treatment along Kipling Avenue, the Public Square, and the Porter Avenue entrances (east and west) defines the Kipling Avenue Junction Gateway.
  - d. Kipling Avenue North: The gateway treatment for the northern end of Kipling Avenue should represent the distinct mature landscape character of the Corridor, as the gateway is situated in the most treed area of the Avenue and at a key northern trailhead. A gateway feature can be represented in the form of enhanced landscaping or art and should incorporate trail signage and way-finding information.

- 11.5.9.6. The following policies apply to Secondary Gateways:
- a. The secondary gateways are located at Meeting House Road and at the intersection of the new Parkside Drive and Porter Avenue West.
  - b. The secondary gateways are key entryways into the neighbourhoods west and east of Kipling and are identified as places that require enhanced streetscaping at the intersections such as special landscaping and paving materials, enhanced frontages to corner heritage buildings, and signalized pedestrian crosswalks.

- 11.5.9.7. The following policies apply to Potential Public Art Sites:
- a. Public art can help to establish a unique identity and can contribute to enhancing the quality of the public realm in ways that conventional streetscape elements cannot. Key locations for public art that have been identified include the primary and secondary gateway nodes, especially those that are defined with heritage sites, new and existing parks, parkettes and public squares. There are also opportunities to display public art in the Fairgrounds, such as in the form of a gateway treatment or as a focal point to guide views out towards the valley.
  - b. Public art may include memorials, statues, water features, or individual art installations and can be incorporated in street furniture, signage and wayfinding, and should be located at visually prominent sites, public squares, parks, gateways, along sidewalks, at trail heads and/or in association with public and heritage buildings.
  - c. Art installations associated with heritage sites should contribute to and not take away from the heritage character, architecture and quality of the heritage building and landscape.
  - d. Art installations associated with the valley corridors should contribute to and not take away from the quality and character of the landscape and natural system, should not interrupt key views and vistas, and should not be disruptive of the natural flora and fauna of the valley lands.
  - e. Public art installations should serve as accents to the Corridor, as orienting devices for moving about, and as focal points in public open spaces.
  - f. The scale of art installations should correspond to the visual prominence of the site.

11.5.9.8. The following policies apply to Transit and Trail Node:

- a. Three nodes along the Avenue, as illustrated in Map 11.5.H, are identified as key locations for information kiosks for pedestrian and bike trails as well as bus routes, and should be incorporated in a signage and wayfinding strategy for the Avenue.

### **11.5.10 Public Realm Enhancements**

- 11.5.10.1. Public realm enhancements and increased parkland can be achieved through land use designations of Parks or public squares, or public realm improvements will be negotiated through the City's development processes and can be achieved through:
- a. The Private Sector - via private development agreements in the form of parkland

dedication, where such lands will be developed and conveyed to the City of Vaughan for parkland or public realm improvement purposes.

- b. The Public Sector - via ongoing municipal capital improvement programs aimed at parkland acquisition, parkland development, and streetscape improvements as identified in the Streetscape Master Plan, as well as other future streetscape initiatives.

### **11.5.11 General Land Use Policies**

The Kipling Avenue area is intended to become a “complete” neighbourhood, with a “fine-grain” mix of land uses located within walking distance. The area is also intended to accommodate a different range of housing types, which will derive in part from different densities.

- 11.5.11.1. Map 11.5.A delineates the boundaries and location of the following land use designations:

- a. Low-Rise Residential
- b. Mid-Rise Mixed-Use
- c. Mid-Rise Residential
- d. High-Rise Mixed-Use
- e. High-Rise Residential
- f. Private Open Space – The Fairgrounds
- g. Parks
- h. Natural Areas
- i. Site Specific Policies – The Floodplain

- 11.5.11.2. General land use policies that apply to all land use designations are:

- a. Commercial uses may include retail and office uses, and in neighbourhood designations may include ancillary commercial uses such as a corner store. Commercial uses, particularly retail should be located at grade level and face the street.
- b. Civic uses shall be limited to the following: government offices, libraries, community centres and places of worship.
- c. Commercial uses are only permitted as part of a mixed-use building.
- d. Structures that include commercial uses must be a minimum of 2 storeys and a minimum of 7 meters high.
- e. Parking areas and parking structures, with the exception of those located within

the **Low-Rise Residential** designation, must not be visible from the street.

Wherever possible parking lots should not be visible from public parks, parkettes or public squares.

- f. Drive-through facilities, gasoline service stations, gas bars and uses that require outdoor storage, are not permitted.

11.5.11.3. For the purpose of calculating density, Floor Space Index (FSI) shall be the Gross Floor Area of a development divided by the Gross Site Area. The deployment of allowable density must not result in buildings exceeding the maximum number of storeys or the maximum lot coverage.

11.5.11.4. The area included in the calculation of residential density shall include areas of a property reserved for setback requirements, the land for the buildings, private roads and driveways, parking areas and landscaping, amenity areas related to the specific development, or any other part of the property where development is restricted through policy or design, but shall exclude all other lands such as natural conservation areas.

### **11.5.12 Low-Rise Residential A**

11.5.12.1. The **Low-Rise Residential A** designation represents development within the healthy neighbourhood fabric.

11.5.12.2. Notwithstanding policy 9.2.1.5, the maximum building height within **Low-Rise Residential A** areas shall be 8.5 meters (2 storeys).

11.5.12.3. Notwithstanding policy 11.7.12.1, the minimum building height within **Low-Rise Residential A** areas shall be 4.0 meters (1 storey).

11.5.12.4. The maximum lot coverage within **Low-Rise Residential A** areas shall be 40%.

11.5.12.5. The minimum setback within **Low-Rise Residential A** areas shall be 4.5 meters.

11.5.12.6. Notwithstanding policy 9.2.1.6, the maximum density within **Low-Rise Residential A** areas shall be 0.5 FSI.

11.5.12.7. Notwithstanding policies 9.2.1.1 residential units shall be permitted within **Low-Rise**

**Residential A** areas identified on Map 11.5.A.

11.5.12.8. Notwithstanding policy 9.2.2.1, the permitted building types are Detached Houses and Semi-Detached Houses pursuant to Section 9.2.3.

11.5.12.9. Notwithstanding 11.7.12.8 above, permitted building types on the frontage facing Gate House Court shall include only Detached Houses to maintain the character of the existing residential court.

### **11.5.13 Low-Rise Mixed-Use**

11.5.13.1. The **Low-Rise Mixed-Use** designation represents a low scale, mixed-use area, with a healthy residential base that provides amenities and ancillary retail for adjacent neighbourhoods. This area is focused around Kipling Avenue, north and south of Woodbridge Avenue. The intent is to allow uses along Kipling Avenue to evolve and adapt, as the demand for commercial and retail space changes. This area is ideal for grade-related live-work units that may change overtime, as new residential neighbourhoods are established, towards the west.

11.5.13.2. Notwithstanding policy 9.2.1.5 the maximum building height within **Low-Rise Mixed-Use** areas shall be 11.0 meters (3 storeys).

11.5.13.3. The minimum building height within **Low-Rise Mixed-Use** areas shall be 8.5 meters (2 storeys).

11.5.13.4. The maximum lot coverage within **Low-Rise Mixed-Use** areas shall be 50%.

11.5.13.5. The minimum setback **Low-Rise Mixed-Use** areas shall be 3.0 meters.

11.5.13.6. Notwithstanding policy 9.2.1.6 the maximum density within **Low-Rise Mixed-Use** areas shall be 0.6 to 1.0 FSI.

11.5.13.7. Notwithstanding policy 9.2.1.1, the following broad variety of residential, commercial, recreational and open space, uses will support the heritage character of Kipling Avenue, and a healthy pedestrian environment:

- a. residential units and live-work units;

- b. small scale, grade related and service commercial, business and professional office, daycares, and eating establishments, as part of a mixed-use building with a maximum gross floor area of 100 square metres; and,
  - c. parks and public squares.
  
- 11.5.13.8. Notwithstanding policy 9.2.2.4 the following building types are permitted, pursuant to Section 9.2.3:
  - a. Detached House
  - b. Semi-detached House
  - c. Townhouses
  - d. Low-Rise Buildings
  
- 11.5.13.9. Kipling Avenue has the highest concentration of heritage resources within the Corridor. Notwithstanding the allowable height and setbacks, all new construction must abide by the guidelines for transitioning from heritage resources outlined in policy 11.5.1.2.
  
- 11.5.14 Low-Rise Residential B**
- 11.5.14.1. The **Low-Rise Residential B** designation represents a transition between the Mid-Rise Residential areas and areas with a lower-scaled residential use.
- 11.5.14.2. Notwithstanding policy 9.2.1.5 the maximum building height within **Low-Rise Residential B** areas shall be 11.0 meters (3 storeys).
- 11.5.14.3. The minimum building height within **Low-Rise Residential B** areas shall be 8.5 meters (2 storeys).
- 11.5.14.4. The maximum lot coverage within **Low-Rise Residential B** areas shall be 60%.
- 11.5.14.5. The minimum setback **Low-Rise Residential B** areas shall be 3.5 meters.
- 11.5.14.6. Notwithstanding policy 9.2.1.6 the maximum density within **Low -Rise Residential B** areas shall be 0.7 FSI.
- 11.5.14.7. Notwithstanding policy 9.2.1.1 residential uses are permitted in the areas designated **Low-Rise Residential B**.

- 11.5.14.8. Notwithstanding the foregoing, the lands immediately west of the 5 storey building located at 8026 Kipling Avenue are subject to the following:
- a. a maximum height of 2 storeys (8.5 metre) for residential units.
  - b. in addition to the uses permitted in policy 11.5.15.7, the following additional uses are permitted:
    - i. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building; and,
    - ii. parkettes and public squares.
- 11.5.14.9. Permitted uses for the heritage building include small scale, grade related neighbourhood amenity uses, such as a daycare, service commercial, or art studios.
- 11.5.14.10. Notwithstanding 9.2.2.3. the following building types are permitted, pursuant to Section 9.2.3:
- a. Semi-Detached House
  - b. Townhouses
  - c. Stacked Townhouses

### **11.5.15 Low-Rise Residential C**

- 11.5.15.1. The **Low-Rise Residential C** designation represents a transition between the Mid-Rise Residential areas and areas with a lower-scaled residential use.
- 11.5.15.2. Notwithstanding policy 9.2.1.5 the maximum building height within **Low-Rise Residential C** areas shall be 13.0 meters (4 storeys).
- 11.5.15.3. The minimum building height within **Low-Rise Residential C** areas shall be 8.5 meters (2 storeys).
- 11.5.15.4. The maximum lot coverage within **Low-Rise Residential C** areas shall be 80%.
- 11.5.15.5. The minimum setback **Low-Rise Residential C** areas shall be 4.5 meters.
- 11.5.15.6. Notwithstanding policy 9.2.1.6 the maximum density within **Low-Rise Residential C** areas shall be 2.0 FSI.

- 11.5.15.7. Notwithstanding policy 9.2.1.1, the following uses are permitted:
- a. residential uses.
  - b. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building; and,
  - c. parkettes and public squares.
- 11.5.15.8. Permitted uses for the heritage building include small scale, grade related neighbourhood amenity uses, such as a daycare, service commercial, or art studios.
- 11.5.15.9. Notwithstanding 9.2.2.3. the following building types are permitted, pursuant to Section 9.2.3:
- a. Semi-Detached House;
  - b. Townhouses;
  - c. Stacked Townhouses; and,
  - d. Low-Rise Buildings

### **11.5.16 Mid-Rise Mixed-Use:**

- 11.5.16.1. The **Mid-Rise Mixed-Use** designation represents the primary retail and commercial centre for the area. It is also intended to concentrate a significant residential population. It is focused at the intersection of Woodbridge Avenue and Kipling Avenue. The intent of the Plan is to establish an animated, pedestrian-oriented, and compact urban environment.
- 11.5.16.2. Notwithstanding policy 9.2.1.4 the maximum building height within **Mid-Rise Mixed-Use** areas shall be 13 meters (4 storeys) podium with 19 metre (6 storeys) maximum, stepping back on a 45 degree angular plane from the podium.
- 11.5.16.3. The minimum building height within **Mid-Rise Mixed-Use** areas shall be 8.5 meters (2 storeys).
- 11.5.16.4. The maximum lot coverage within **Mid-Rise Mixed-Use** areas shall be 60%.
- 11.5.16.5. The minimum setback **Mid-Rise Mixed-Use** areas shall be 0.0 meters (consider 3.0 meters when additional public realm is required).

- 11.5.16.6. Notwithstanding policy 9.2.1.6 the maximum density within **Mid-Rise Mixed-Use** areas shall be 3.0 FSI.
- 11.5.16.7. Notwithstanding policy 9.2.1.1, the following broad variety of residential, commercial, employment, institutional, civic, recreational and open space uses will support a “fine-grained” urban street and a healthy pedestrian environment:
- a. residential units;
  - b. small scale, grade related retail and service commercial, cafes and restaurant uses as part of a contiguous mixed-use building, with a maximum gross floor area of 200 square metres, where the street frontage width of any individual store front does not exceed a maximum of 10 metres;
  - c. small scale office uses that are a contiguous component of a mixed-use building, (located at grade);
  - d. public and institutional uses, public health facilities, places of worship, cultural and social facilities, (located at grade);
  - e. community facilities such as day care facilities, parks and public squares, (located at grade).
- 11.5.16.8. Notwithstanding policy 9.2.2.6 the following building types are permitted, pursuant to Section 9.2.3:
- a. Townhouses
  - b. Low-Rise buildings
  - c. Mid-Rise buildings
- 11.5.16.9. The street-facing ground level shall not include residential uses.
- 11.5.16.10. Stand alone, single use buildings are not permitted.
- 11.5.16.11. All buildings must be sited close to, and fronting, the street right-of-way.

### **11.5.17 Mid-Rise Residential**

- 11.5.17.1. The **Mid-Rise Residential** designation is intended to develop a residential neighbourhood that integrates residential and open space uses oriented to a scale that is pedestrian friendly.

- 11.5.17.2. Notwithstanding policy 9.2.1.5 the maximum building height within Mid-Rise Residential areas shall be 13 meters (4 storeys) podium with 19 metres (6 storeys) maximum, stepping back on a 45 degree angular plane from the podium.
- 11.5.17.3. The minimum building height within **Mid-Rise Residential** areas shall be 8.5 meters (2 storeys).
- 11.5.17.4. The maximum lot coverage within **Mid-Rise Residential** areas shall be 80%.
- 11.5.17.5. The minimum setback **Mid-Rise Residential** areas shall be 3.0 meters
- 11.5.17.6. Notwithstanding policy 9.2.1.6 the maximum density within **Mid-Rise Residential** areas shall be 2.5 FSI.
- 11.5.17.7. Notwithstanding policy 9.2.1.1 and 9.2.2.3 permitted uses include:
- a. residential units
  - b. parkettes and public squares;
  - c. one convenience retail store located at grade, with a maximum GFA of 100 square metres as part of a residential building.
- 11.5.17.8. Notwithstanding policy 9.2.2.5 the following building types are permitted, pursuant to policy 9.2.3:
- a. Townhouses
  - b. Low-Rise buildings
  - c. Mid-Rise buildings
- 11.5.17.9. Notwithstanding policy 9.2.2.2 and 9.2.3.3, a maximum of 4 storeys are permitted facing the street. Additional storeys must stepback at a 45 degree angle.

### **11.5.18 Private Open Space - The Fairgrounds**

- 11.5.18.1. The Fairgrounds is a unique area within Vaughan and requires a site specific designation. The intent of this Secondary Plan (and of the Woodbridge Heritage Conservation District) is to conserve the Fairgrounds as a significant cultural heritage landscape, and as a significant open space amenity within Vaughan.

- 11.5.18.2. Notwithstanding policy 9.2.1.5 the maximum building height within **Private Open Space- The Fairgrounds** shall be 11.0 meters (3 storeys).
- 11.5.18.3. In addition to policy 9.2.1.1 and 9.2.2.16, permitted uses include:
- a. uses permitted in the Park designation per policy 11.5.19;
  - b. outdoor exhibits and fairs;
  - c. outdoor markets;
  - d. a track for the racing of animals;
  - e. any general farming, agricultural use or animal husbandry, which is not obnoxious to the public welfare, and such uses or enterprises as are customarily carried on in the field of general agriculture;
  - f. office space associated with the management of the Fairgrounds;
  - g. ancillary buildings associated with the Fairgrounds activities, may be considered and shall be designed to contribute to the framing and layout of the Fairgrounds;
  - h. does not include a track for the racing of snowmobiles or any type of motorized vehicle, golf courses, golf driving ranges or miniature golf courses.

### 11.5.19 Parks

- 11.5.19.1. The **Parks** designation represents a variety of publicly accessible land uses that centre on an open space, ranging from a naturalized woodlot to a hard-landscaped public square. It is the intent of this Secondary Plan to develop a diverse range of open space types, connected through a larger network of open spaces.
- 11.5.19.2. Permitted uses include:
- a. parks, playgrounds, sport and athletic fields, picnic areas, swimming pools, day camps, and outdoor passive amenities;
  - b. indoor and outdoor recreational facilities;
  - c. cultural uses;
  - d. public utilities, where required; and,
  - e. does not include a track for the racing of animals, snowmobiles or any type of motorized vehicle, golf courses, golf driving ranges or miniature golf courses.
- 11.5.19.3. **Parks** must be publicly accessible.
- 11.5.19.4. **Parks** must be easily accessible to pedestrians.

- 11.5.19.5. When a private property abuts an open space, buildings shall be designed to front the open space, with doors and windows, creating a built form edge to the public open space.
- 11.5.19.6. Blank walls, surface parking, garages, structured parking, and service areas should not front directly onto an open space / park.
- 11.5.19.7. Lands utilized for stormwater management facilities should not be considered as any part of the parkland dedication requirement provided through the Planning Act.

#### **11.5.20 Regional Storm Flood Plain and Abutting Natural Areas**

The **Regional Storm Flood Plain** and abutting **Natural Areas** (lands that extend from the flood plain to the TRCA Regulation Limit, whether in public or private ownership), are intended to conserve and enhance the natural ecosystem and to maintain biological diversity within Vaughan. Natural Areas have been identified by the TRCA and are regulated and managed by the TRCA and the City of Vaughan. The TRCA Regulation Limit is the area subject to the TRCA's Development, Interference with Wetlands and Alterations to Shorelines and Watercourses Regulation (Ontario Regulation 166/06). The Regulation Limit does not represent the development limit. The development limit is established through the application review process. Proposed development works for properties located within the Regulation Limit Area will require a permit under the Ontario Regulation 166/06.

#### **Regional Storm Flood Plain**

- 11.5.20.1. Properties within the Kipling Avenue Study Area are, for the most part, above the flood line, meaning, these properties are located outside of the **Regional Storm Flood Plain**. Properties, or parts of a property that lie within the flood plain or Natural Areas as determined by the TRCA, must seek approval by the City of Vaughan and the TRCA for any development including:
  - a. construction and/or reconstruction;
  - b. change of use to a building;
  - c. site grading;
  - d. temporary or permanent placement or removal of fill material; and,
  - e. the interference and alterations to watercourses, including the
  - f. construction of storm water management ponds and outfalls.

## Natural Areas

- 11.5.20.2. Notwithstanding policy 9.2.1.1 permitted uses within **Natural Areas** include compatible recreational uses such as trails and low impact playgrounds;
- 11.5.20.3. No Conservation Lands shall be used in calculating the lot frontage, lot area, coverage or yards required for a development, or be considered as any part of the parkland dedication requirement provided through the Planning Act.
- 11.5.20.4. A minimum 10 metre ecological buffer from the greater of the stable top of bank, dripline of significant vegetation or Regional Storm Flood Plain shall be provided to the City of Vaughan, outside of the development lot or block, adjoining a valley or stream corridor, as part of the public open space system.

## 11.5.21 Urban Design - Built Form

The built form framework defines the desired future character and function of built elements within the Kipling Avenue Corridor Area. The intent is to ensure that new buildings reinforce a coherent, harmonious and appealing urban environment, as well as contribute to the enhancement of the public realm.

## Height Zones

- 11.5.21.1. Height, in this Secondary Plan, is determined by the land use designations as follows:
- Low-Rise Residential A** – 4 m (1 storey) minimum; 8.5 m (2 storeys) maximum
  - Low-Rise Mixed-Use** – 8.5 m (2 storeys) minimum; 11 m (3 storeys) maximum
  - Low-Rise Residential B** – 8.5 m (2 storeys) minimum; 11 m (3 storeys) maximum
  - Low-Rise Residential C** – 8.5 m (2 storeys) minimum; 13 m (4 storeys) maximum
  - Mid-Rise Mixed-Use** – 8.5 m (2 storeys) minimum; 19 m (6 storeys) maximum
  - Mid-Rise Residential** – 8.5 m (2 storeys) minimum; 19 m (6 storeys) maximum.
  - Parks** – n/a
  - Regional Flood Plain/Natural Areas** – n/a
  - Private Open Spaces – The Fairgrounds** – 11 m (3 storeys) maximum
- 11.5.21.2. To ensure that the building envelope and height make a positive contribution to adjacent properties and the public realm, the following must be considered with all new or renovated buildings:
- Consider the human scale:



adjacent buildings by providing a proper transition in heights. Designing a proper transition is most important when new development is adjacent to: heritage properties, a lower density land use, or a public open space.

### **Building frontages, orientation and placement**

- 11.5.21.3. Building frontages, orientation and placement are shown on Map 11.5.I.
- 11.5.21.4. The siting of buildings, in terms of their location within a property and in terms of the orientation of their facades, are a critical factor in the definition of a welcoming and safe pedestrian environment along streets and public spaces.
- 11.5.21.5. All building frontages must be oriented toward public streets and other public spaces, in order to clearly define the public realm, create a consistent street wall, and to create an attractive street environment for pedestrians.
- 11.5.21.6. All buildings within the study area, oriented toward public streets, must have clearly defined primary entry points that open directly on to the public sidewalk.
- 11.5.21.7. For all new development, buildings must be sited to first address the intersection or corner locations, secondly the primary street frontages, and thirdly the local street frontages and other publicly accessible open spaces.
- 11.5.21.8. Architectural features are encouraged at all corner building locations to enhance the visual prominence and identity of the Corridor.
- 11.5.21.9. Buildings located along the river valley and/or any other public space must have double frontages; facing both the street and the river valley and/or any other public space, but will only require primary entry points from the street.
- 11.5.21.10. Pavilion style buildings within a public square must have frontages on all sides and service entrances for these buildings should be kept to a minimum and be discretely integrated in the design of the building facades.
- 11.5.21.11. For all new development, parking will not be permitted between the edge of the public right-of-way or public realm and the building face or setback area.

### **Minimum Building Front-yard Setbacks**

- 11.5.21.12. To achieve a continuous street wall for all streets within the area, and especially along Kipling Avenue, a set of building setback conditions must be applied for all new development, as illustrated in Map 11.5.J. Generally, a minimum 3 metre building setback should be applied along most streets within the Corridor except at commercial nodes, where building restrictions exist (such as at the existing northern zero setback condition of the heritage church at Porter Avenue West), and along all neighbourhood streets within the Low-Rise Residential fabric. The building setback conditions identified on Map 11.5.J apply for all new development except where heritage contributing buildings on either side of the subject site are set further back from the front property line; then the setback will be the average of the front yard setbacks of the two properties on either side.
- a. With the exception of some parts of Kipling Avenue, Woodbridge Avenue and intersection, and a few of the existing neighbourhood streets, the minimum building setback should be 3.0 metres from the right-of-way.
  - b. Along Woodbridge Avenue, and at the Woodbridge/Kipling Avenue gateway, there should be a zero setback condition to the right-of-way. The City of Vaughan may require additional setbacks in conditions where streetscape and public realm improvements are needed to create an improved pedestrian environment.
  - c. Along the special streets such as Porter Avenue East and West, with the exception of the existing heritage church pinch point on the southern side of Porter Avenue West, there should be a minimum 4.5 metre setback from the right-of-way to accommodate enhanced landscaping, special streetscape features and green connections to major open spaces within the Corridor.
  - d. All new main roadways such as Parkside Drive and Rainbow Creek Road should have a minimum 3.0 metre building setback from the right-of-way.
  - e. All other residential streets should have a minimum 4.5 metre building setback from the right-of-way.
  - f. Buildings adjacent to the valley lands must meet the setback requirements of the TRCA Regulation Limit.

### **Views**

- 11.5.21.13. As Kipling Avenue runs along a topographic ridge, significant views open towards the river valleys east and west. These views are a defining characteristic of the Corridor, and provide a significant opportunity to establish a quality urban experience. These

views, as identified in Map 11.5.K, must be protected and enhanced.

- a. Existing views to the river valleys should be maintained and enhanced.
- b. New views to the valley corridors should be established where ever possible.
- c. Where possible, view opportunities should be associated with, and be accessible along, the trail system.
- d. Views to open space and built form landmarks should be maintained and enhanced.

### **Ground Level Use, Entrances**

- 11.5.21.14. The design, use and animation of the ground level of buildings are significant elements in defining the character and experience of the street as a welcoming and safe pedestrian environment. Ensuring that buildings provide an attractive and animated face, especially at the ground level, is a priority.
- a. All mixed-use buildings, or buildings with a commercial use must have the commercial use located at grade and have identifiable at grade entrances fronting the street.
  - b. All commercial use building entrances must be flush with the public sidewalk.
  - c. All corner buildings should have corner entrances where possible.
  - d. All ground levels uses, including residential uses, should have a prominent presence on the street.
  - e. All residential units located at grade must have a direct entrance from the street that functions as a front door and has the characteristics of a front door.

### **Corner and terminus sites**

- 11.5.21.15. Corner and terminus sites are shown on Map 11.5.I.
- 11.5.21.16. Some sites, buildings and/or portions of buildings, given their visual prominence, have a special role to play within the urban design of the street.
- a. To enhance the distinction and landmark quality of new buildings on corner or visual terminus sites, modest exceptions to special features of buildings are permitted to stepbacks of 3 metre maximum and height to 3 metre maximum restrictions may be permitted to encourage massing and designs that accentuate the visual prominence of the site – architectural treatments can include tall slender elements such as spires or turrets that are complimentary to the surrounding heritage character.

- b. New developments on corner sites should orient to both street frontages.
- c. New developments on terminus sites should align design features to the view axis which, in addition to tall elements, can include aligned entries or portico openings.
- d. As new development on corner and terminus sites can shape the image and character of an area, they should have greater civic obligations to ensure that the highest possible standards in design and material quality are achieved.

### **Parking and Servicing**

- 11.5.21.17. Parking and servicing are a necessary aspect of any development. They should be fully integrated within buildings, and directly linked to the areas where they are most effective. In general, parking and servicing should be located within the development block, and not adjacent to, and or visible from, the street or pedestrian areas.
- a. Wherever possible, parking for new developments should be provided below-grade and accessed by a rear or side yard lane.
  - b. Surface parking and service areas should be located to the rear of a development and generally not visible from the street.
  - c. Where surface parking or service areas are exposed, they should be designed to include landscaping, clearly marked pedestrian access and paths, lighting and buffered with landscaping such as hedges or shrub planting and/or other mitigating design measures.
  - d. Where a structured public parking facility is visible, it should be designed to be integrated into the surrounding context by using similar façade articulation and materials, including small openings that function as ‘windows’.
  - e. Where a structured parking facility fronts onto a street or public space, the building frontage should be faced with active uses.

### **Landscaping and Amenity**

- 11.5.21.18. For new development, especially in areas where there are higher building heights and densities, private open space enhancements are encouraged as part of the built form design in order to contribute to the visual aesthetics and quality of the public realm.
- a. On broad sidewalks in retail areas, outdoor spill-out activities such as patios are encouraged to further animate the street.
  - b. Roof gardens should be encouraged on all above-grade surfaces especially on structured parking roof tops.
  - c. Balconies should be designed as an integral part of the building rather than

appearing to be “tacked-on”.

- d. Courtyards, forecourts and other intimate spaces accessible to the public and animated with at-grade uses are encouraged.

### 11.5.22 Heritage Conservation

A significant part of the Kipling Avenue Corridor area lies within the Woodbridge Heritage Conservation District (Woodbridge HCD Study Area). The Woodbridge HCD Study, to date identifies that the heritage character of certain areas within the Woodbridge Core, Kipling Avenue and some adjacent streets should be conserved. To do so, it describes the heritage character and prescribes guidelines to manage change within a defined district boundary.

- 11.5.22.1. Heritage conservation is a core principle of this Secondary Plan. The Plan considers that heritage conservation of heritage properties (See Map 11.5.M), structures and landscapes, is key to achieving an attractive and liveable urban environment that will be cherished by residents and visitors alike. To this end:
  - a. Heritage resources and/or properties within the boundary of the Woodbridge HCD must abide by the policies and guidelines of the Woodbridge HCD Plan. Where conflict may arise between both documents, the Woodbridge HCD Plan will take precedence over this Secondary Plan.
  - b. Properties beyond the boundary of the Woodbridge HCD, must be sympathetic to the heritage character and attributes described in the Woodbridge HCD Plan and provide an appropriate transition to the Woodbridge HCD.
  - c. Properties that contain heritage resources that have been listed or designated as part of the City of Vaughan Heritage Inventory, must conserve the heritage character and heritage attributes identified as part of the listing and/or in the Designation Report.
  - d. Properties that are adjacent or near heritage resources that have been listed or designated as part of the City of Vaughan Heritage Inventory, must be sympathetic to the heritage character and attributes identified as part of the listing and/or in the Designation Report.
  - e. Notwithstanding the above, the City of Vaughan may require a Heritage Impact Assessment as part of the application process for any municipal approval including new construction, renovation or demolition of a structure or landscape. The City of Vaughan may also require a Conservation Plan secured by a letter of credit, as

part of any municipal approval, for properties where heritage resources have been identified either as part of the City of Vaughan Heritage Inventory, as part of the Woodbridge HCD, or as a result of the Heritage Impact Assessment.

### **11.5.23 Transitions of New Buildings in Relation to Heritage Resource**

11.5.23.1. One of the key driving factors for this Secondary Plan is the conservation of the existing heritage fabric within the Kipling Corridor which includes structures and landscapes and open space that contribute to the existing heritage character. The intent is to establish a framework for development that guides the integration of new structures within the existing heritage context, mainly along Kipling Avenue.

11.5.23.2. The Kipling Avenue Corridor Study supports the recommendation of the Woodbridge HCD Study in relation to the existing contributing heritage fabric which states that, “new and renovated structures and landscapes, as well as additions to existing structures and landscapes, must be sympathetic to the heritage character and the heritage attributes of ‘adjacent’ heritage resources. The term ‘adjacent’ includes properties that a) touch; b); form part of a cluster; c) form part of a continuous street wall; and d) are visible from each other.

11.5.23.3. Once approved, the Woodbridge HCD will be the governing policy over this study and will be used as the mechanism to conserve the structures and landscapes that contribute to the HCD’s heritage character, and to manage the introduction of new structures and landscapes in such a way that they harmonize with heritage resources and contribute to the district’s heritage character.

#### **The HCD Study Guidelines**

11.5.23.4. The following guidelines, as established in the Woodbridge HCD Study, shall be used to assist in the process of achieving the proper transition of building scales, heights and presence in order to create a harmonious relationship between new structures and landscapes with contributing properties that fall within heritage resources within the Kipling Avenue Corridor Area.

11.5.23.5. The term “contributing buildings” describes the buildings that have been identified through the Woodbridge HCD Study as contributing to the heritage character of the Woodbridge HCD, and which cultural heritage value must be conserved. A list of the

properties with contributing buildings is included on Schedule 11.5.M.

- a. Conservation of Heritage Resources
  - i. Buildings and structures listed as 'contributing' in the Woodbridge HCD may not be demolished or moved.
  - ii. The City of Vaughan may require a Heritage Impact Assessment when it considers that cultural heritage value may exist, or be effected by any new construction.
- b. Conservation of Heritage Character
  - i. Contributing buildings display a variety of setbacks and side yard conditions, reflecting the different construction periods and original use.
  - ii. New development must be sympathetic to this character and must develop in a way that does not detract, hide from view, or impose in a negative way, on existing heritage contributing resources, as per the following height and setback guidelines.
- c. The historic setbacks of contributing buildings should be maintained and contributing buildings should not be relocated to a new setback line. New buildings must be sympathetic to the setbacks of adjacent contributing buildings.
- d. Height Guidelines
  - i. The height of contributing buildings should be maintained.
  - ii. New buildings must be sympathetic to, and transition from, the height of adjacent contributing buildings with a minimum 45 degree angular plane, starting from the existing height of the contributing building, measured at the building's edge.
- e. Sideyard and Backyard Setback Guidelines
  - i. New buildings must setback from contributing buildings a distance equivalent to half the height of the contributing building.
  - ii. Consideration may be given to the construction of new buildings, and additions to contributing buildings, adjacent to, or joining with, contributing buildings only when:
    - A. new construction is located in the parts of the contributing building that is not visible from the street or from a public space;
    - B. new construction is setback from the street frontage of the contributing building, maintain the heritage character of the street;
    - C. the parts of the contributing building that will be enclosed or hidden from view by the new construction, do not contain significant heritage attributes; and,

- D. new construction contributes to the district's heritage character.
- f. Frontyard Setback Guidelines
  - i. The historic setbacks of contributing buildings should be maintained and contributing buildings should not be relocated to a new setback line. New buildings must be sympathetic to the setbacks of adjacent contributing buildings.
  - ii. When new buildings are located adjacent to existing contributing buildings that are set back from the property or street line, new buildings should transition back to the setback line of existing contributing buildings in order to maintain open views and vantage points from the street to the contributing buildings.
- g. Along Kipling Avenue, where heritage contributing buildings are set further back from the recommended 3.0 metre minimum building setback line, any new development adjacent to the heritage contributing building must be set back, at a minimum, to a line measured at 45 degrees from the front corner of the existing heritage contributing building, (see Map 11.5.L, Diagram E).
- h. Landscape Guidelines
  - i. New buildings and landscapes must contribute to the heritage character and attributes of adjacent contributing landscapes such as parks, public squares, open spaces, recreational areas, and landscaped areas within private properties.
  - ii. Pedestrian connections between adjacent landscaped areas should be maintained and enhanced.
  - iii. Significant visual connections between adjacent landscaped areas should be maintained and enhanced.
  - iv. New and renovated buildings must provide an active, welcoming façade facing significant landscapes and public spaces.
- i. Architectural Character Guidelines
  - i. New Buildings-Architectural Design
    - A. Within the heritage district new architecture will invariably be constructed. This will occur as buildings on vacant sites, replacement buildings for non-contributing existing structures or severely deteriorated older buildings.
    - B. New buildings will fall into two categories, replica or reconstructed build-

- ings and contemporary buildings.
- ii. Replica / reconstructed buildings
    - A. On some sites the opportunity may exist to replicate a formerly existing structure with a new building or as a part of a larger building proposal. This approach is to be encouraged where good documentary evidence exists. The replication of a historic building should proceed in a similar manner to the restoration of an existing but altered or deteriorated structure.
    - B. Design of the building should be based upon documentary evidence including photographs, maps, surveys and historic design/construction drawings.
    - C. Replica designs should be prepared by practitioners skilled in historic research and historic construction design and detailing.
    - D. The interior space and basic structure of a replica building is not required to, but may also, use historic materials or details as long as the exterior presentation replicates the original structure.
  - iii. New buildings in the heritage district
    - A. Entirely new buildings may be proposed where no previous buildings existed, or where original buildings are missing or severely deteriorated, and where non contributing buildings have been removed.
    - B. The intention in creating designs for new buildings should not be to create a false or fake historic building, instead the objective must be to create a sensitive well designed new structure “of its time” that fits and is compatible with the character of the district and its immediate context. Designers of new buildings in the district should have a proven track record respecting the creation of designs in similar historic contexts.
    - C. The design of new buildings in the HCD should carefully consider requirements elsewhere in this document for density, scale, height, setbacks, coverage, landscape open space, view corridors, angular plane and shadowing. Further, character areas have been identified in the district. Each character area has identifiable characteristics including commercial mainstreet as opposed to residential, building scale spacing and setback, which should also be understood and respected.
    - D. Architectural considerations include the following attributes: contempo-

rary design, material pallet, proportions of parts, solidity verses transparency and detailing.

- iv. Contemporary design
  - A. Contemporary work should be “of its time”. This is consistent with the principals stated in the Venice Charter, Appleton Charter and other Charters recognized internationally as a guide for heritage work. This does not mean that new work should be aggressively idiosyncratic but that it should be neighbourly and calmly, respectfully, fit its “village” context while at the same time representing current design philosophy. Quoting the past can be appropriate, however, it should avoid blurring the line between real historic “artifacts”, buildings, bridges and other structures.
  - B. Contemporary as a design statement does not simply mean current. Current designs with borrowed detailing inappropriately, inconsistently, or incorrectly used, such as pseudo- Victorian detailing, should be avoided.
- v. Material pallet
  - A. Whereas there is a very broad range of materials in today’s design pallet, materials proposed for new buildings in the district should include those drawn from ones historically in use in Woodbridge. This includes brick, stone, stucco, wood siding and trim; glass windows and store-fronts and various metals. The use and placement of these materials in a contemporary composition and their incorporation with other modern materials is critical to the success of the fit of the proposed building in its context. The proportional use of materials, drawing lines out of the surrounding context, and careful consideration of colour and texture, all add to the success of a composition.
- vi. Proportions of parts
  - A. Architectural composition has always had at its root the study of proportion. In various styles rules of proportion have varied from the complex formulas of the classical orders to a more liberal study of key proportions in buildings of the modern movement.
  - B. In the design of new buildings in this heritage district, work should take into account the proportions of buildings in the immediate context and

consider a design with proportional relationships that make a good fit.

- C. An example of this might be windows. Nineteenth century buildings without fail used a vertical proportion system in the design and layout of windows, including both overall windows, singly or in built up groups, and the layout of individual panes.
- D. In buildings of the art deco and art modern period, windows are often found to be of a horizontal proportion. Although not universally the case, this horizontal proportioning is a character defining feature of these styles.

j. Solidity Verses Transparency

- A. Similar to proportion, it is a characteristic of historic buildings of the 19th century to have more solid walls with punched windows. This relationship of solid to void makes these buildings less transparent. It was a characteristic that was based upon technology (ability to make large windows and to heat space), societal standards for privacy, and architectural tradition. Buildings of many 20th century styles in contrast use large areas of glass and transparency as part of the design philosophy.
- B. In this historic district the relationship of solidity to transparency, is a characteristic of new buildings that should be carefully considered. It is an element of fit. The nature of the immediate context for the new building in each of the defined character areas should be studied. The level of transparency in the new work should be set at a level that provides a good fit on the street frontages.
- C. In the Woodbridge Avenue character area, a main street approach can be taken and a more transparent building permitted between the proportion of 20% solid to 70 % solid.
- D. In the other character areas this proportion should reflect a more traditional residential proportion of 40% solid to 80% solid.

i. Detailing

- A. In contemporary design philosophy the expression goes “God is in the details”, this is to say a beautifully detailed building is usually a successful one (alas not always so). In part this is due to the expressive nature of the fundamental construction components of modern buildings.
- B. In past styles, for example, structure was often hidden behind a veneer of other surfaces and “detailing” was largely provided by the use of

coloured, shaped, patterned or carved masonry and /or added traditional ornament, moldings, finials, cresting and so on. In contemporary buildings every element of a building, such as architectural, structural, mechanical even electrical systems, can potentially add to the artistic composition.

- C. For new buildings, the detailing of the work should once more refer to the nature of the immediate context and the attributes of the area in which it is to be placed.

## **11.5.24 Parks and Open Space**

11.5.24.1. The following Parks policies apply to the Secondary Plan Area:

- a. The City may acquire lands anywhere within the Secondary Plan for the purposes of establishing parkland. The parkland designated by this Plan shall be shown on draft plans of subdivision. Their actual size, configuration and design will be subject to confirmation by the City during the development approval process.
- b. All development applications may be required to identify a potential location for an urban square, to the satisfaction of the City.
- c. Where the City has determined that parkland dedication is not required on an individual development site, cash-in-lieu of parkland dedication shall be payable to the City in accordance with the Planning Act and the City's cash-in-lieu policy.
- d. All parkland design and construction shall be carried out by the City in accordance with the City's Development Charges By-law. However, in the event that the City and the relevant landowner agree that the owner will develop the park, the design, securities and construction for the park will be addressed through an agreement in accordance with the City's "developer-build/parks development policy".
- e. Public squares are intended as formal spaces for passive recreation, in support of the adjacent development. Urban squares should address the following design guidelines:
  - i. Public squares are to be planned as focal points;
  - ii. Relationships with adjacent buildings should provide optimal sunlight penetration, sky view and wind conditions;
  - iii. Streetscape and buildings that abut a public square should be designed to reinforce an interactive relationship between the open space and its adjacent land use;

- iv. Buildings should front onto a public square to create built form edges to the public space;
- v. The landscape along the street frontage, including high canopy street trees, should be complementary on both sides of the street;
- vi. Entry and access points should be located conveniently and incorporate civic design themes; and
- vii. Hard and soft landscape elements and features shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.

### **11.5.25 Environmental Policies**

11.5.25.1. Redevelopment of lands for residential purposes on lands currently occupied by industrial uses, and abutting or adjacent to industrial uses or CPR lands, shall require that the City and the Ministry of the Environment and Energy be satisfied that there are no environmental or land use impacts. A Phase One and Two Environmental Report shall be submitted.

11.5.25.2. Development shall occur in a manner consistent with the City's philosophy of ecosystem planning which provides for the protection and conservation of the quality of environmental resources. The objectives associated with this principle are:

- a. To provide for a system of storm water management for both water quality and quantity, through the application of state-of-the-art Stormwater Management Practices, designed to protect lands and receiving waters within and outside the Amendment Area from detrimental effects such as erosion, flooding and sedimentation resulting from increased levels and rates of runoff.
- b. To ensure that environmental impacts are addressed in an ecologically responsible manner, consistent with the City's environmental policies.
- c. To encourage environmental remediation measures within new developments, where appropriate.
- d. To encourage the use of passive and active renewable energy sources, where appropriate.
- e. To encourage the use of 'green' technologies in the design of new buildings and infrastructure, where appropriate by providing easy access to public transit.
- f. To reduce vehicular dependency by locating residential and work related uses in

proximity to one another in order to encourage residents to both live and work in vicinity, and by providing easy access to public transit.

### **11.5.26 Stormwater Management Ponds**

- 11.5.26.1. Stormwater management facilities shall be permitted in all land use designations.

### **11.5.27 Transportation**

- 11.5.27.1. Currently the entire Kipling Avenue Corridor is operating at acceptable levels of service.
- 11.5.27.2. The entire Corridor will approach its roadway capacity with the proposed development scenario. The Plan encourages a shift in environment that caters to cyclists and pedestrians, slower speeds to calm traffic, and an increase in the orientation to transit.
- 11.5.27.3. There are opportunities to revise the Kipling Avenue Street cross section north of Woodbridge to include single-sided on-street parking and the addition of two bike lanes.
- 11.5.27.4. Much of the vehicular traffic congestion in the area currently is due to background traffic (vehicular trips through the area, rather than from the area).
- 11.5.27.5. As the proposed development scenario is built out, the increased local traffic will have the effect of reducing through traffic from other areas.
- 11.5.27.6. There are opportunities to redesign Kipling Avenue south of Woodbridge Avenue to have two travel lanes, two parking lanes, bike lanes and no left turning lanes.
- 11.5.27.7. Prior to any development or redevelopment within the Secondary Plan Area, applicants will prepare a detailed traffic impact study to the satisfaction of the City and the Region, which will address in part, but not limited to capacity, access, function, neighbourhood infiltration, transportation and transit objectives.
- 11.5.27.8. Streetscape design for arterial roads, collector roads, and local roads shall be subject to the urban design requirements and policies articulated in this Secondary Plan.
- 11.5.27.9. In terms of streetscape design, the main objectives of the Streetscape Master Plan with

respect to the Transportation System are as follows:

- a. Walkable, connected pedestrian oriented streets.
- b. A reduction of road widths providing ample opportunity for enhanced landscaping and generous sidewalks.
- c. Reduced front car parking garage access onto Kipling and the development of rear lane access where possible.
- d. Defined street edges with frontages to the street.
- e. An elimination of surface parking fronting public streets.
- f. Publicly accessible streets, discouraging private roads and laneways.
- g. A cohesive character and identity for each street typology.
- h. On-street parking and bike lanes where possible.
- i. Safe street and rail crossings.
- j. An identification of opportunities for enhanced streetscape treatments and pedestrian priority zones.
- k. Increased transit service and streetscaping that supports increased transit use.

### **Street Network**

- 11.5.27.10. The Street Network for the Kipling Avenue Corridor area is shown in Map 11.5.D. Notwithstanding which, any additional road, public or private, that is deemed by the City to be necessary to the efficient implementation of this Street Network can be constructed without further amendment of this Secondary plan.
- 11.5.27.11. Reduced roadway and transit standards should be encouraged in order to minimize street widening and increase the opportunity for a generous public realm.
- 11.5.27.12. The Street Network road widths shall be:
  - a. Travel lanes - 3.0m - 3.3m wide
  - b. Combined Travel and Transit lanes - 3.75m wide
  - c. Transit lanes - 3.75m wide
  - d. On-street parking lanes - 2.5m wide
  - e. Bike lanes - 1.45 - 1.5m

### **Street Connections and Linkages**

- 11.5.27.13. The vision strives to achieve a pattern of streets and blocks that enable the present and future development of complete and accessible neighbourhoods for the corridor. As stated in Section 11.5.3: Block Pattern, Street Network and Linkages, achieving a

permeable block system and a fine network of vehicular and pedestrian connections is desirable. As well, establishing new access points and linkages to main streets, parks, public amenities and new residential development is an integral component in the development of the Corridor and in shifting the existing vehicular oriented transportation system to a pedestrian oriented, multi-modal system.

- 11.5.27.14. Additional study and site review should be undertaken to ensure that the values inherent in the Transportation Framework are achieved and are in keeping with the long term vision for the Corridor.

### **Public Transit**

- 11.5.27.15. The street cross-section for Kipling Avenue allows for the continued accommodation of a southbound transit loop as well as the opportunity for increased service. As the Corridor develops, a full time transit service along Kipling Avenue is recommended especially as the new Rainbow Creek neighbourhood develops.
- 11.5.27.16. As well, improved transit facilities should be provided such as increased transit stops, bus shelters and signage. Transit furnishings should be complimentary to the Streetscape Master Plan design and should be considered holistically along with other street furniture, prior to implementation of the master plan. Transit furnishings should also be complementary to the heritage character and should be reviewed by the City of Vaughan Cultural Services Staff and Planning Department Staff.

### **Parking**

- 11.5.27.17. All streets should be considered for on-street parking, especially along Parkside Drive to ensure public access to the river valley trails and parks.
- 11.5.27.18. On-street parking should be accommodated along the main streets such as Kipling and Woodbridge Avenue as it provides a safety buffer for pedestrian movement and increases the opportunity to establish a healthy commercial presence.
- 11.5.27.19. Wherever possible, and as redevelopment occurs, parking garages and front parking access should not be allowed along Kipling Avenue with the exception of the existing heritage buildings that already have this condition. Parking access and garages should be located at the rear of buildings.

- 11.5.27.20. Parking and servicing access to buildings should not be allowed to front Kipling Avenue.
- 11.5.27.21. Landscaping, fencing, architectural elements and other appropriate screening treatments should be provided to reduce the impact of at grade parking (where existing) and service areas, and contribute to the visual continuity of the public realm, ensuring that safety and security measures are maintained.
- 11.5.27.22. Wherever possible, the City shall encourage cash-in-lieu of parking, pursuant to the provisions of the Planning Act and Municipal Act.

### **11.5.28 Phasing of Development**

- 11.5.28.1. Phasing of development may be addressed through the appropriate use of the holding (H) provisions of this plan.
- 11.5.28.2. Development Applications within the new Rainbow Creek residential area shall provide a Phasing Plan which:
  - a. Describes and illustrates how existing and proposed development can be incorporated into the site to achieve the full development potential of the site.
  - b. Identifies the public infrastructures and facilities required to serve the development, including water, sewer, stormwater, roads, transit, parks and open space facilities, and their proposed phased construction.
  - c. Describes the expected financial requirements for such public infrastructure, and the appropriate financial contributions from benefitting landowners.
  - d. Given that the Rainbow Creek area will be undergoing a conversion of land use from the existing industrial uses to residential uses, and that this conversion will occur in different phases as landowners decide to re-develop their particular lands; it is therefore understood that re-development of individual parcels may be dependent on the land use status and zoning of abutting lands. Any development application within the Rainbow Creek industrial lands should adhere to the Ministry of the Environment (MOE) Guidelines pertaining to land use separation distance requirements. The approval of any development applications within the Rainbow Creek industrial lands is conditional upon achieving land use separation distances that are satisfactory to the City of Vaughan.
  - e. It should be noted that the MOE Separation Distance Guidelines apply throughout the Secondary Plan Area.

### **11.5.29 Developers' Group Agreement**

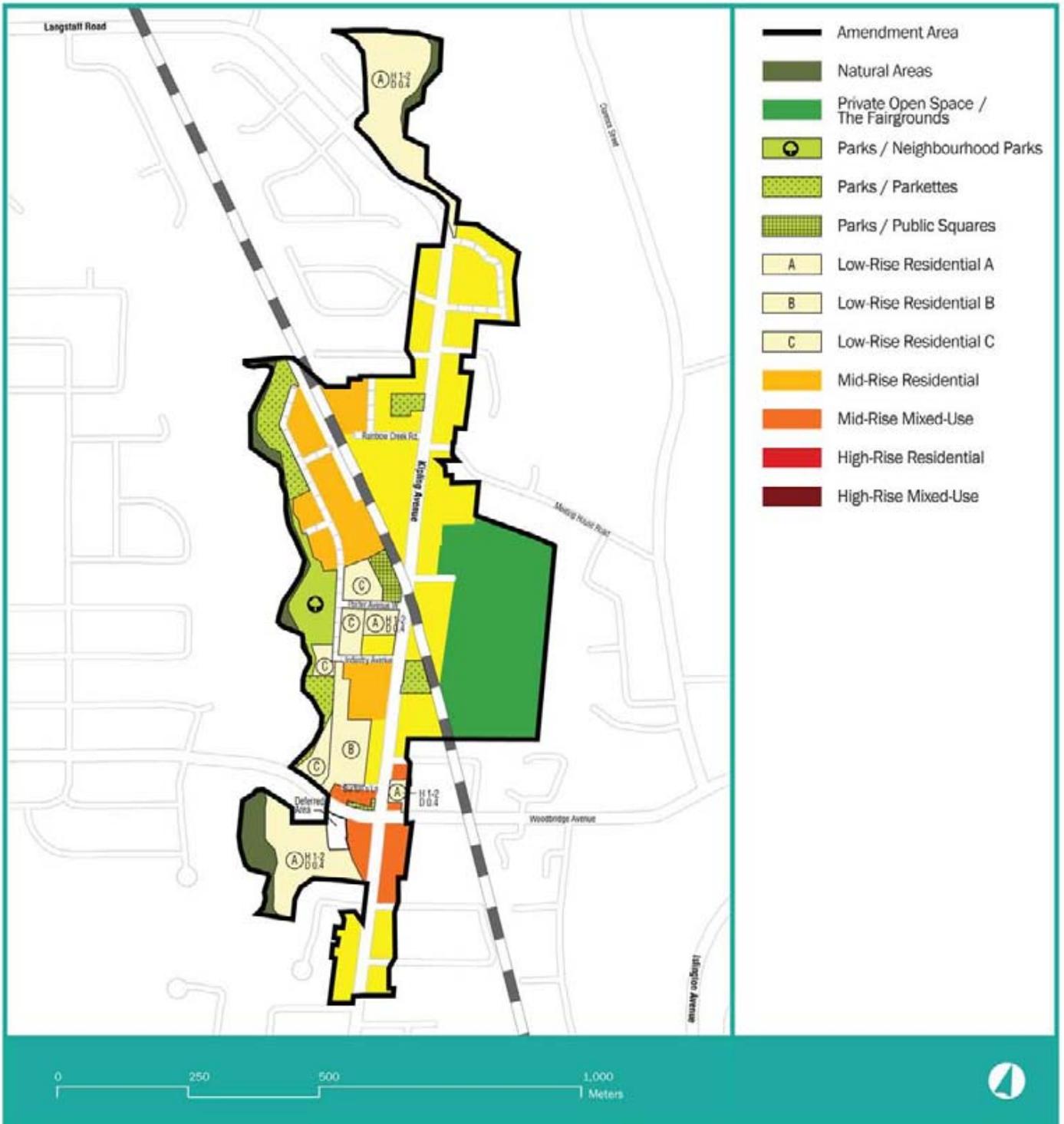
- 11.5.29.1. Prior to final approval of any draft plans of subdivision/draft plan of condominium/site development plan for lands location within the new Rainbow Creek neighbourhood, the landowner shall enter into a developer's group agreement with landowners who fall within their phase of development (Either Phase 1 or Phase 2) to the satisfaction of the City. The agreement shall be regarding, but not limited to, all cost sharing for the particular phase of development, for the provision of parks, cash-in-lieu of parkland, roads and municipal services within that development area. This agreement shall include a provision for additional developers to participate in the Developers' Group Agreement when they wish to develop their lands.
- 11.5.29.2. Prior to final approval of any draft plan of subdivision/draft plan of condominium/site development plan application, the Trustee for each of the phases of development shall provide the City with a letter indicating that the Owner has fulfilled all cost sharing and other obligations of the Rainbow Creek Phase 1 or 2 cost sharing and other obligations of the Developer Agreement.

### **11.5.30 Interpretation**

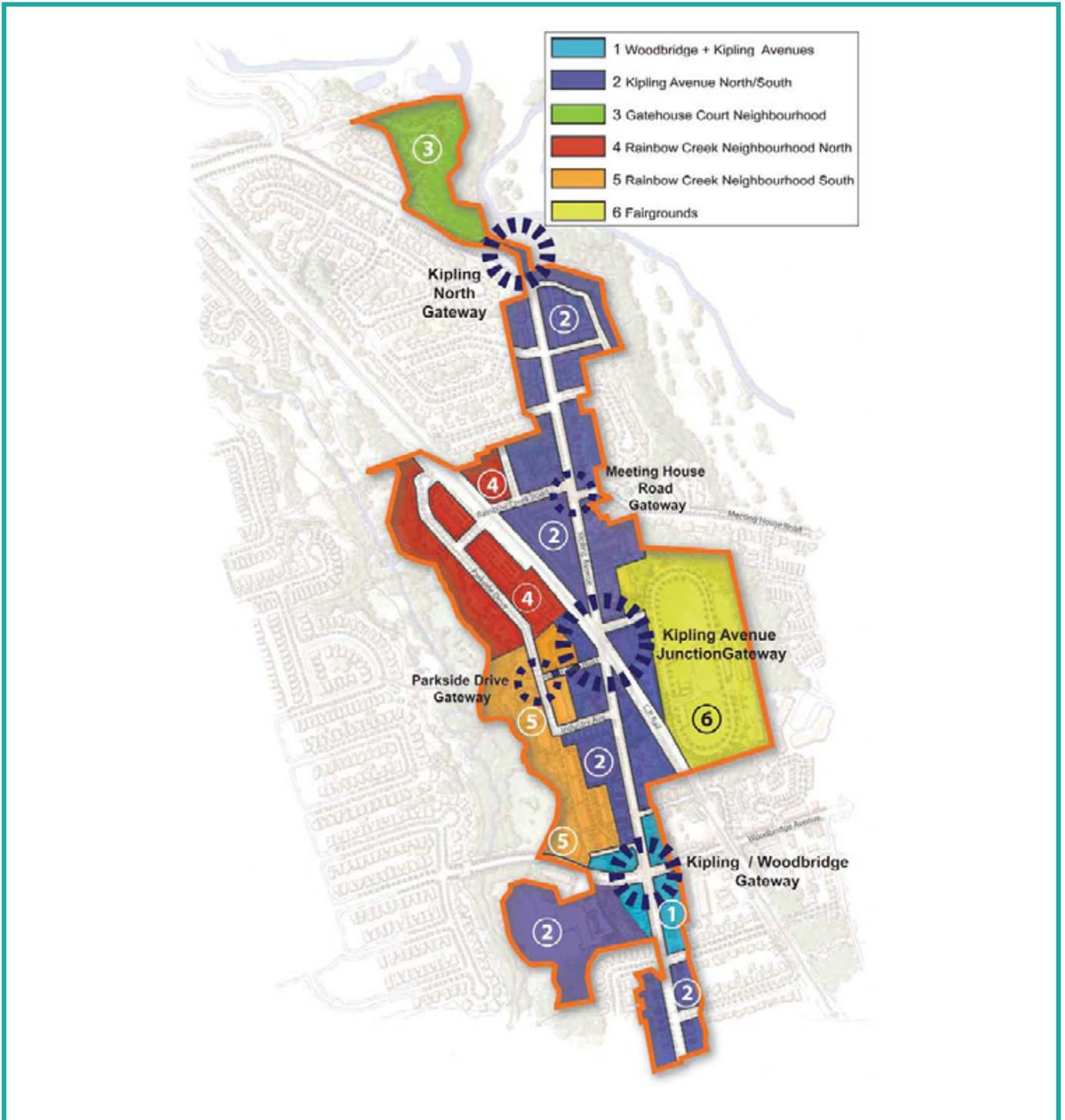
- 11.5.30.1. This Secondary Plan is a statement of policy. It is intended to guide the development of the Amendment Area. Some flexibility in interpretation is permitted, provided the intent of the policies and principles of this Plan are maintained.
- 11.5.30.2. The designation identified on Map 11.5.A are intended to show general areas. Minor adjustments to the boundaries of the designations may occur through the City's approval process without amendment to the Plan, except where the designations are established by fixed boundaries, such as road rights-of-way, or where specifically stated to be fixed in the policies of this Secondary Plan.
- 11.5.30.3. Where lists or examples of permitted uses are provided, they are intended to indicate the possible range and type of uses that are to be considered. Specific uses not listed, but considered by the City to be similar to the listed uses and to conform to the general intent of the applicable land use designation may be recognized as a permitted use and recognized in the implementing zoning by-law.
- 11.5.30.4. Minor variations from numerical requirements in the Plan may be permitted without an

Official Plan Amendment provided that the general intent of the Plan is maintained. Minor variations, in this Secondary Plan, shall mean a maximum of 5 percent variation from the identified numerical requirement.

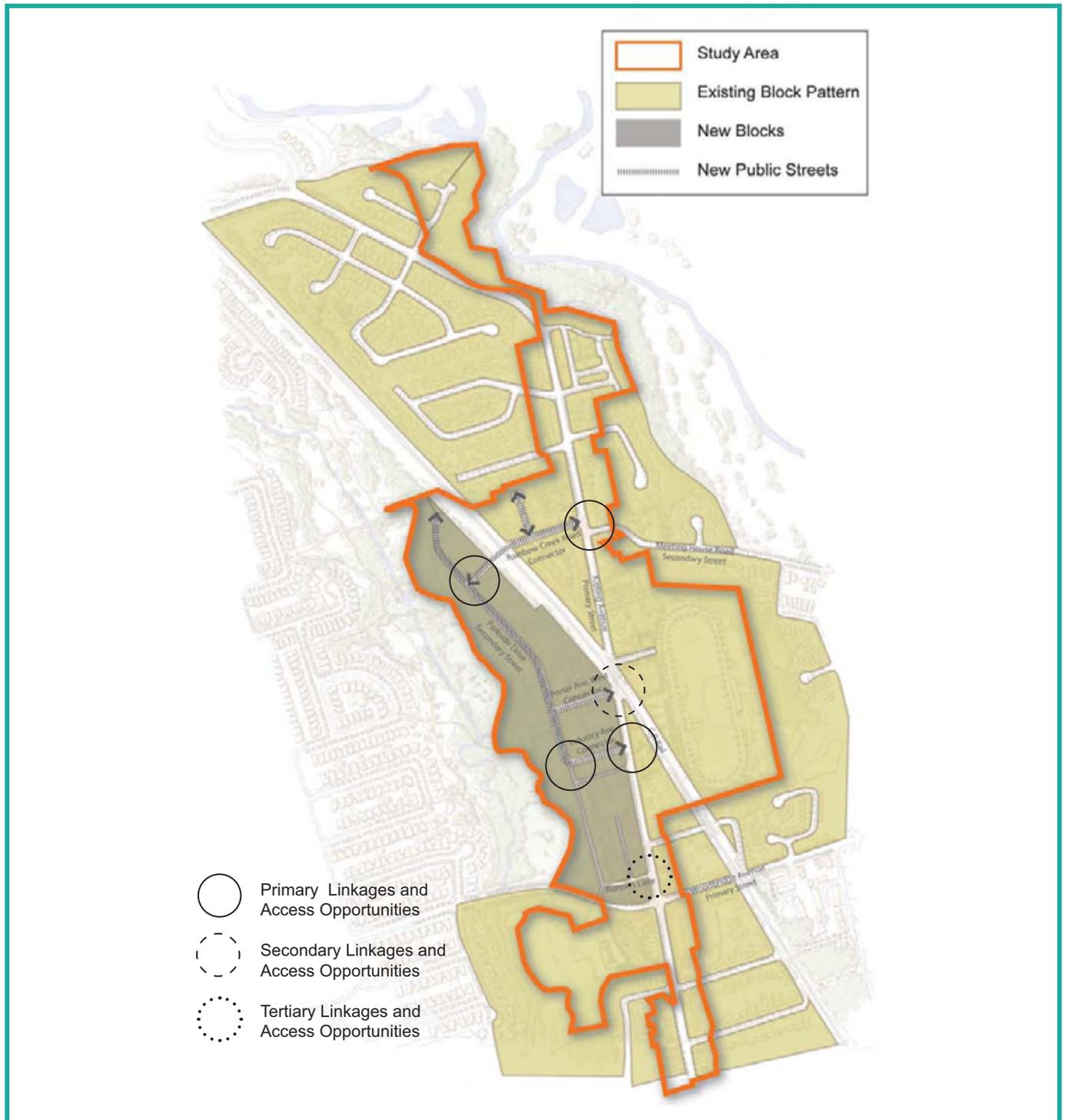
▼ Map 11.5.A  
Kipling Avenue - Land Use



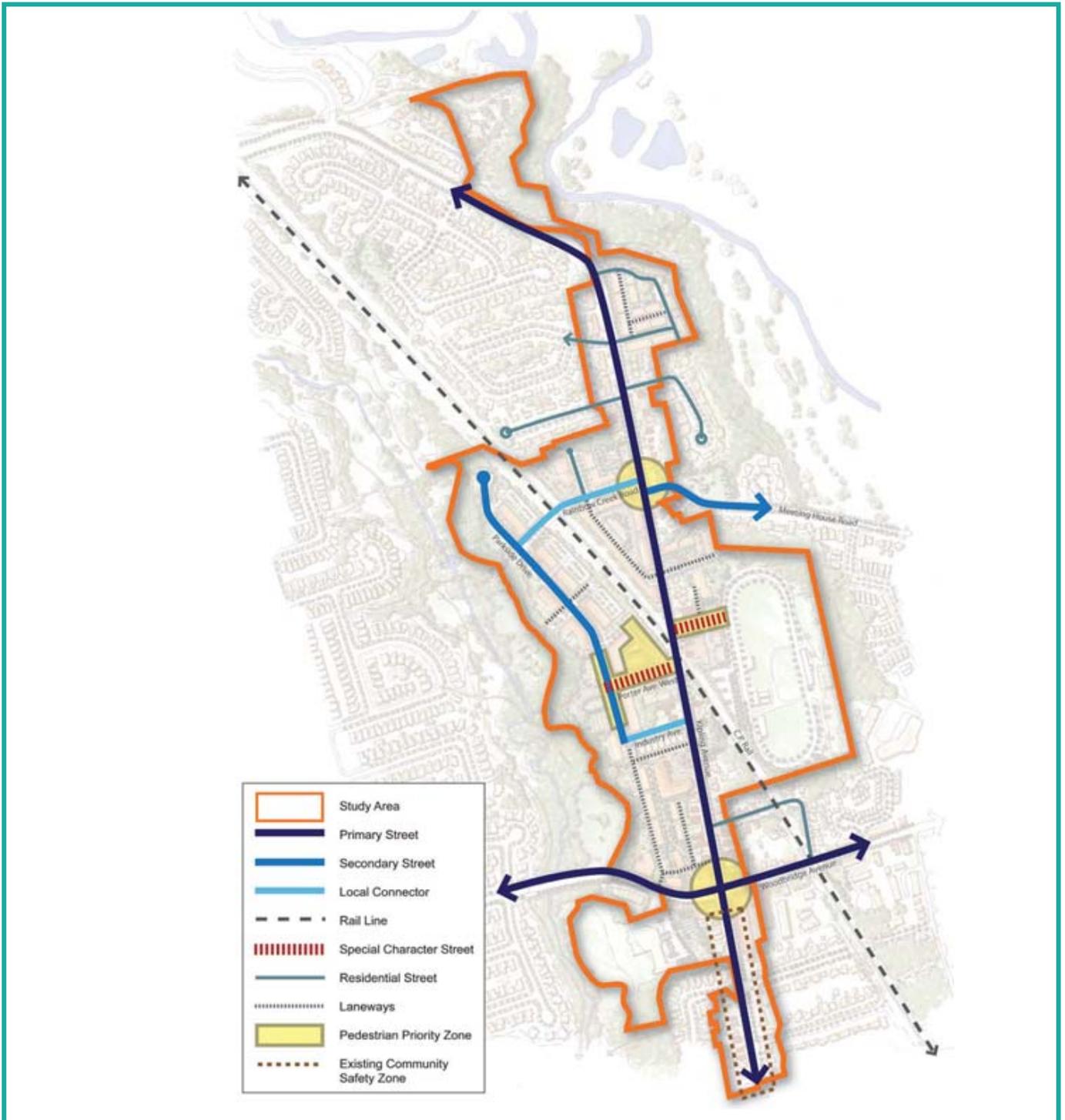
▼ Map 11.5.B  
Precincts and Gateways



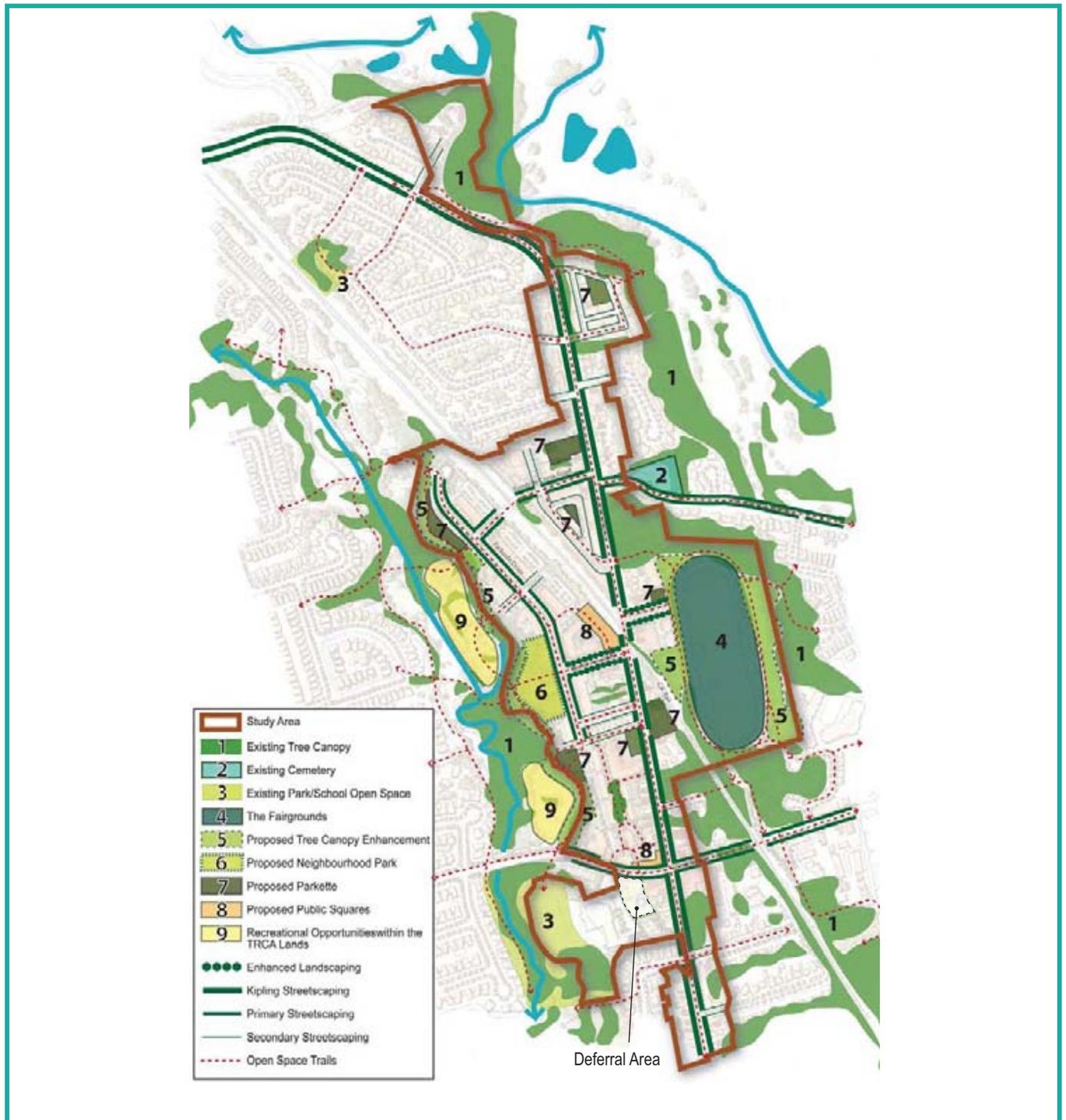
▼ Map 11.5.C  
 Block Pattern, Street Network and Linkages



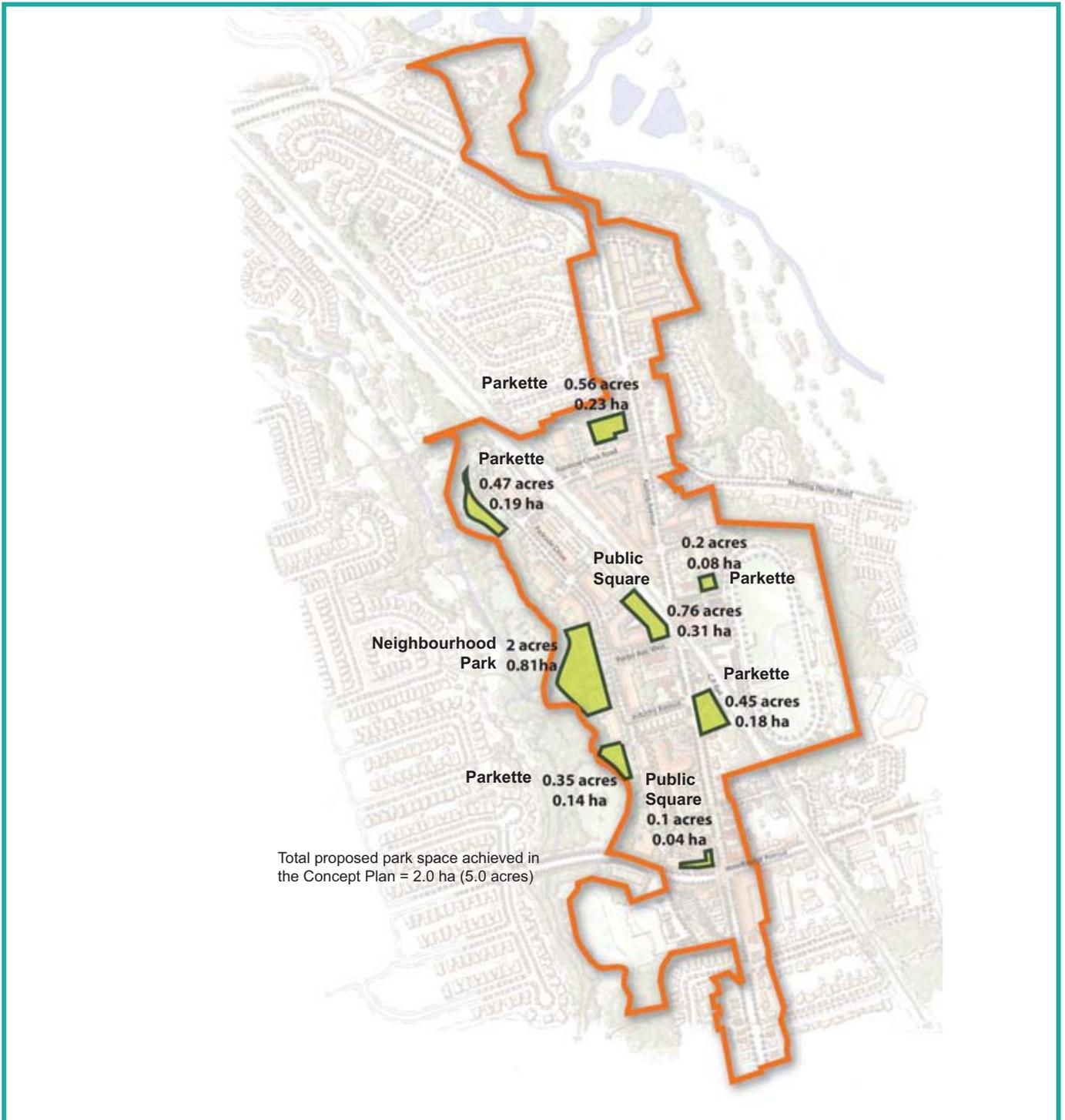
▼ Map 11.5.D  
Street Network



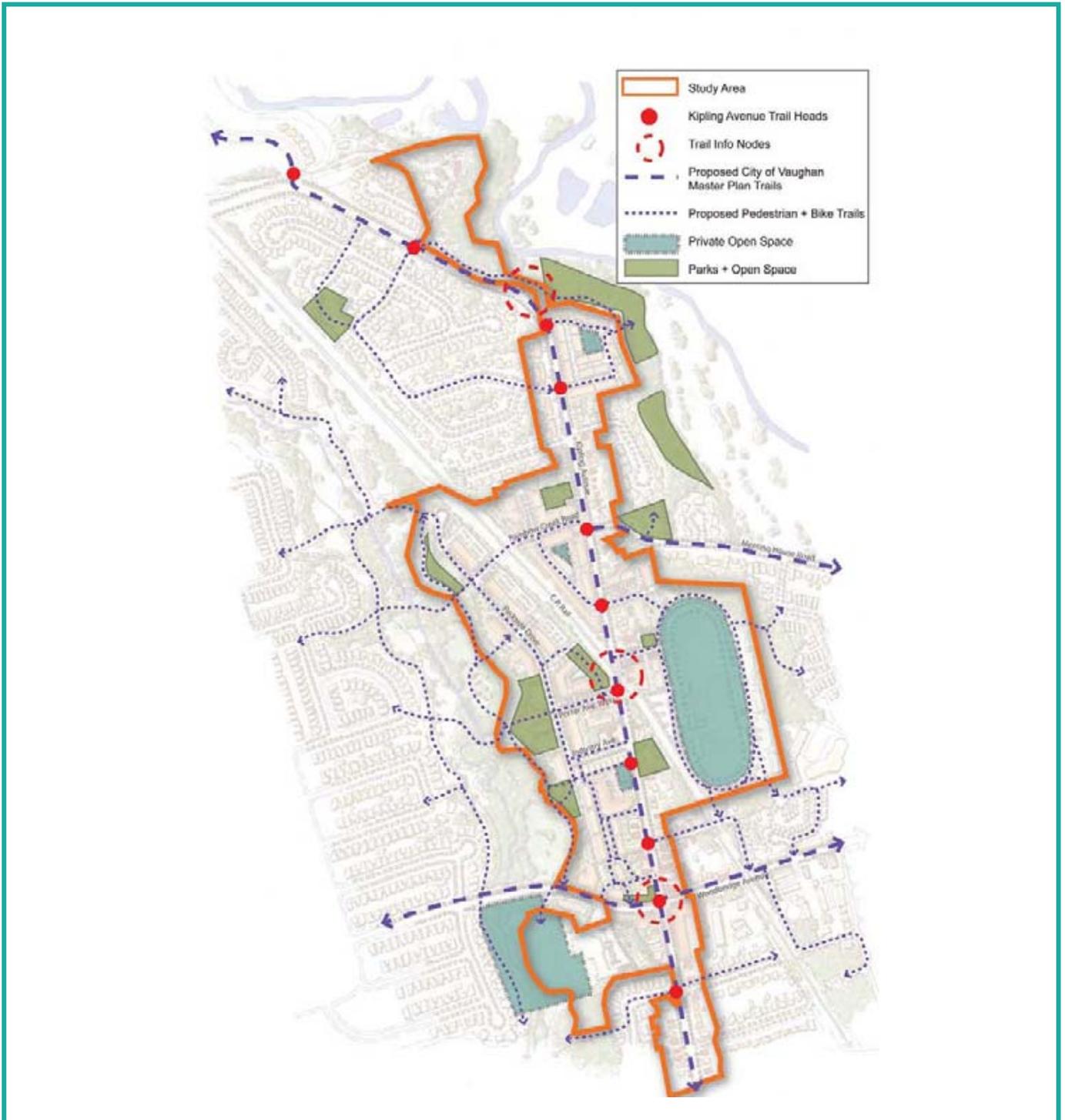
▼ Map 11.5.E  
Open Space Framework



▼ Map 11.5.F  
Proposed Parks, Parkettes and Public Spaces



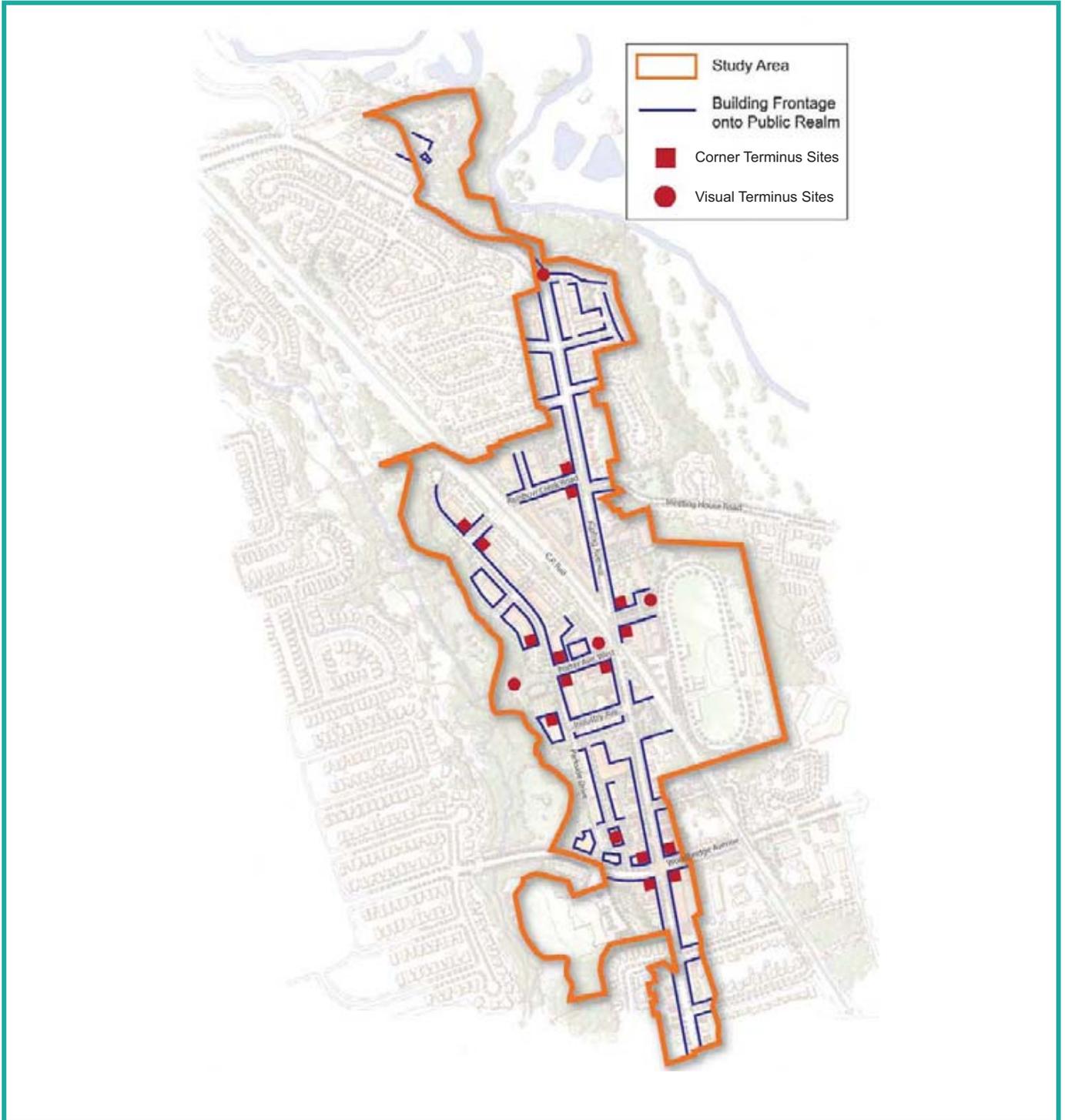
▼ Map 11.5.G  
Pedestrian and Bicycle Trail Network



▼ Map 11.5.H  
Landmark Sites and Gateways

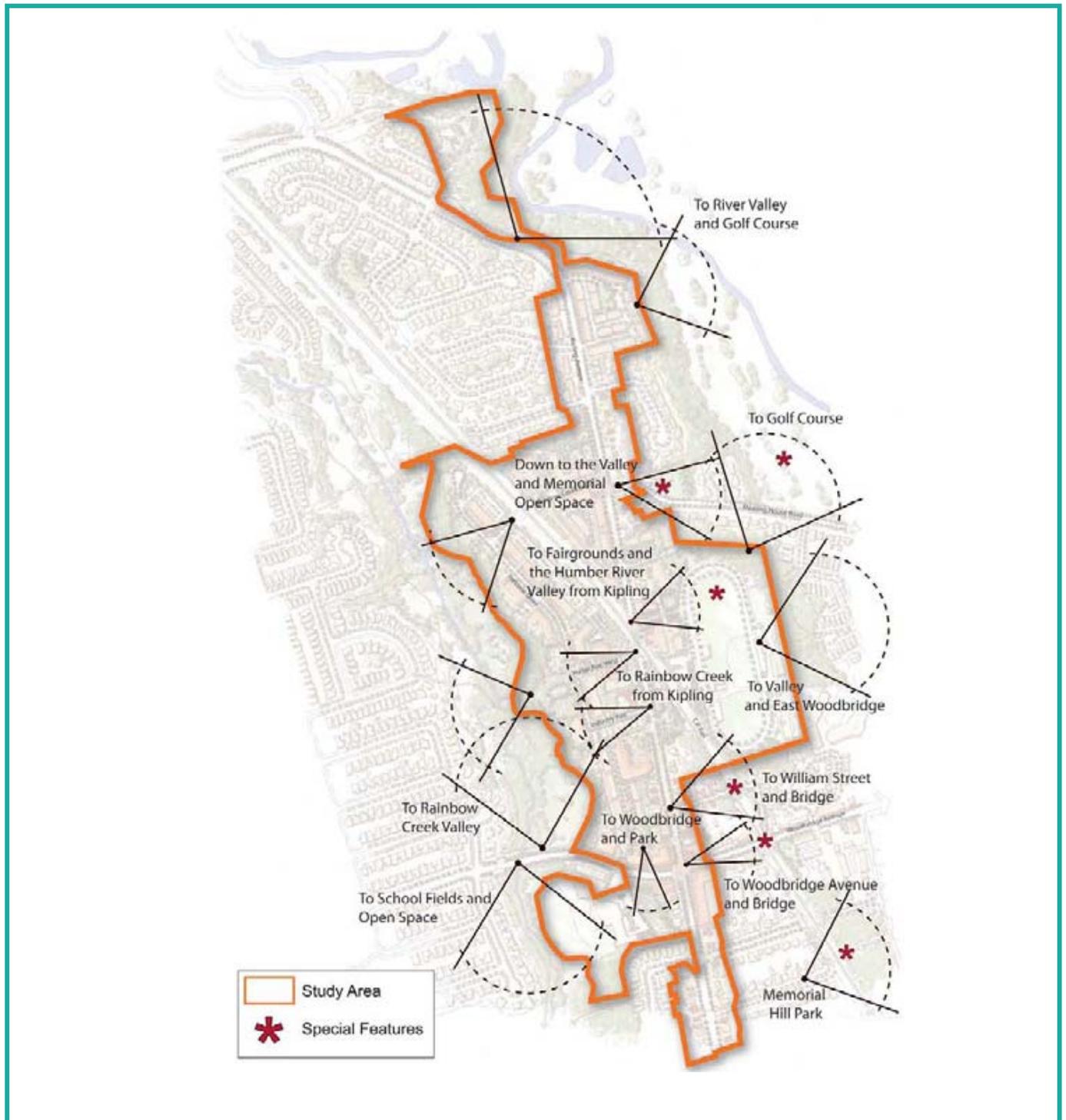


▼ Map 11.5.I  
Building Frontages, Orientation and, Corner and Terminus Sites





▼ Map 11.5.K  
Views and Landmark Features



▼ Map 11.5.L  
 Transitions of New Buildings in Relation to Heritage Resource

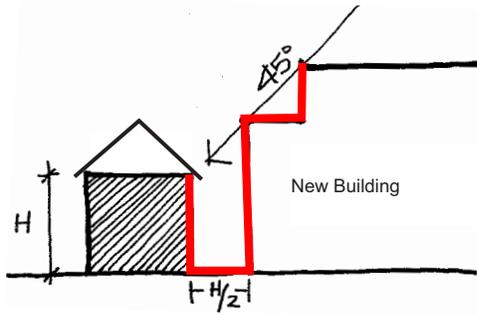


Diagram A

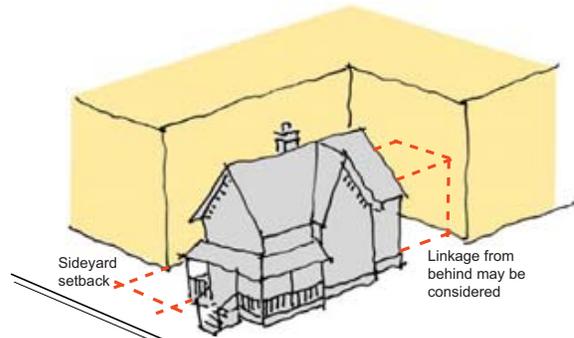


Diagram C

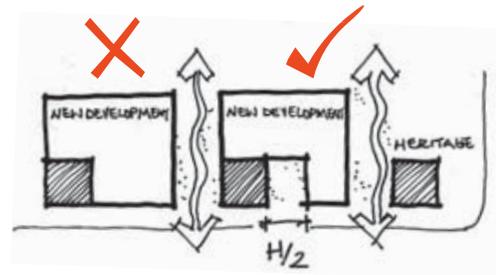
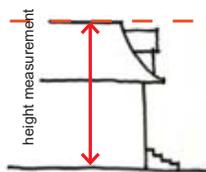
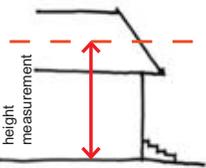


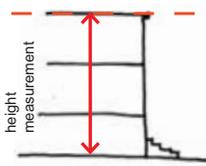
Diagram D



Building height measurement for a mansard roof



Building height measurement for a gable, hip, or a gambrel roof



Building height measurement for a mansard roof

Diagram B

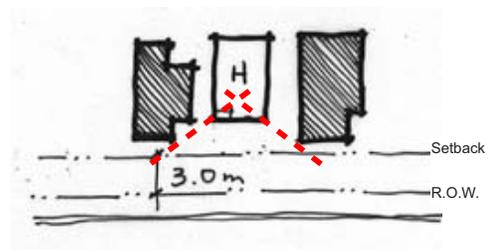


Diagram E

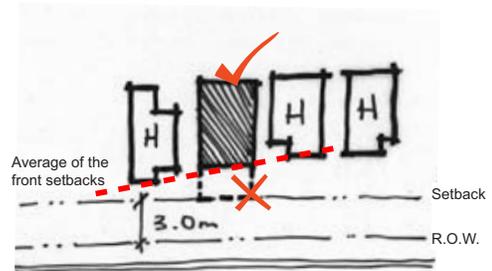


Diagram F

▼ Map 11.5.M  
Kipling Avenue - Identification of Heritage Resources



**Kipling Avenue - Identification of Heritage Resources**

The following is a list of properties within the Kipling Avenue Study Area considered to have cultural heritage value as evaluated through the Woodbridge Heritage Conservation District Study (HCD), and as contributing to the Woodbridge HCD Heritage Character:

7827 Kipling Avenue	8039
7833	8066
7844	8071
7845	8074
7848	8090
7856	8101
7866	8109
7872	8127
7880	8161
7883	8177
7895	8191
7909	8204
7912	8215
7924	8221
7934	8227
7938	8228
7955	8233
7971	8237
7974	8238
7981	8243
7984	8248
7985	8255
7988	8258
8000	8265
8001	3 Burton's Lane Industrial
8006	Building (image below)
8009	

The following two properties are within the Kipling Avenue Study Area (east of Gate House Court), but are not within the proposed Woodbridge Heritage Conservation District (HCD) boundary. These two properties, nonetheless, have been evaluated as contributing to the HCD Heritage Character:

- 8399 Kipling Avenue (Listed Heritage Property)
- 8265 Kipling Avenue (Identified in the Vaughan Heritage Inventory)

▼ Map 11.5.N  
Kipling Avenue - Projected New Unit Counts and Density



*Kipling Avenue - Projected New Unit Counts and Density*

**Number of Units**

A	84
B	280
C	70
D	95
E	16
F	72
G	40
H	49
I	46
J	32
K	18
L	42
M	0
N	41
O	52
P	74
Q	7

Total Units: 1022

For the purpose of calculating density, FLOOR SPACE INDEX (FSI) shall be the GROSS FLOOR AREA of a development divided by the GROSS SITE AREA. The deployment of allowable density must not result in buildings exceeding the maximum number of storeys or the maximum lot coverage.

The area included in the calculation of residential density shall include areas of a property reserved for setback requirements, the land for the buildings, private roads and driveways, parking areas and landscaping, amenity areas related to the specific development, but shall exclude all other lands such as natural conservation areas.

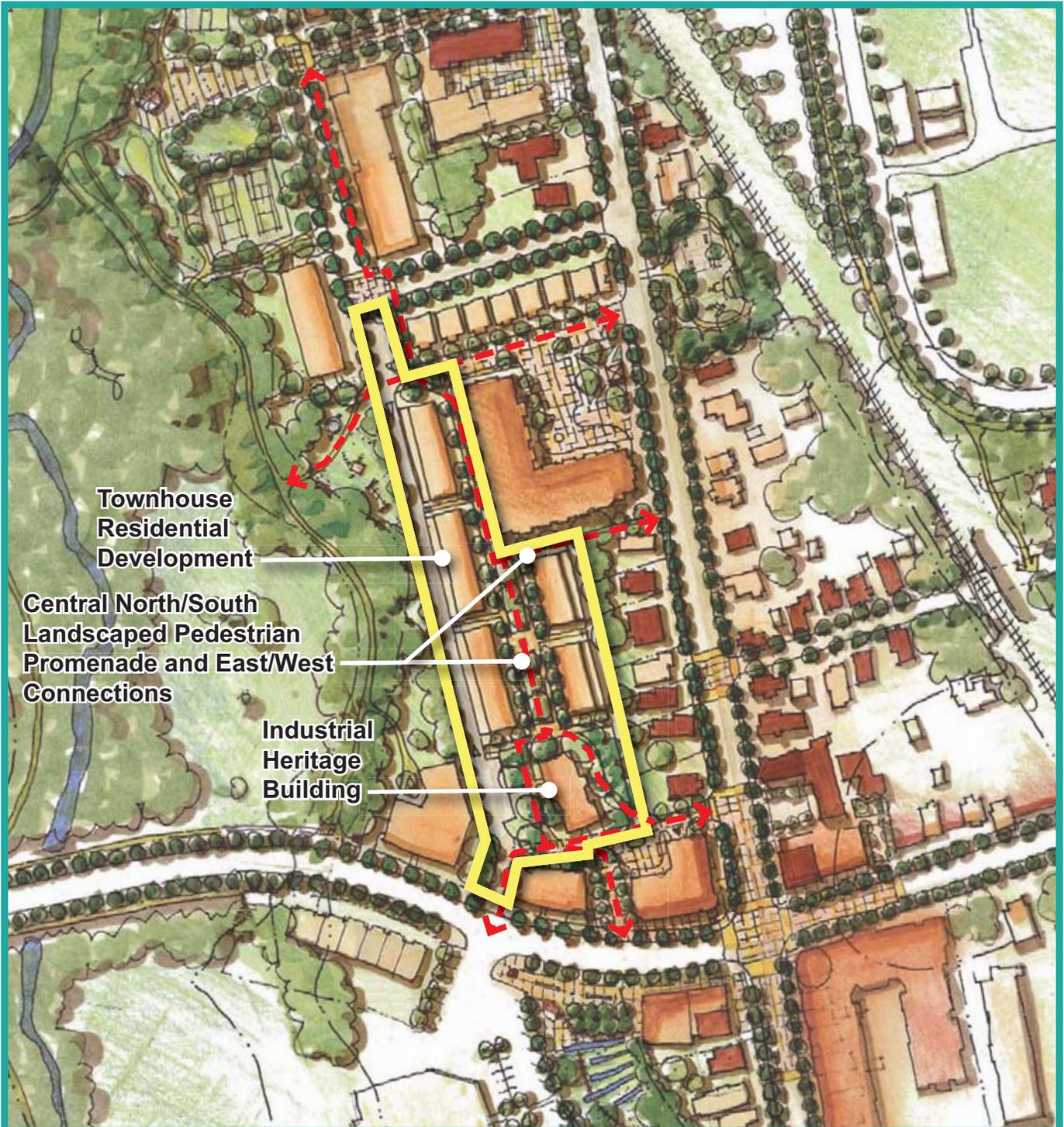
This is the maximum density and may not be achieved in all cases due to site constraints, zoning restrictions of adjacent properties, etc.

An average residential unit size of 1000 square feet (92.9 square metres) is used to calculate the number of units for apartments/condominiums. The townhouse units are calculated based on two to three storey standard townhouse frontages.

Block	Block Area M2	Development M2				Total Units	FSI	Coverage	Observations
		Residential	Commercial	Parking garage	Total				
A	5,213	7,824		4,596	12,421	84	1.5	70%	
B	20,845	26,000	2,250	9,606	37,856	280	1.4	51%	
C	3,400	6,508			6,508	70	1.9	33%	
D	3,259	8,867			8,867	95	2.7	47%	
E	2,286	2,163			2,163	16	0.9	32%	Townhouses
F	3,323	6,653			6,653	72	2.0	50%	
G	3,982	3,800			3,800	41	1.0	24%	
H	12,554	6,804	951		7,755	49	0.6	35%	Townhouses + Apts.
I	2,051	4,540	908		5,448	49	2.7	44%	
J	1,547	2,948	406		3,353	32	2.2	54%	
K	988	1,677	559		2,236	18	2.3	57%	
L	4,440	3,397			3,397	42	0.8	38%	Townhouses
M	79,279		3,349	2,950	6,299	0	0.0	1%	Fairgrounds
N	5,913	4,483			4,483	41	0.8	26%	Townhouses
O	8,911	5,972			5,972	52	0.7	24%	Townhouses
P	6,242	6,920		2,896	9,816	74	1.1	42%	
Q	4,725	2,847			2,847	7	0.6	22%	Single Detached Houses
<b>TOTAL</b>	<b>168,957</b>	<b>101,404</b>	<b>8,422</b>	<b>20,048</b>	<b>129,874</b>	<b>1,022</b>			

▼ Map 11.5.0

*Kipling Avenue - The Rainbow Creek South Industrial Lands Low Density Residential Neighbourhood*



# Appendix: Streetscape Master Plan

## Detailed Streetscape Characteristics

The general characteristics of each street section is outlined in Section 11.5.XX: Block Pattern, Street Network and Linkages. The Detailed Streetscape Guidelines is based on a general concept plan and is meant to be considered in conjunction with the Built Form Framework Guidelines.

The concept identifies opportunities to create more urban and pedestrian oriented streets for the area, which may require additional detail design review and input by various agencies such as the Vaughan Fire Department (VFRS), and Public Works and Emergency Services, to ensure that consideration for road safety, servicing and access are being met.

### Kipling Avenue North - Primary Main Street

- R.O.W. = 26 m
- Two travel lanes = 3.75 m (to accommodate buses)
- Parking lane = 2.5 m (on east side of street)
- Two bicycle lanes = 1.5 m (one on each side of the street)
- Average pavement width = 13 m
- Boulevard width = 6.5 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 3 m

### Kipling Avenue South - Primary Main Street

- R.O.W. = 26 m
- Two travel lanes = 3.3 m (one as a left turn lane at intersection)
- Two bus and travel lanes = 3.75 m
- On-street Parking = (off peak parking use of travel lane on east side only)
- Left turning lane = at Kipling Avenue and the Highway 7 intersection
- Two Bicycle lanes = 1.45 m (one on each side of the street, the east lane to have a dedicated curb edge or rumble strip to the adjacent travel lane)
- Average pavement width = 17.0 m
- Boulevard width = 4.5 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 3 m

### **Woodbridge Avenue East - Primary Main Street**

- R.O.W. = 20 m
- One Travel lane = 3.3 m (travel and turning lane)
- Two Bus lanes = 3.75 m (shared bus, bike and travel lane)
- Parking lane = 2.5 m (lay-by parking beyond intersection)
- Left turning lane = at Kipling Avenue
- Average pavement width = 10.8 m (15.8 m including lay-by parking east of intersection)
- Minimum setback for buildings = 0 m - 3 m (3 m minimum setback for the north/west corner of Kipling Avenue and Woodbridge Avenue)
- Boulevard width = 4.0 m (on north side of the street), 5.2 m (on south side of street, including bump-out)
- Sidewalks = 1.5 m (on both sides of the street)

### **Woodbridge Avenue West - Primary Main Street**

- R.O.W. = 26 m
- Two Travel lanes = 3.3 m (travel and dedicated turning lane)
- Two Bus lanes = 3.75 m (shared bus, bike and travel lane)
- Parking lane = 2.5 m (lay-by parking beyond intersection)
- Left turning lane = at Kipling Avenue
- Average pavement width = 14.1 m (19.1 m including lay-by parking east of intersection)
- Boulevard width = 5.95 m (on both sides of the street)
- Sidewalks = 1.5 m (on both sides of the street)
- Minimum setback for buildings = 0 m - 3 m (3 m minimum setback for the north/west corner of Kipling Avenue and Woodbridge Avenue)

### **Parkside Drive - Secondary Street**

- R.O.W. = 20 m
- Two Travel lanes = 3.25 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 9 m
- Boulevard width = 5.50 m (on both sides of the street)

- Sidewalks = 3.5 m zone (on both sides of the street with integrated street tree planting and landscaping)
- Minimum setback for buildings = 3 m

### **Rainbow Creek Road - Local Connector**

- R.O.W. = 20 m
- Two Travel lanes = 3.25 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 9 m
- Boulevard width = 5.50 m (on both sides of the street)
- Sidewalks = 3.5 m zone (on both sides of the street with integrated street tree planting and landscaping)
- Minimum setback for buildings = 3 m

### **Industry Avenue - Local Connector**

- R.O.W. = 17.5 m
- Two travel lanes = 3.0 m
- Parking lane = 2.5 m on north side
- Left turning lane = none
- Average pavement width = 8.5 m
- Minimum setback for buildings = 3 m
- Sidewalks = 1.5 m (on north side of the street)

### **Porter Avenue East - Special Character Street**

- R.O.W. = 20 m
- Two travel lanes = 3.5 m
- Parking lanes = none
- Left turning lane = none
- Average pavement width = 7.0 m
- Sidewalks = 8.5 m pedestrian promenade on the south side (2.5 m and 3.0 m double sidewalk with integrated pedestrian furnishings), and a 4.5 m promenade on the north side (2.5 m and 2.0 m double sidewalk)
- Minimum setback for buildings = 4.5 m

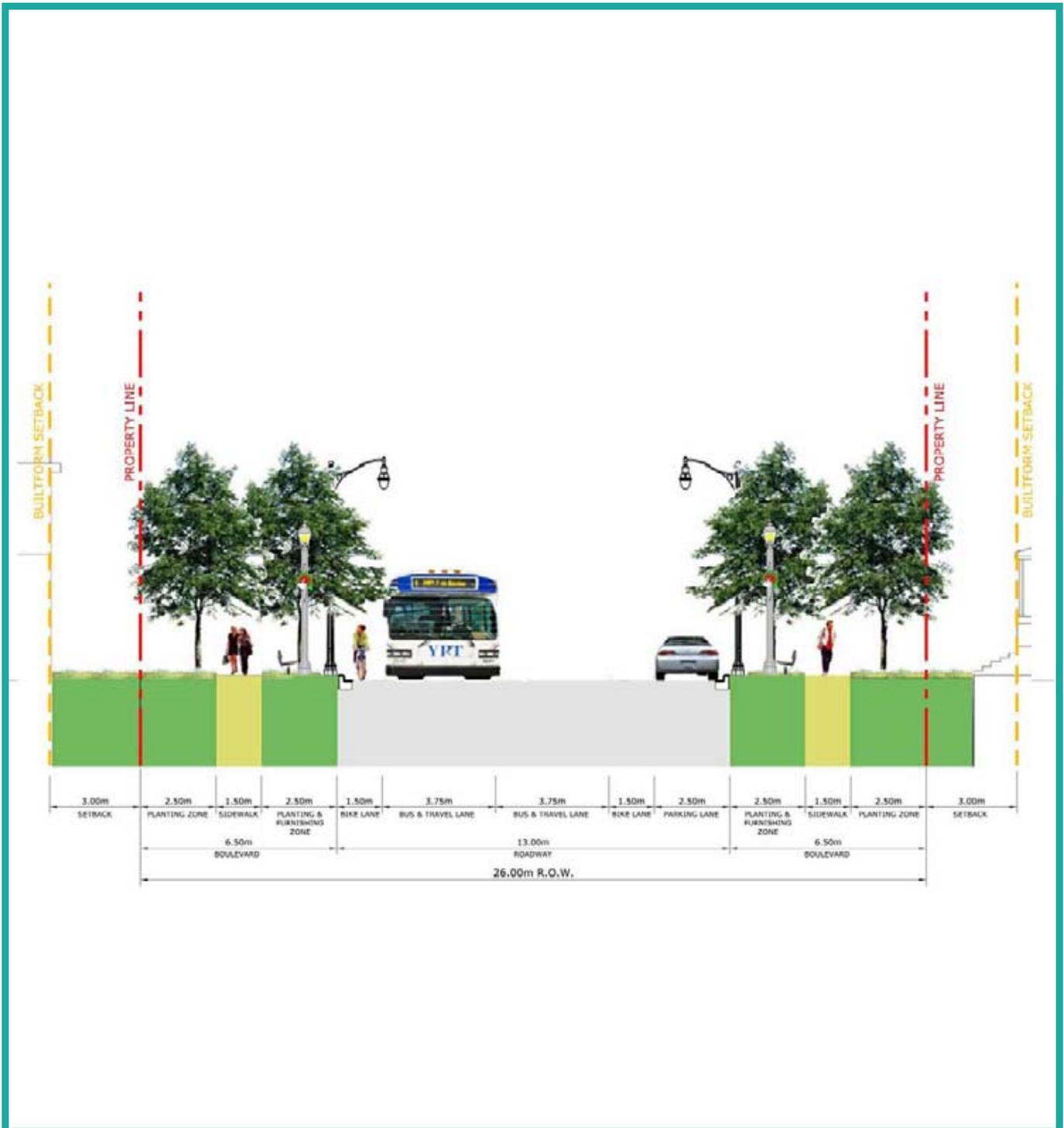
### **Porter Avenue West - Special Character Street**

- R.O.W. = 17.5 m
- One travel lane and lay-by parking = 6.0 m (one way direction east to Kipling Avenue, and lay-by parking on north side)
- Left turning lane = none
- Average pavement width = 6.0 m (including lay-by parking)
- Sidewalks = 2.5 m on the north side of the street, and 1.5 m on the south side where possible, (considering the existing heritage church pinch point)
- Minimum setback for buildings = 4.5 m (with the exception of the existing heritage church 1.5 m pinch point to the R.O.W.)

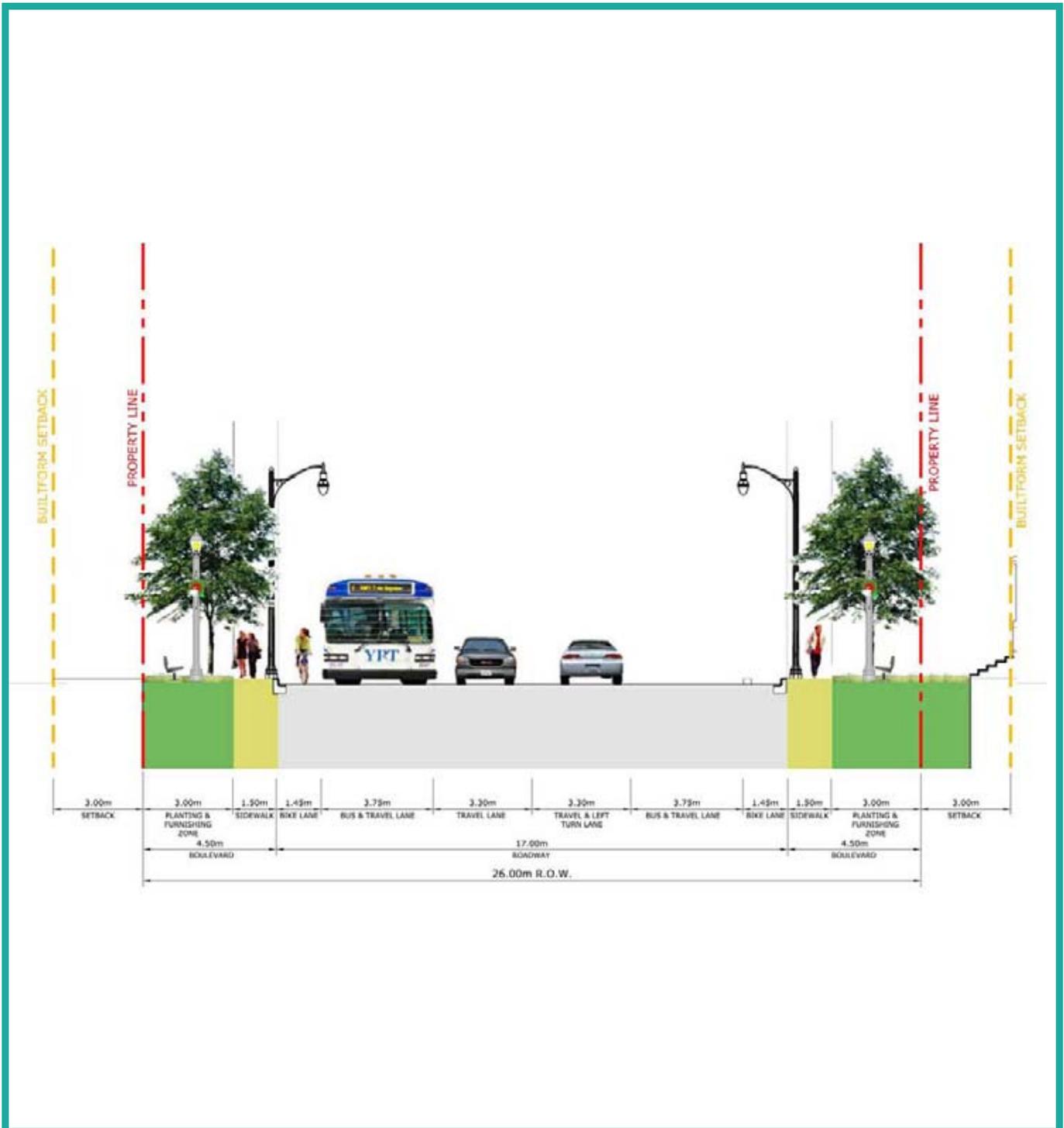
### **Residential Streets - Typical**

- R.O.W. = 20 m
- Two travel lanes = 3.0 m
- Parking lane = 2.5 m
- Left turning lane = none
- Average pavement width = 8.5 m
- Minimum setback for buildings = 4.5 m
- Sidewalks = 2.0 m (on both sides of the street)

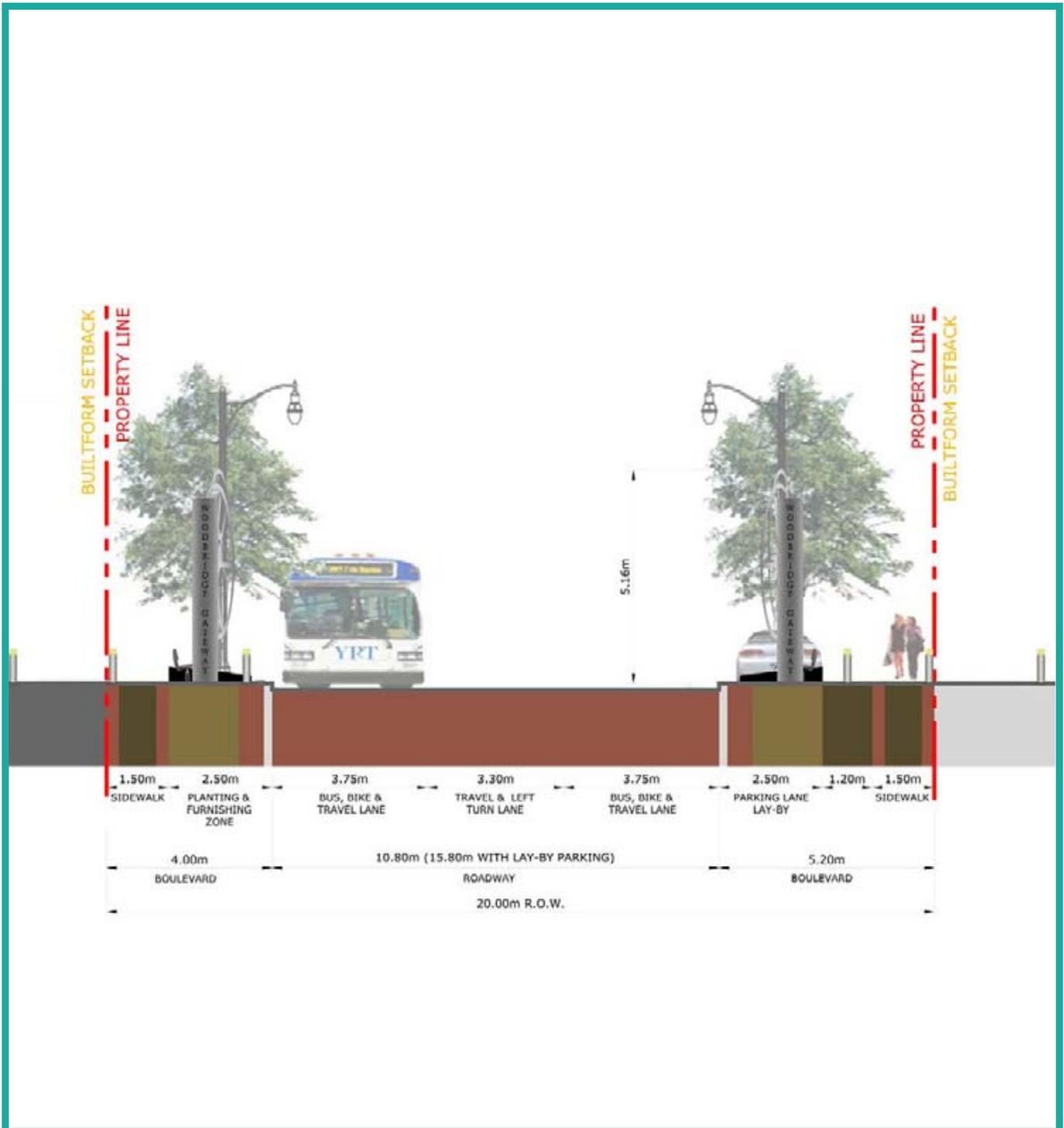
▼ Figure 11.5.A  
 Kipling Avenue - Street Section - Kipling Avenue North - Primary Main Street - View North



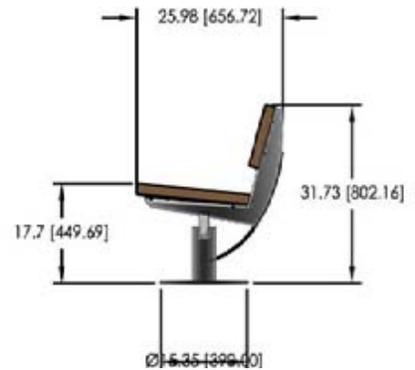
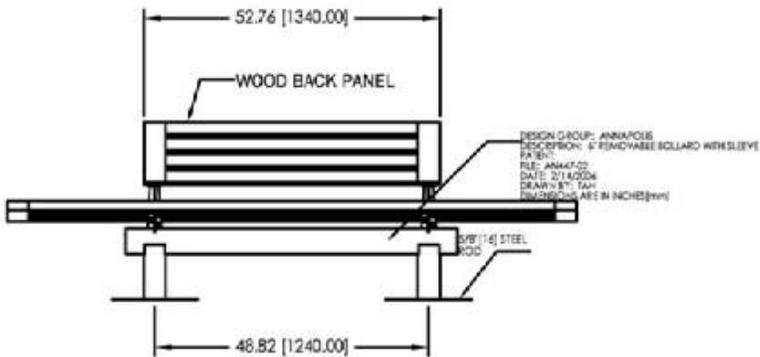
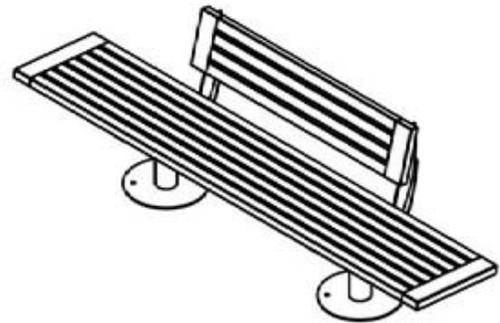
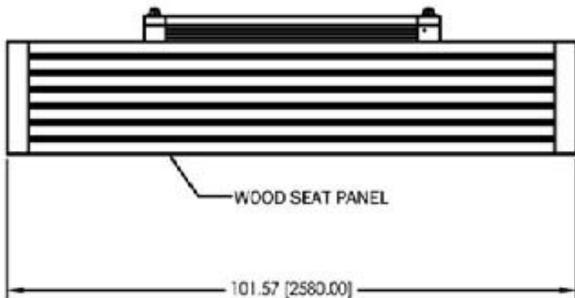
▼ Figure 11.5.B  
 Kipling Avenue - Street Section - Kipling Avenue South - Primary Main Street - View North



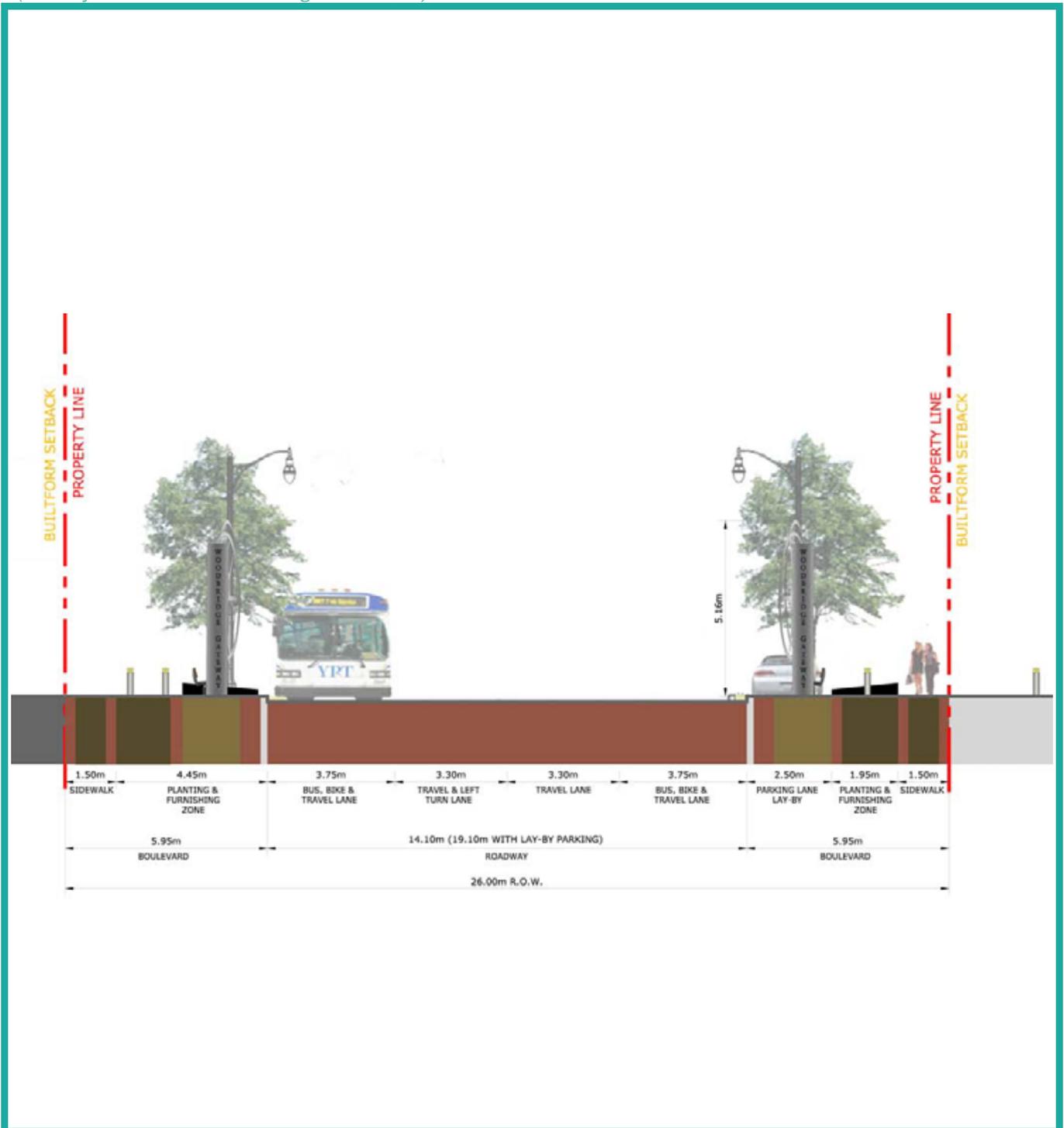
▼ Figure 11.5.C  
 Kipling Avenue - Street Section/Elevation - Woodbridge Avenue East - Primary Main Street and Gateway Details - View East



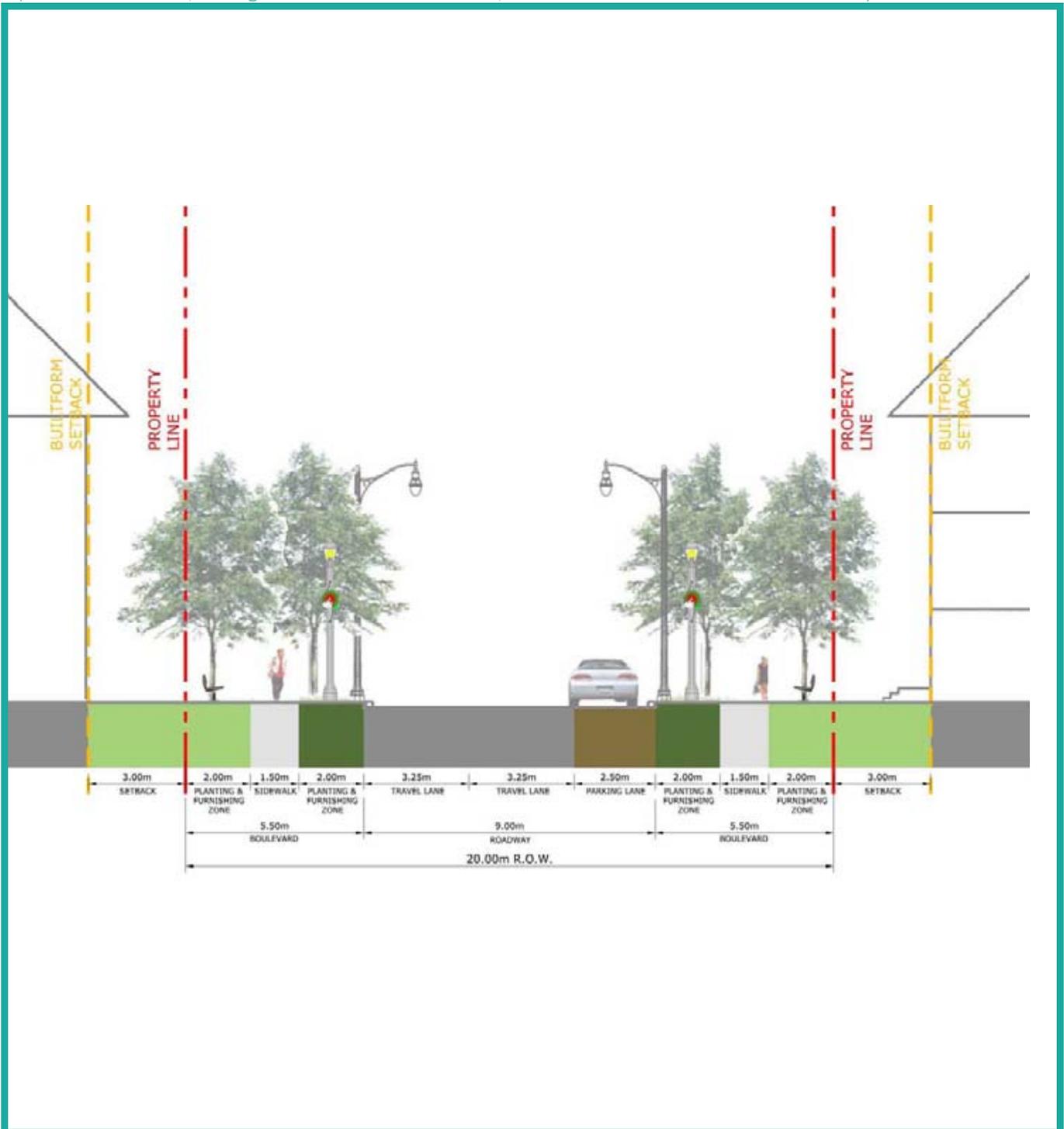
Kipling Avenue - Street Section/Elevation - Woodbridge Avenue East - Primary Main Street and Gateway Details - View East



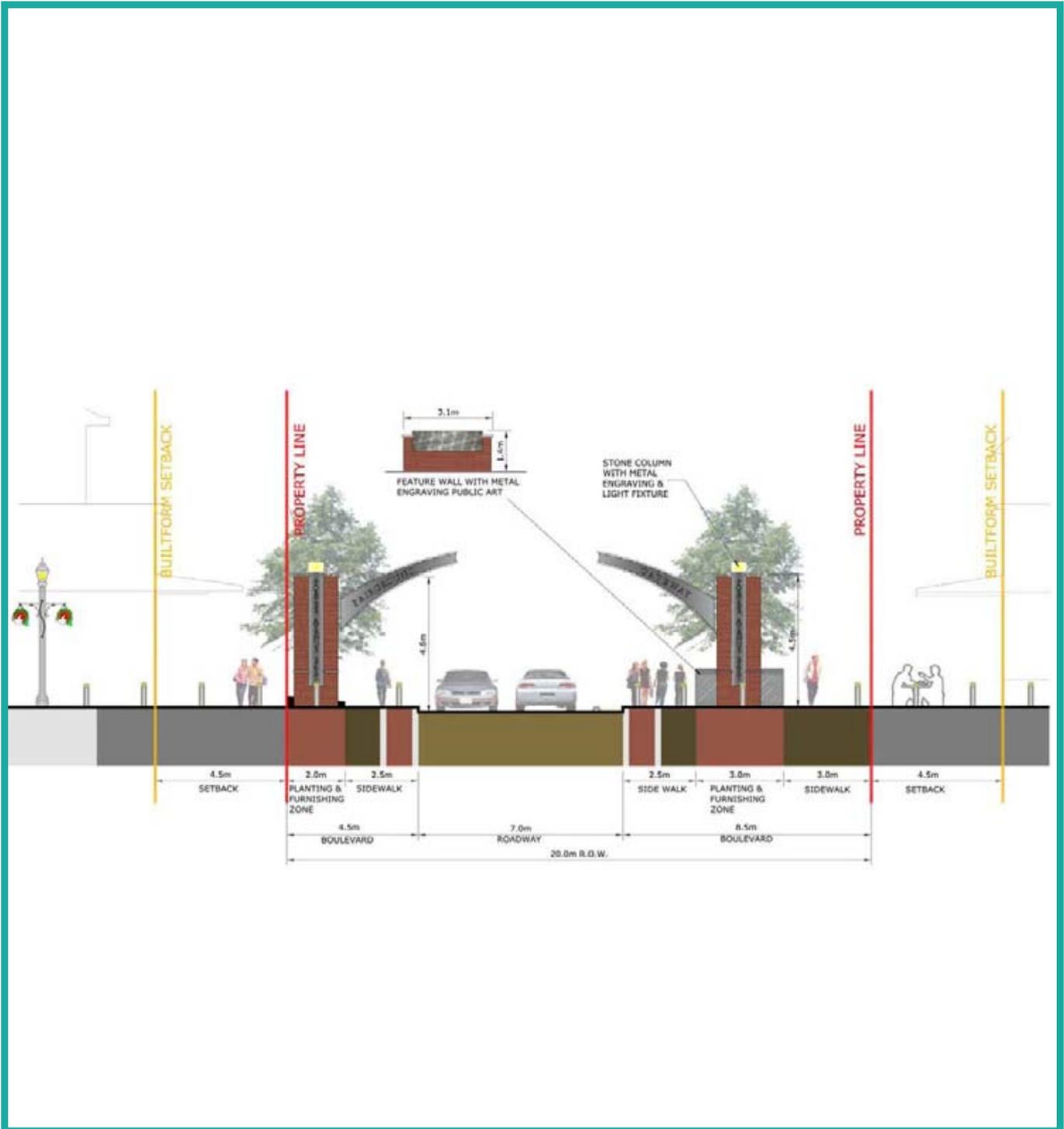
▼ Figure 11.5.D  
 Kipling Avenue - Street Section/Elevation - Woodbridge Avenue West - Primary Main Street - View West  
 (Gateway details same as Woodbridge Avenue East)



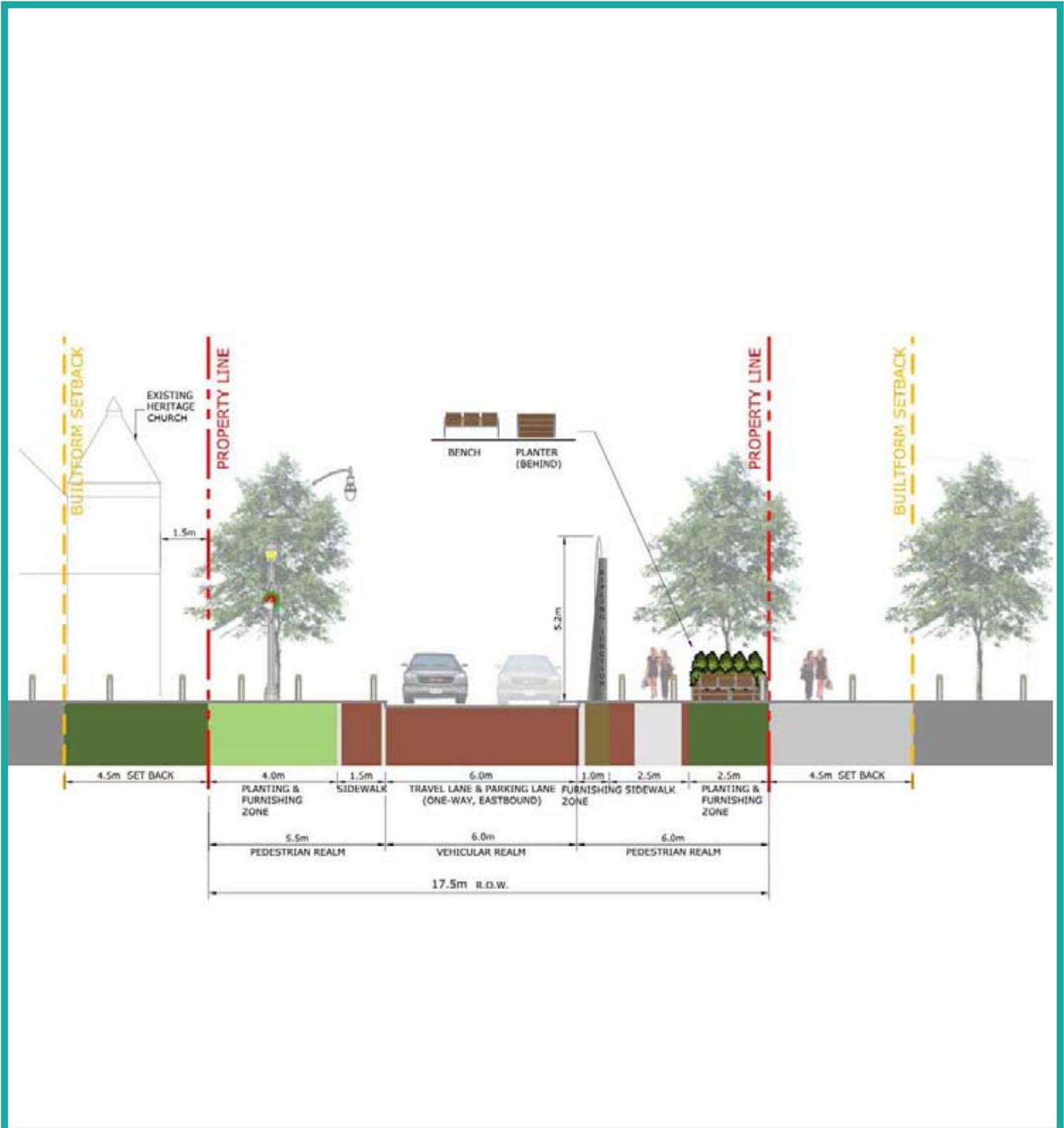
▼ Figure 11.5.E  
 Kipling Avenue - Street Section/Elevation - Parkside Drive - View West  
 (Rainbow Creek Road, although considered a local connector, has the same cross section as Parkside Drive)



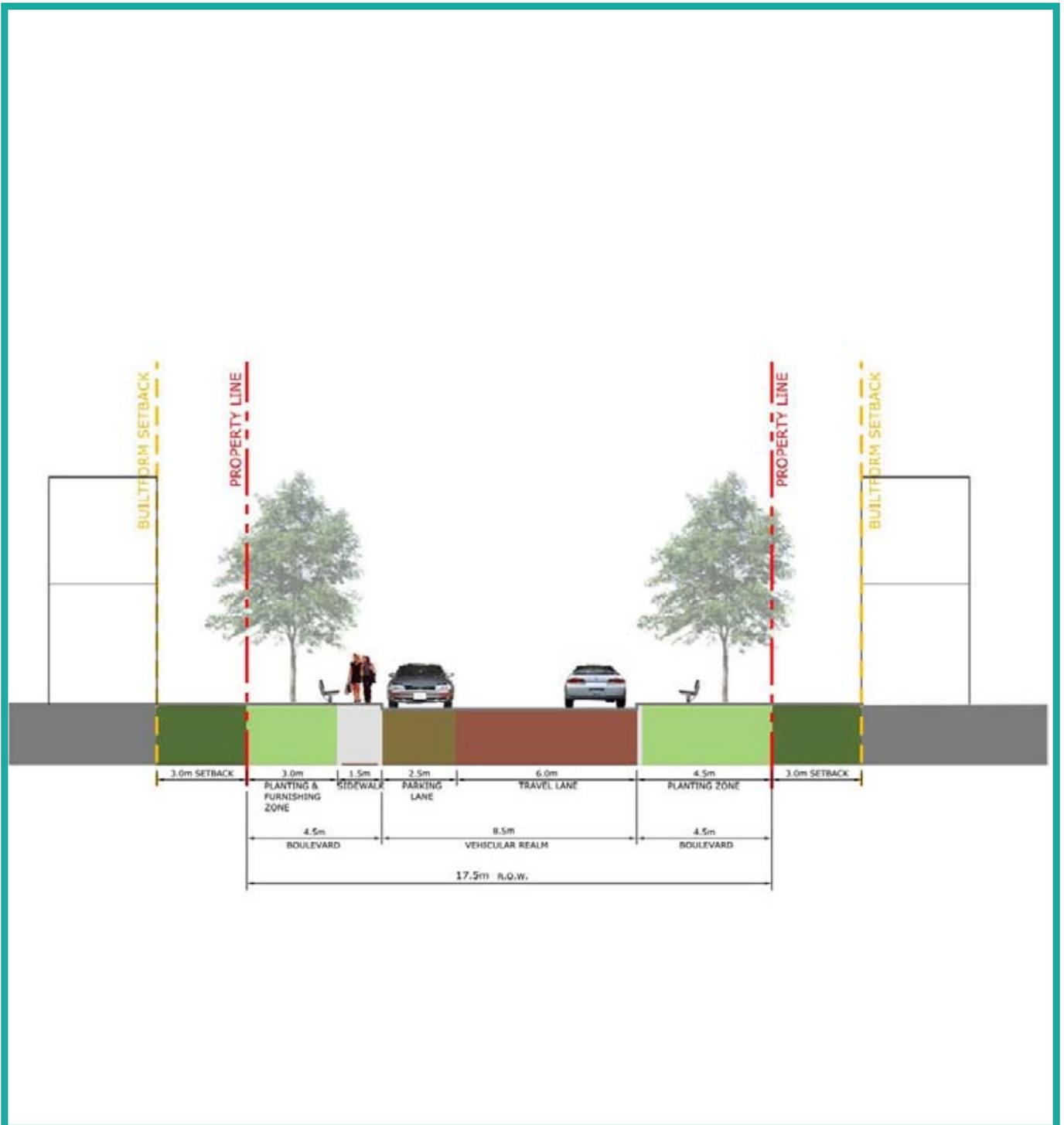
▼ Figure 11.5.F  
 Kipling Avenue - Street Section/Elevation - Porter Avenue East - Special Character Street - View East

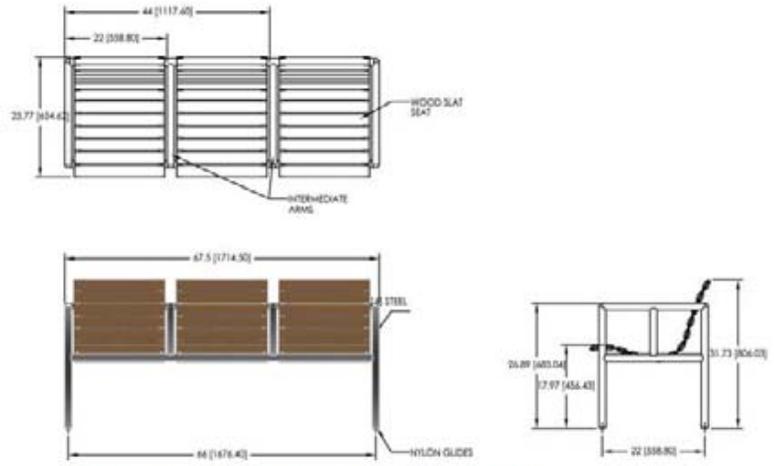


▼ Figure 11.5.G  
 Kipling Avenue - Street Section/Elevation - Porter Avenue West - Special Character Street - View West

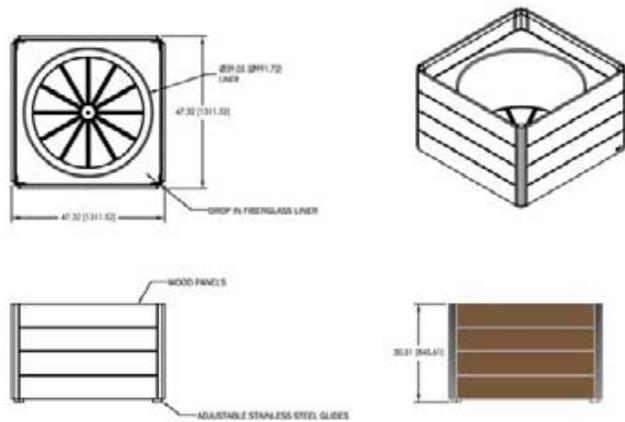


▼ Figure 11.5.H  
 Kipling Avenue - Street Section/Elevation - Industry Avenue - Local Connector - View East



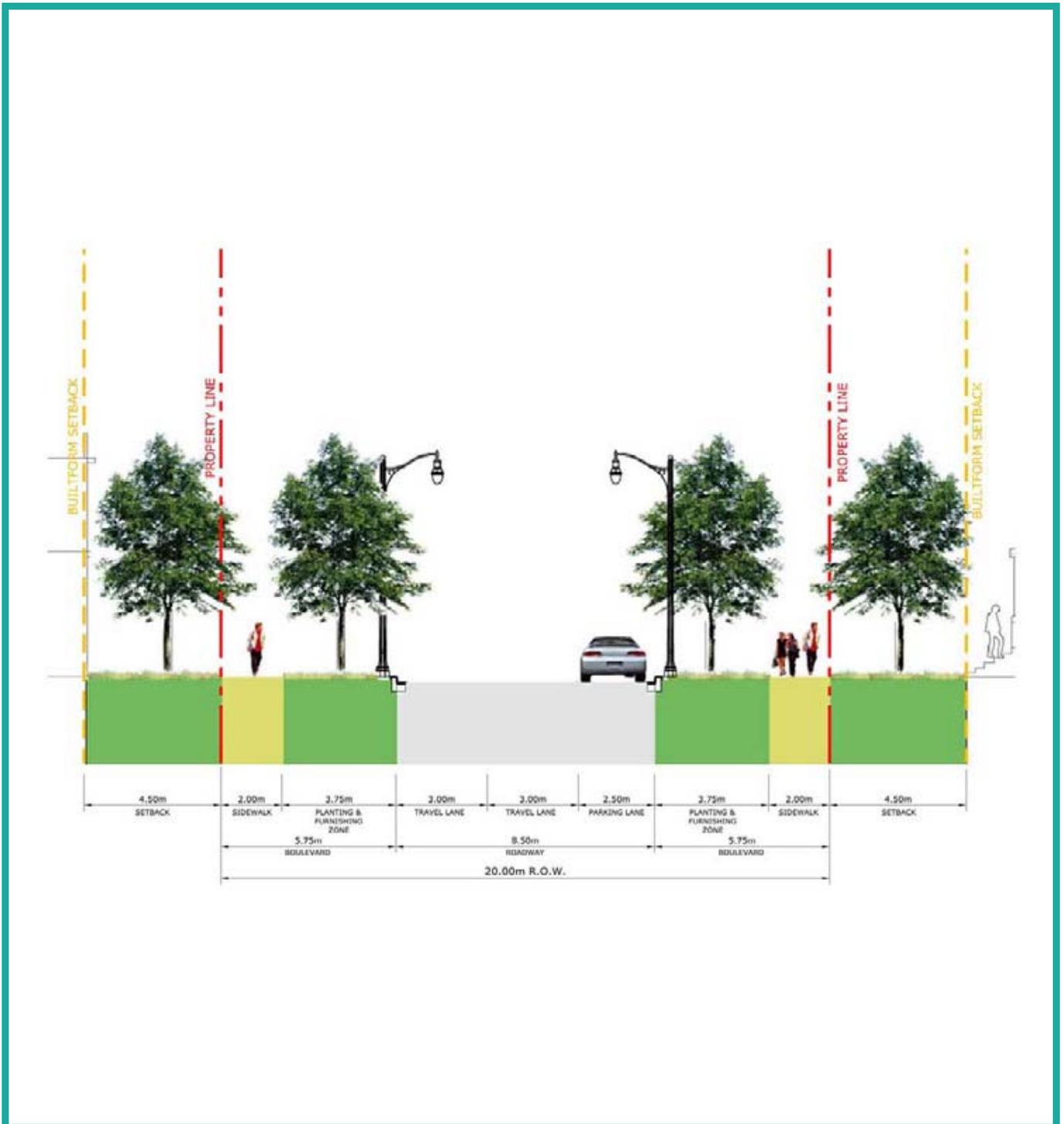


**BENCH - KIPLING JUNCTION TYPICAL**

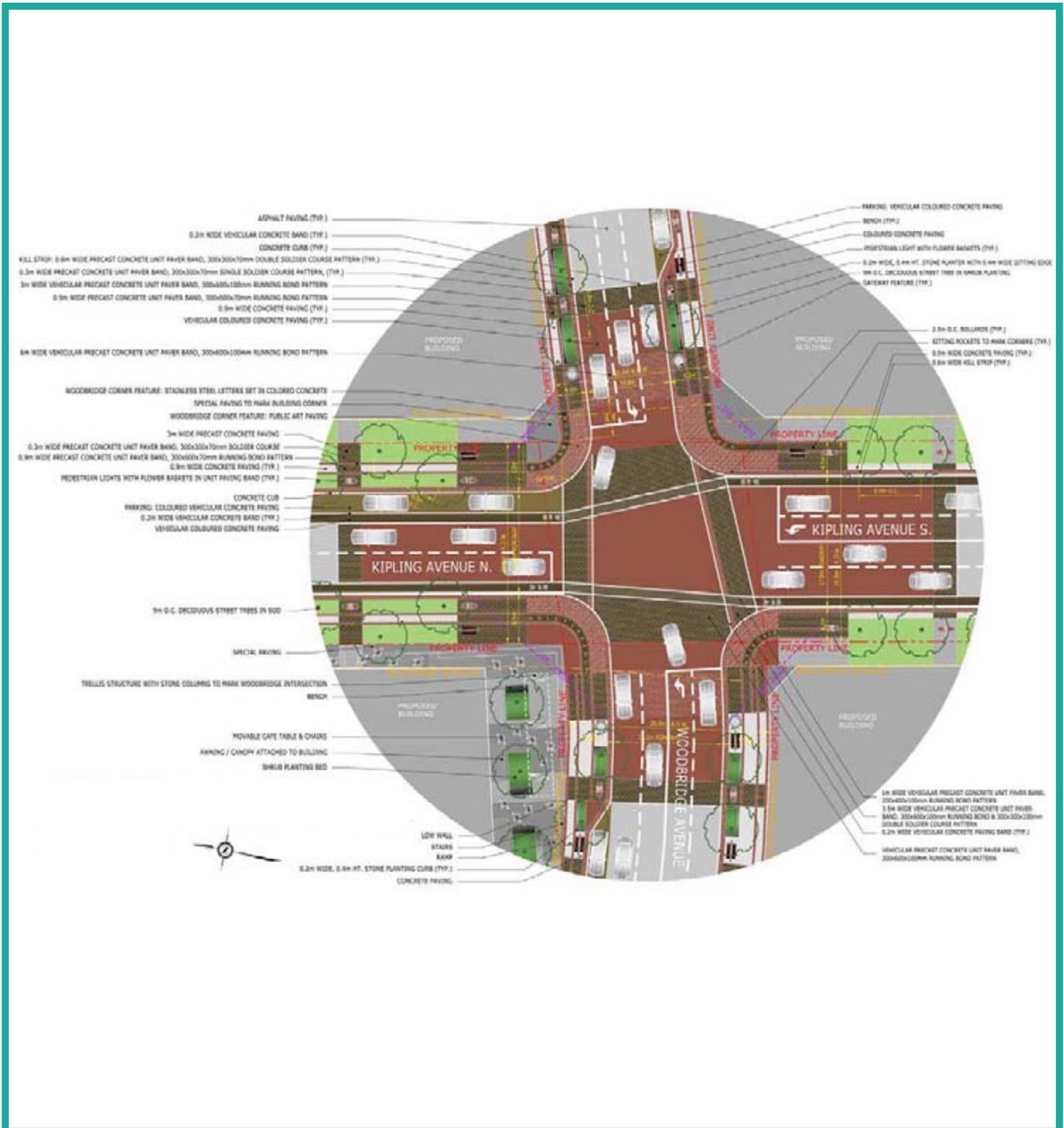


**PLANTER - KIPLING JUNCTION TYPICAL**

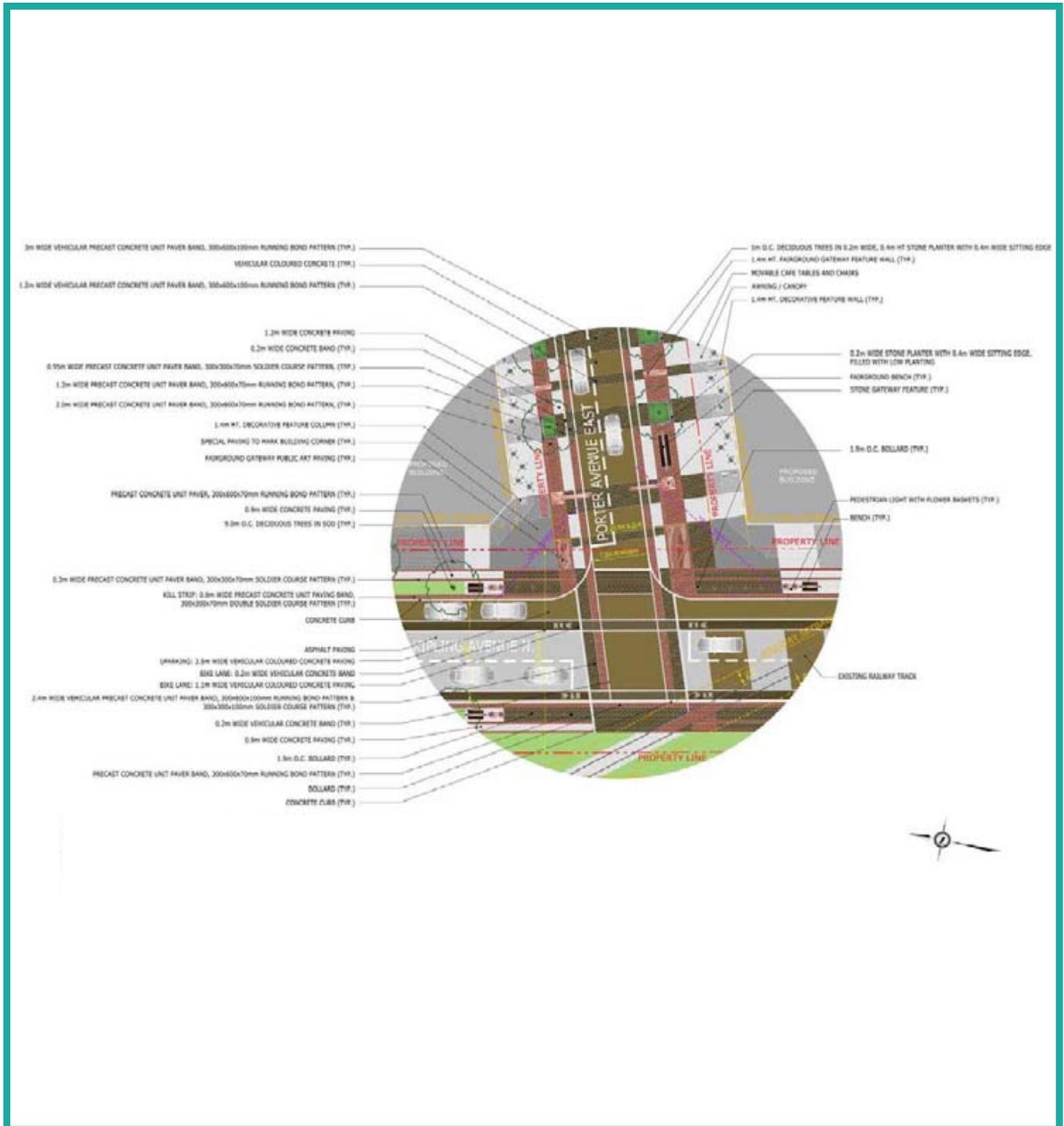
▼ Figure 11.5.1  
 Kipling Avenue - Street Section/Elevation - Residential Street - Typical



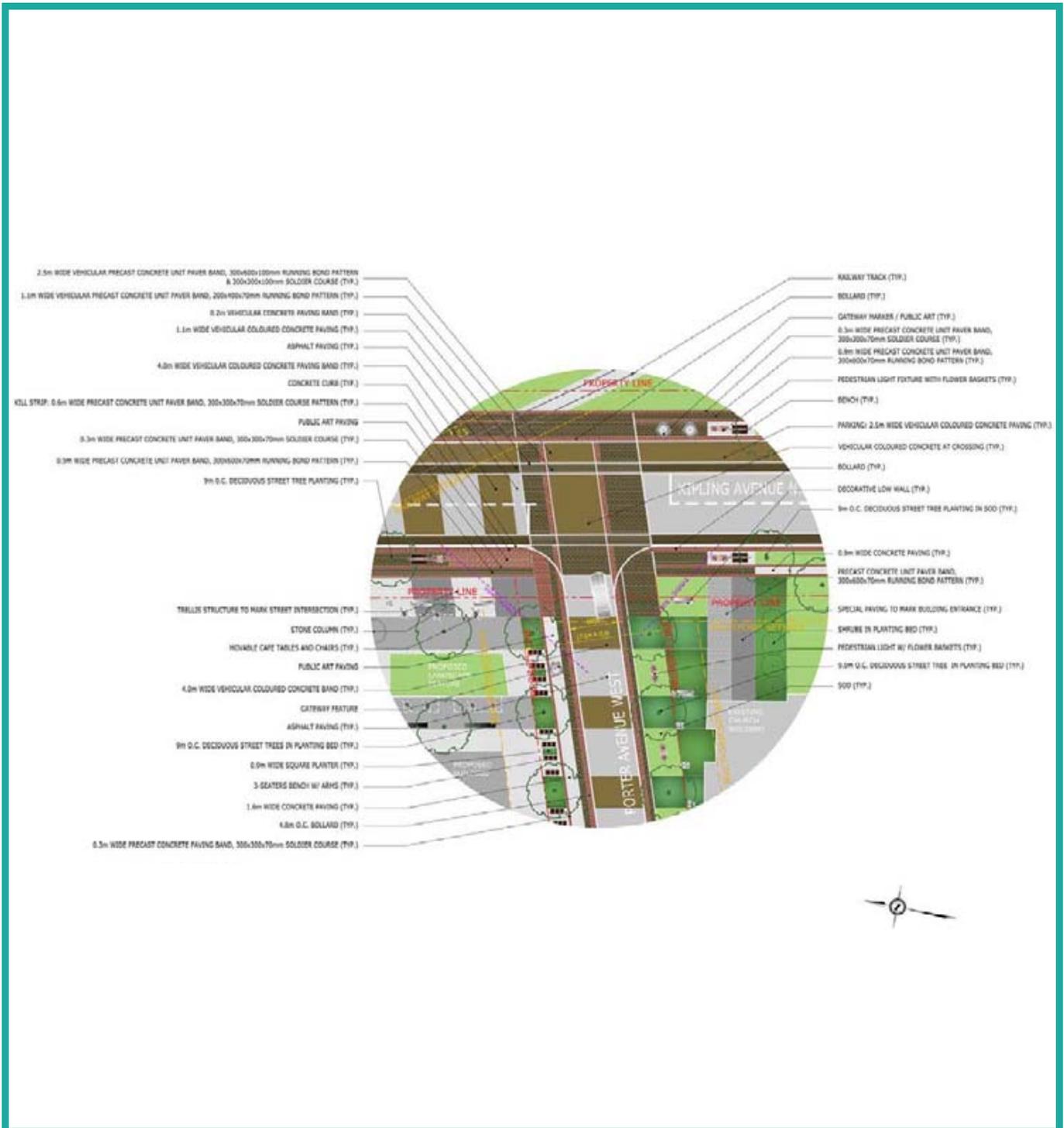
▼ Figure 11.5.J  
 Kipling Avenue - Woodbridge Avenue and Kipling Avenue Gateway Node



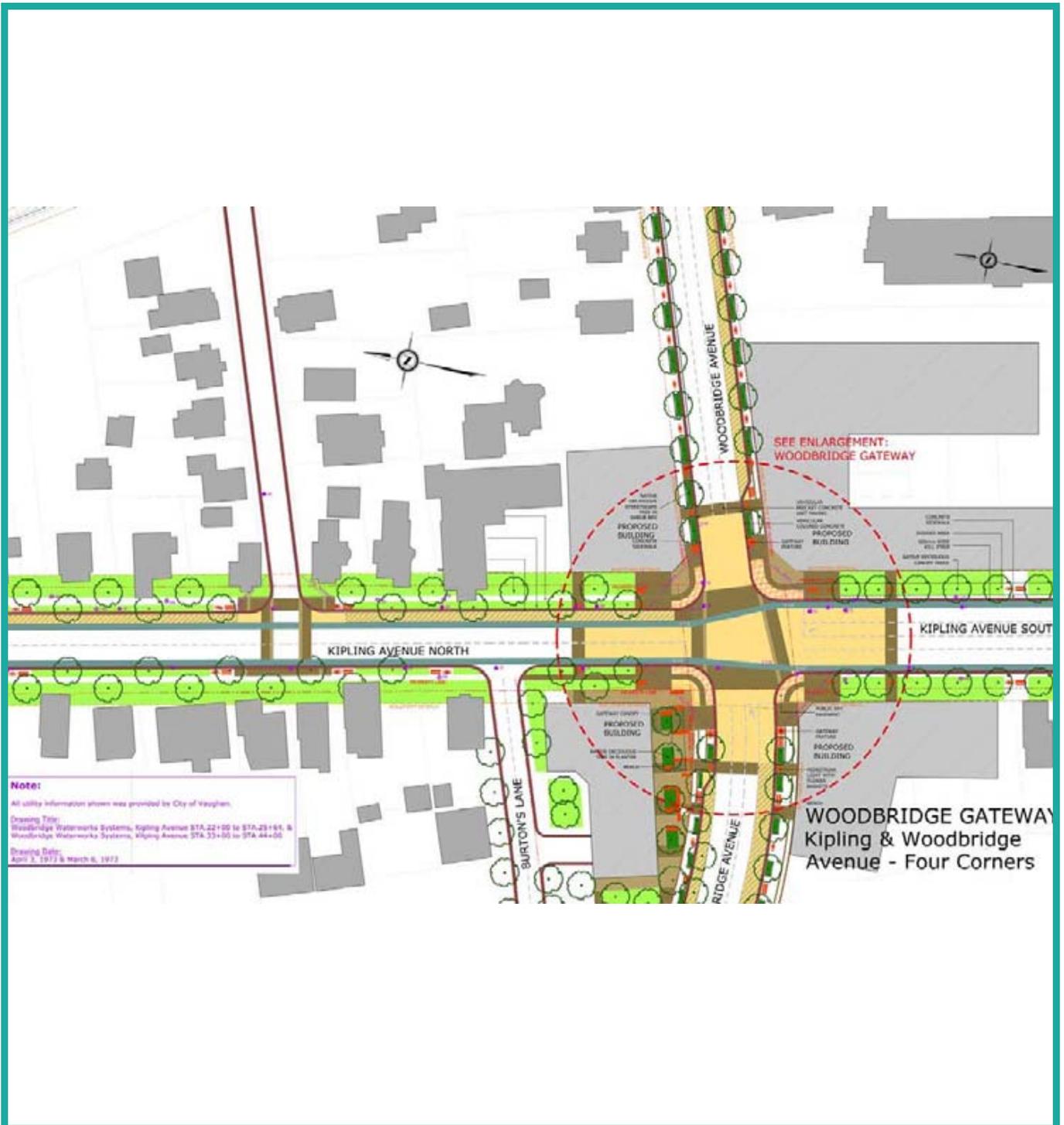
▼ Figure 11.5.K  
 Kipling Avenue - Porter Avenue East - The Faiground Gateway



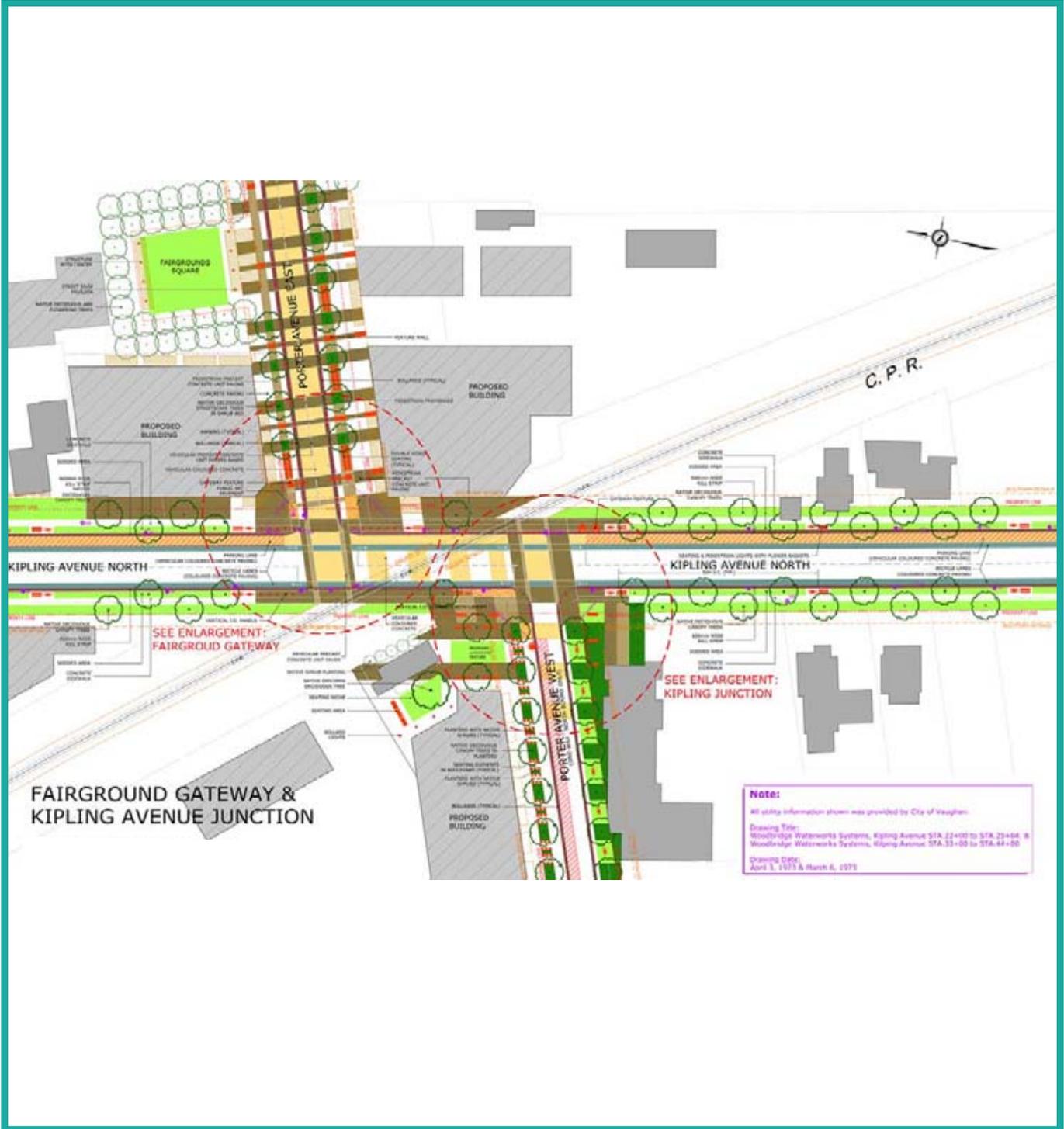
▼ Figure 11.5.L  
 Kipling Avenue - Porter Avenue West - The Junction



▼ Figure 11.5.M  
 Kipling Avenue - Woodbridge Avenue and Kipling Avenue Gateway Master Plan



▼ Figure 11.5.N  
 Kipling Avenue - Porter Avenue East - The Junction Master Plan





# 12 AREA SPECIFIC POLICIES



## 12.1 Lands Subject to Area Specific Policies

### 12.1.1 Lands subject to Area Specific Policies

It is the policy of Council that:

- 12.1.1.1. The Heritage Conservation Districts identified on Schedule 14-B are subject to policies set out in Section 12.2 of this Plan.
- 12.1.1.2. The lands known as the Keele Valley Landfill Area are identified on Schedule 14-B and are subject to policies set out in Section 12.3 of this Plan.
- 12.1.1.3. The lands known as the Kleinburg Core are identified on Schedule 14-B and are subject to policies set out in Section 12.4 of this Plan.
- 12.1.1.4. Adult Entertainment uses are permitted on certain lands identified on Schedule 14-B, subject to the policies set out in Section 12.5 of this Plan.
- 12.1.1.5. The lands known as Vaughan Mills Mall identified on Schedule 14-B are subject to policies set out in Section 12.6 of this Plan.
- 12.1.1.6. The lands known as the North East Quadrant of Vellore Centre are identified on Schedule 14-B and are subject to policies set out in Section 12.7 of this Plan.
- 12.1.1.7. The lands known as Nashville Heights are identified on Schedule 14-B and are subject to policies set out in Section 12.8 of this Plan.
- 12.1.1.8. The lands known as Yonge Street Corridor in Thornhill are identified on Schedule 14-B and are subject to policies set out in Section 12.9 of this Plan.
- 12.1.1.9. The lands know as the Centre Street Corridor are identified on Schedule 14-B and are subject to the policies set out in Section 12.10 of this Plan.
- 12.1.1.10. The lands at Kipling Avenue and Highway 7 are identified on Schedule 14-B and are subject to policies set out in Section 12.11 of this Plan.

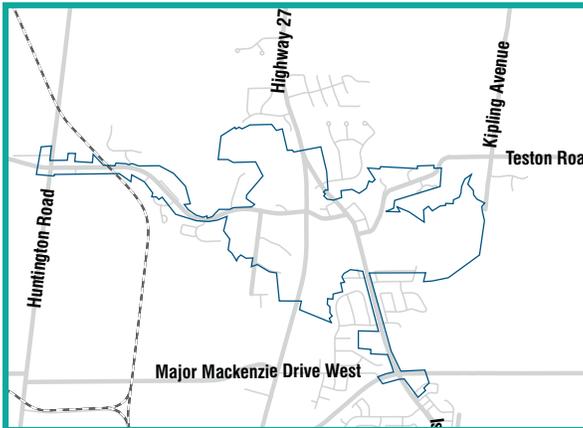
- 12.1.1.11. The lands at Bathurst and Centre Streets are identified on Schedule 14-B and are subject to policies set out in Section 12.12 of this Plan.
- 12.1.1.12. The lands known as Huntington Business Park are identified on Schedule 14-B and are subject to the policies set out in Section 12.13 of this Plan.

## 12.2 Heritage Conservation Districts

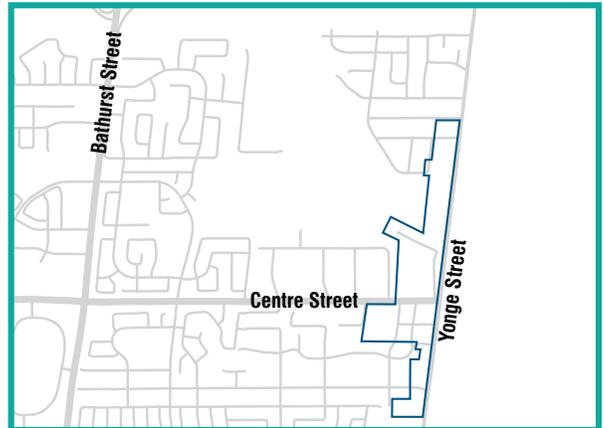
### 12.2.1 Heritage Conservation Districts

12.2.1.1. Land use planning decisions within the areas identified on Schedule 14 as Heritage Conservation Districts shall conform to the requirements of the applicable Heritage Conservation District Plan noted below and identified on Map 12.2.A:

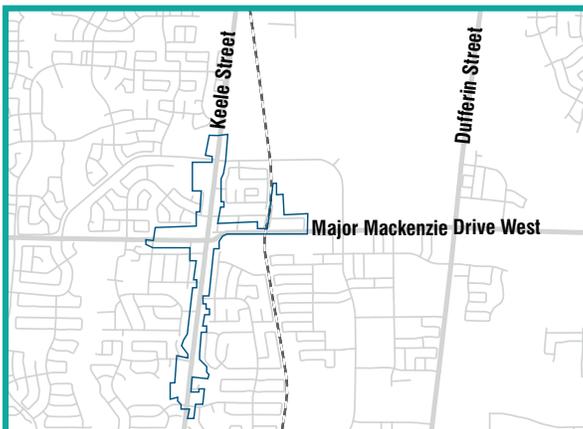
- a. Kleinburg-Nashville Conservation District Plan, By-Law 268–2003
- b. Thornhill Heritage Conservation District Plan, By-Law 323–2007
- c. Village of Maple Heritage Conservation District Plan, By-Law 167-2007
- d. Woodbridge Heritage Conservation District Plan, By-Law 101-2009



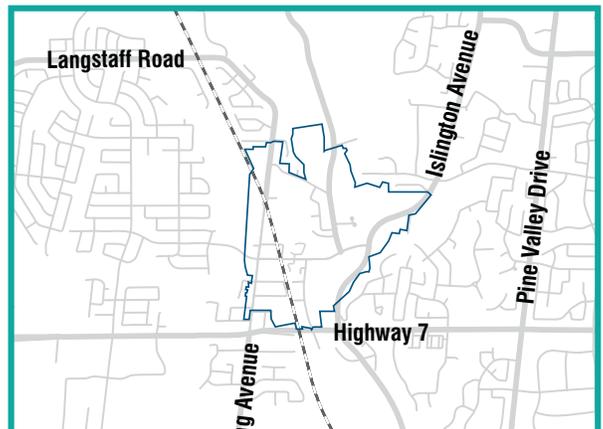
Kleinburg Heritage Conservation District Plan Area



Thornhill Heritage Conservation District Plan Area



Maple Heritage Conservation District Plan Area



Woodbridge Heritage Conservation District Plan Area

▲ Map 12.2.A: Areas Subject to Heritage Conservation District Plans

## 12.3 Keele Valley Landfill Area

### 12.3.1 General

- 12.3.1.1. The purpose of this policy is to support the ongoing monitoring and remediation activities of the area identified on Map 12.3.A as the former Keele Valley Landfill Area. Vaughan will coordinate the safe reuse of waste disposal sites with landowners, developers and other government agencies, and will support the implementation of contamination assessments, identification of potential future uses and clean up strategies.

### 12.3.2 Land Use Policy Area 1

- 12.3.2.1. The area identified on Map 12.3.A as Policy Area 1 comprises the existing fill area of the former Keele Valley Landfill Site, recognizing its former use as a waste disposal area subject to a maximum approved volumetric capacity of 33,125,254 cubic metres of waste, daily and interim cover.
- 12.3.2.2. Vaughan encourages the continual rehabilitation of the site for the intended after-use as a public open-space.
- 12.3.2.3. The design, operation, maintenance, monitoring, remediation, and rehabilitation of the former Keele Valley Landfill Site shown as Policy Area 1 is subject to the provisions of the Environmental Protection Act and its regulations and other applicable legislation.
- 12.3.2.4. Notwithstanding policy 9.2.1.1, the following uses are permitted within Policy Area 1:
- a. The extraction of sand and gravel only for the completion of the former Keele Valley Landfill Site;
  - b. Rehabilitation on a progressive basis;
  - c. Extraction and processing of landfill gas; and,
  - d. Upon completion of the sanitary landfilling operation, public open space uses in combination with uses related to the monitoring and mitigation of the effects of waste disposal activities, are permitted.
- 12.3.2.5. Council shall endeavour to provide notice and an opportunity for the public to provide comment on proposed developments and agreements related to the Keele Valley Landfill Area prior to the adoption of any Council-associated approval.

## **Policy Area 2**

- 12.3.2.6. The area identified on Map 12.3.A as Policy Area 2 comprises the Primary Buffer Area of the former Keele Valley Landfill Site and is intended to provide for its long term use as an area for the monitoring and mitigation of the effects of waste disposal.
- 12.3.2.7. The objectives for Policy Area 2 are to:
- a. provide for on-going monitoring and control of groundwater, stormwater and landfill gas conditions around the former landfill site; and,
  - b. confirm that the design operation, maintenance, monitoring, remediation and rehabilitation of the lands in Policy Area 2 are subject to the provisions of the Environmental Protection Act, and its regulations and other applicable legislation.
- 12.3.2.8. Notwithstanding policy 9.2.1.1, Policy Area 2 may only be used for monitoring and mitigation of the effects of waste disposal and activities required for the remediation, and rehabilitation of the former Keele Valley Landfill Site, or as may be required to implement an expansion required pursuant to Section 18 of the Waste Management Act.
- 12.3.2.9. No industrial activities or extraction of materials such as sand or gravel will be permitted, other than those works or activities essential to the primary objective of establishing the necessary screening of the former Keele Valley Landfill Site in conformance with any conditions imposed by Certificate A-230610, or as may be required to implement any conditions imposed through approvals pursuant to Section 18 of the Waste Management Act.
- 12.3.2.10. Despite anything in the Official Plan, on land located in Part Lot 22, Concession 3, City of Vaughan, an electrical power generating facility fuelled by landfill gas shall be permitted, in accordance with the terms and conditions of a Certificate of Approval as may be required by the Ministry of the Environment and Energy pursuant to the Environmental Protection Act, and in accordance with the terms and conditions of Powerstream with respect to connecting to the local hydro grid.

## **Policy Area 3**

- 12.3.2.11. The area identified on Map 12.3.A as Policy Area 3 is considered to be subject to significant land use constraints, due to the effects of waste disposal.

- 12.3.2.12. The objectives for Policy Area 3 are to:
- a. ensure that lands other than Policy Area 1 and Policy Area 2 are maintained for such purposes as groundwater, stormwater and landfill gas monitoring and control, and the attenuation of leachate; and,
  - b. confirm that the design, operation, maintenance, remediation and rehabilitation of the lands in Policy Area 3 are subject to the provisions of the Environmental Protection Act, its regulations and other applicable legislation.
- 12.3.2.13. Notwithstanding policy 9.2.1.1, Policy Area 3 may only be used for the monitoring and mitigation of the effects of waste disposal, and other uses such as drainage works and passive recreation uses including pedestrian and bicycle trails. On lands identified as Policy Area 3 south of Teston Road permitted uses shall also include a golf course and ancillary uses and a golf driving range/training centre and ancillary uses. The disposal of waste shall not be permitted in Policy Area 3.
- 12.3.2.14. Prior to rezoning lands or as a condition of lifting a Holding Zone (H) for any public open space uses permitted within Policy Area 3 the requirements of the Waste Disposal Assessment Area described in Section 12.3.3 of this Plan shall be satisfied. A change in actual use will require an amendment to the MOE Certificate of Approval A-230610.

#### **Policy Area 4**

- 12.3.2.15. In addition to the uses permitted under policy 9.2.1.1, within Policy Area 4, identified on Map 12.3.A, permitted uses also include those related to the monitoring and mitigation of the effects of waste disposal activities.

#### **200 McNaughton Road**

- 12.3.2.16. Notwithstanding policy 9.2.1.1 the following uses are permitted on the lands located at 200 McNaughton Road and bounded by Major Mackenzie Drive, the CN Rail Line and McNaughton Road, and identified on Map 12.3.A:
- a. Industrial uses that do not require outside storage including processing, manufacturing, assembling, warehousing;
  - b. Office uses;
  - c. Limited retail commercial facilities for the sale of goods manufactured and/or assembled on the premises;
  - d. Uses related to the monitoring and mitigation of waste disposal activities;

- e. retail warehousing; and,
  - f. Other compatible uses which do not conflict with development or operation of the primary industrial uses and other commercial uses directly servicing the industrial enterprises and their employees including financial institutions, restaurants, and personal service establishments.
- 12.3.2.17. Notwithstanding 12.3.2.16 above, for the lands at 200 MacNaughton Road, commercial development including retail and retail warehousing uses shall also be permitted provided that the maximum total gross floor area devoted to all such uses does not exceed 25,000 square metres. Such uses shall be further defined in the implementing zoning by-law but shall not include a supermarket, a drug store or pharmacy unless a market study confirming that there will be no significant detrimental market impact on other existing or planned commercial centres within the City has been undertaken to the satisfaction of the City.
- 12.3.2.18. Any development at 200 MacNaughton Road shall provide that all necessary lands are available into the future for the purposes of monitoring and remediation of any effects of waste disposal and shall recognize that the lands are subject to the Ministry of Environment Certificate of Approval A-230610. Any change in actual use will require an amendment to the Certificate of Approval.
- 12.3.2.19. Prior to rezoning lands or as a condition of lifting an “H” Holding Zone for the subject lands, the City will require studies with respect to:
- a. Method of implementation;
  - b. An urban design/streetscape masterplan;
  - c. Satisfying the requirements of policy 12.3.3.3 for the Waste Disposal Assessment Area; and,
  - e. Any other studies required pursuant to the policies of this amendment or as determined by the City in consultation with other levels of government.

### **12.3.3 Waste Disposal Assessment Area**

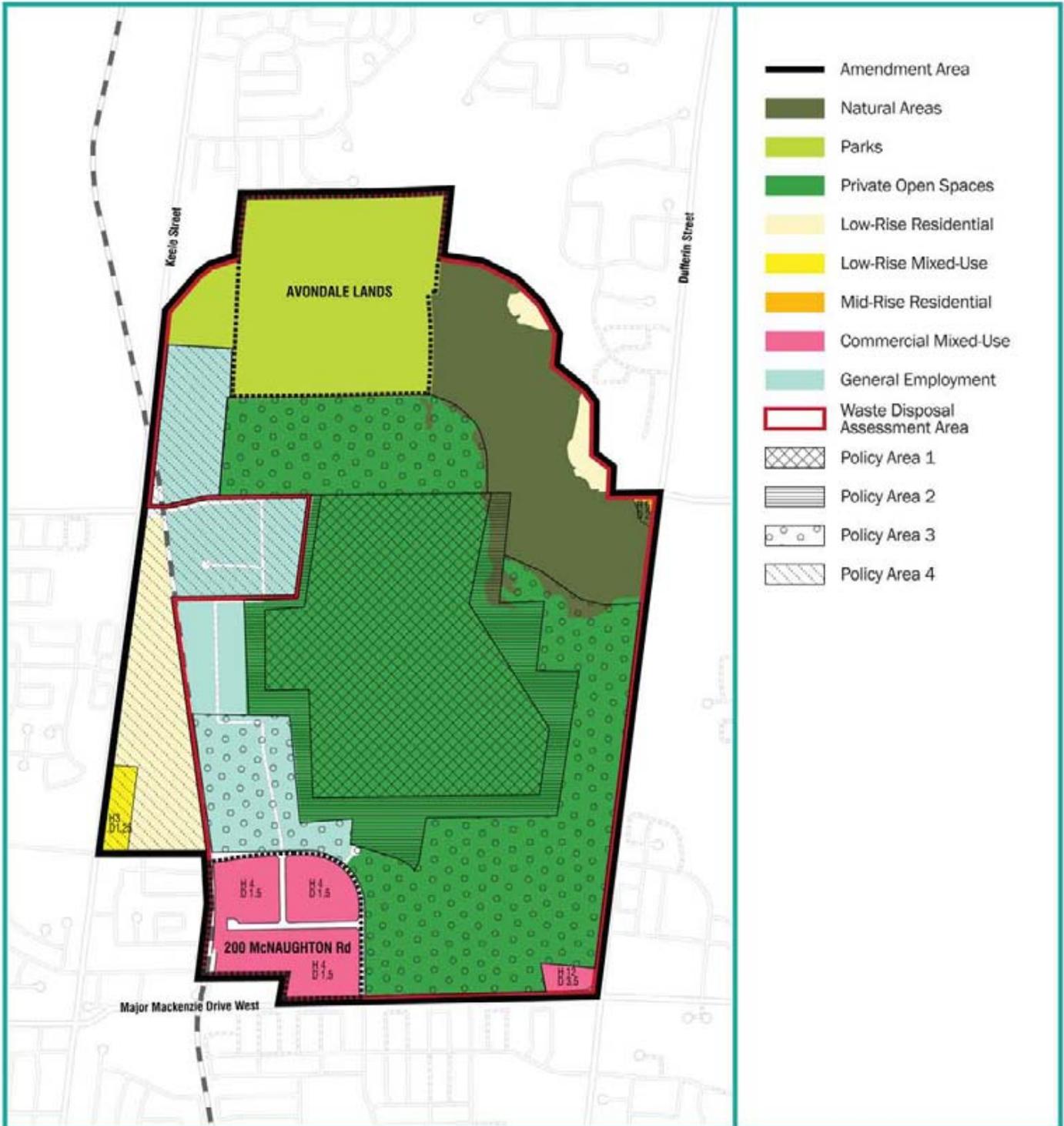
- 12.3.3.1. That a Waste Disposal Assessment Area is identified on Map 12.3.A.
- 12.3.3.2. To ensure that any development which takes place within the Waste Disposal Assessment Area is considered with due regard for the possible effects of waste

disposal operations and as such development will not necessarily be permitted within any Area which lies within the Waste Disposal Assessment Area.

- 12.3.3.3. That the Waste Disposal Assessment Area is classified as a potentially environmentally sensitive area for future uses. Since the use of land for the disposal of waste has long-term effects on these, and possibly adjacent lands, the following policies will safeguard any future use of the lands:
- a. within the Waste Disposal Assessment Area, no development may proceed until waste disposal and final cover operations within 500 metres and associated with the assessment area have ceased. Only land uses compatible with the potential impacts of a waste disposal site and its engineered controls, may be permitted;
  - b. an application for an amendment to the Official Plan or Zoning By-law to permit development within the Waste Disposal Assessment Area shall meet the following requirements:
    - i. studies of gas, leachate, stormwater management and hydrogeology being carried out by a qualified engineer as may be required to the satisfaction of the City, in consultation with the Toronto and Region Conservation Authority, and the Province, which indicate that development can safely take place;
    - ii. implementation of such remedial measures indicated by the required studies, to the satisfaction of the City in consultation with the Ministry of Environment. Prior to a change of use, approval under S.46 of the Environmental Protection Act may be required;
    - iii. the construction and phasing of all development to coincide with the control of any problems identified by the engineering studies;
    - iv. the City being satisfied with the required studies with respect to any matter regarding structural stability, safety and integrity of any or all structures;
    - v. studies to demonstrate to the satisfaction of the City in consultation with the Ministry of Environment and the City of Toronto that:
      - A. the existing or closed landfill will not cause an adverse impact on the proposed use;
      - B. the proposed use will not cause or potentially cause any change to ground water quality;
      - C. the proposed use will not affect the implementation of long term environmental controls related to landfilling; and,

- D. required ongoing monitoring and maintenance of the site will not be disrupted;
  - c. any undertaking which is subject to the Environmental Assessment Act shall not be permitted until such time as an approval or exemption under that Act has been given by the appropriate authority; and
  - d. The City may implement the provisions of policy 12.3.3.3 through the enactment of a Holding By-law pursuant to the policies of Section 10.1.2 and policy 12.3.3.4. However, prior to lifting the Holding Zone (H) within the Secondary Buffer Area (Policy Area 3 and Policy Area 4) or change of use on lands covered by the Certificate of Approval, the City shall have received the approval of the zoned use from the Province.
- 12.3.3.4. The Holding Symbol shall not be removed until the reason for the imposition of the Holding Symbol no longer applies. In addition to the policies of Section 10.1.2, the following specific policies shall apply to the removal of the Holding Symbol “H”:
- a. That where a holding symbol H is applied to lands located within the Waste Disposal Assessment Area, it shall not be removed except in respect of temporary use permission until the requirements set out in 12.2.2.19 above have been addressed to the satisfaction of the City and the Province. When such areas are deemed suitable for development, they may be rezoned in accordance with the City’s zoning policies;
  - b. That the holding symbol shall be removed from any portions of the composting envelope at Avondale (identified on Map 12.3.A) once a certificate, of approval therefore under the Environmental Protection Act has been issued; and,
  - c. that where a holding symbol is applied to lands subject to **General Employment** or **Prestige Employment** designations, in addition to any requirements of 12.3.3.3.a above, the holding symbol shall not be removed until servicing capacity is demonstrated and any required development applications have been approved.
- 12.3.3.5. That in recognition of the diversity of uses located in the Keele Valley Landfill Area there is the potential for impacts on sensitive land uses by virtue of noise, dust, odour or other contaminants. Warning clauses will be require where warranted. Such warnings shall be required at the time of subdivision/condominium/site plan approval and shall be in a form satisfactory to the City, the Province and the agency requesting such warning.

▼ Map 12.3.A: Former Keele Valley Landfill



## 12.4 Kleinburg Core

### 12.4.1 Goals

12.4.1.1. The following are the goals for the Kleinburg Core area:

- a. General:
  - i. Ensure that land use and built form are compatible with the scale and character of the existing community and integrated with the existing and contemplated pattern of development in the surrounding area;
  - ii. Establish a well-defined public realm;
  - iii. Promote pedestrian-oriented development patterns; and,
  - iv. Ensure, to the fullest extent possible, that the heritage resources, both built and natural, of the Kleinburg Core area are protected in accordance with the Kleinburg-Nashville Conservation District Study and Plan.
- b. Commercial Growth:
  - i. Ensure a prosperous and vibrant Kleinburg core area;
  - ii. Ensure servicing capacity for redevelopment/intensification;
  - iii. Ensure core area development complements existing development in overall size and scale;
  - iv. Establish policies to implement the Kleinburg-Nashville Heritage Conservation District Plan as per Part V of the Ontario Heritage Act;
  - v. Protect existing core area neighbourhoods and residences from incompatible commercial intrusion;
  - vi. Encourage mixed-use in the core area at a modest scale;
  - vii. Provide for limited to modest expansion of the commercial area;
  - viii. Ensure that commercial development takes place in accordance with the provisions of the Kleinburg-Nashville Heritage Conservation District Plan, ensure it remains at a village scale and ensure it complements the historic, rural village character and architectural heritage of the community;
  - ix. Encourage physical design which promotes safety and security;
  - x. Develop a public realm composed of streets and public spaces that are clearly defined by buildings and other visual amenities, including landscape elements, sidewalks, and bicycle paths;
  - xi. Sensitively manage the core area of Kleinburg through the reinforcement of the traditional pattern of development and preservation of existing historic buildings and the unique environmental features which give the Village its special character;
  - xii. Develop guidelines for new development and redevelopment within the core

- and to encourage a high quality of urban and architectural design;
  - xiii. Ensure that neighbouring developments are physically compatible and respect existing development conditions, scale and building placement;
  - xiv. Encourage the development of vacant lands and other redevelopment sites in a way which will enhance the character of Kleinburg;
  - xv. Encourage the protection of significant trees; and,
  - xvi. Encourage business activities of a sort which provide jobs for local residents consistent with overall growth management objectives.
- c. Residential Areas/Neighbourhoods:
- i. Provide for predominantly low-density residential development consistent with the character of the existing community;
  - ii. Establish a neighbourhood focus;
  - iii. Ensure quality urban design and architecture;
  - iv. Create an environment that provides for safe, functional and attractive residential neighbourhoods;
  - v. Provide attractive streetscapes through attention to the design of the public realm, built form and the relationship between private development and the public realm; and,
  - vi. Ensure that the residential development within the Kleinburg Core Area takes place in accordance with the provisions of the Kleinburg-Nashville Heritage Conservation District Plan, adopted by By-law 183-2003 as amended by By-law No. 268-2003, and remains at a village scale and complements the historic, rural village character and architectural heritage of the community.
- d. Heritage:
- i. Protect and preserve the existing heritage features including buildings and other structures, sites, landscapes, natural features and vegetation through the application of the Ontario Heritage Act and other relevant legislation;
  - ii. Encourage the retention and incorporation of existing heritage resources including buildings and other structures in the redevelopment of heritage property;
  - iii. Encourage that new development along the historic core areas of Kleinburg be sympathetic in scale, massing and architectural design with the existing 19th and early 20th Century heritage buildings in these historic core areas;
  - iv. Ensure that development or redevelopment occurs in accordance with the provisions of the Kleinburg-Nashville Heritage Conservation District Plan

adopted by By-law 183-2003 as amended by By-law No. 268-2003 and designated under Part V of the Ontario Heritage Act;

- v. Encourage the protection of, or where appropriate, the excavation of local archaeological resources;
  - vi. Preserve and incorporate significant heritage and archaeological sites into public and commercial environments and public open spaces;
  - vii. Promote an understanding of, and an appreciation for the community's heritage among local residents and visitors; and,
  - viii. Recognize the importance of and protect natural heritage features including the Humber River valley lands for their distinctive topography and scenic views.
- e. Parks and Open Space:
- i. Provide parkland that has a clear functional relationship to the community and residential neighbourhood.
- f. Transportation:
- i. Improve Islington Avenue north of Major Mackenzie Drive to make the section of Islington Avenue passing through Kleinburg more compatible with the human scale of the Village; and,
  - ii. Review the opportunity for traffic calming particularly at the intersection of Islington Avenue and Bindertwine Boulevard that will calm traffic and create a more consistent and attractive streetscape.

## **12.4.2 Community Planning Objectives**

- 12.4.2.1. An important planning objectives for the Kleinburg Core is to ensure that the scale of development is commensurate with the role of Kleinburg as an historic Local Centre.

## **12.4.3 Residential Areas/Neighbourhoods**

- 12.4.3.1. The area included in the calculation of Residential Density for all residential designations in the Kleinburg Core area shall be based on a net residential hectare which includes local and primary roads, the land for dwelling units, environmental linkages located on tablelands, and tableland valley buffer areas. Density will be calculated on a draft plan or block plan basis.

#### 12.4.4 The Kleinburg Core Area – General

- 12.4.4.1. All development within the **Mainstreet Commercial** area, identified on Map 12.4.A, will be subject to the Urban Design Policies of this Plan, the Kleinburg-Nashville Heritage Conservation District Study and Plan, a Streetscape Master Plan, a Parking Study with a view to potentially establishing a municipal parking lot, an inventory of significant trees, a Tree Preservation By-law, and other studies as identified in this Plan.
- 12.4.4.2. Residential infilling within the Village shall be permitted subject to conformity with the Urban Design provisions of this amendment and provided the proposed lot or development is compatible in size, shape, and configuration with adjacent lots and the development located thereon.
- 12.4.4.3. Detailed development standards addressing scale and massing, yard setbacks, height, maximum floor areas, parking requirements, landscape standards and other matters shall be set out in the implementing zoning by-law. In establishing the standards within the Zoning By-law, the scale and massing of the historic development and the existing natural landscape shall be considered.
- 12.4.4.4. Any proposed re-zoning to a commercial use requiring an amendment to the zoning bylaw shall only be permitted when the City is satisfied that the proposed use is compatible with any adjacent land uses in respect of urban design/heritage preservation, noise, traffic, parking loading/delivery requirements and servicing.
- 12.4.4.5. Any uses other than those stipulated in policies 12.4.5.2 and 12.4.6.2 are prohibited.
- 12.4.4.6. The following policies apply to the Kleinburg Core Area, exclusive of the **Mainstreet Commercial** area:
- a. Residential infilling within the Village shall be permitted subject to conformity with the Urban Design provisions of this Plan, as detailed in the Kleinburg Nashville Heritage Conservation District Study and Plan, provided the proposed lot or development is compatible in size, shape and configuration with adjacent lots and the size and form of development located thereon.
  - b. For all development within the Kleinburg Core area, the urban design policies and heritage conservation policies set out in the Plan, as outlined in the Kleinburg Nashville Heritage Conservation District Study and Plan, shall also apply.

12.4.4.7. The consolidation or severance of lands shall be discouraged. Any consolidation or severance of lands may be permitted if the proposed lot pattern is consistent with the existing lot pattern in the immediate surrounding area, in situations of minor lot line adjustment or where otherwise required by law.

12.4.4.8. In order to ensure the long term viability of Kleinburg Core as a tourist destination and as a local and neighbourhood shopping area the City may undertake an economic development review.

### 12.4.5 Mainstreet Commercial Area Permitted Uses

12.4.5.1. Within the **Mainstreet Commercial** area shown on Map 12.4.A the permitted uses include Village “mainstreet commercial” uses located on Islington Avenue and Nashville Road. Limited small scale mixed-use developments on the main streets may also be permitted, subject to certain requirements. These requirements may be included in the provisions of an implementing Zoning By-law. These provisions should consider matters such as the separation of uses, landscaping, the design of the site and the relationship between the site and the street.

12.4.5.2. Uses permitted in the **Mainstreet Commercial** area, as set out in the implementing zoning by-law, excluding automobile oriented uses, may include:

- a. Retail Stores;
- b. Personal Service Shops;
- c. Professional and Business Offices;
- d. Small scale hotels or “Village Inns”;
- e. Bed and Breakfast operations;
- f. Restaurants including sidewalk cafes;
- g. Small scale mixed-use developments with at-grade commercial uses and an upper floor residential component;
- h. Detached residential dwellings;
- i. Converted Dwellings in the form of residential conversions to permitted commercial and mixed-use developments;
- j. Parks and Open Space; and,
- k. Public and Institutional uses.

## 12.4.6 Village Residential Area Permitted Uses

12.4.6.1. The **Village Residential** Area currently consists of residential uses, public and institutional uses.

12.4.6.2. Residential uses include the following:

- a. Detached residential dwellings;
- b. Bed and Breakfast establishments;
- c. Parks and Open Space; and,
- d. Public and Institutional Uses.

## 12.4.7 Area A

12.4.7.1. Notwithstanding the above, for the lands located on the west side of Islington Avenue, south of Nashville Road, municipally known as 10360 and 10384 Islington Avenue, in Part of Lot 23, Concession 8, identified on Map 12.4.A as **Area A**, the built form may consist of one (1) building having a maximum Gross Floor Area of 4,416 square metres. The existing 260.13 square metres (Martin Smith House) heritage dwelling shall be used as an ancillary amenity space and shall not be included in the above stated maximum Gross Floor Area. The development shall be a maximum of 2-storeys on all east elevations facing Islington Avenue. A maximum of 3-storeys will be permitted adjacent to the Valley located at the rear of the Subject Lands (west elevation). A transition to 3-storeys may be permitted at the side of the building where the change in grade permits. Specific details respecting height and mass shall be established in the site-specific zoning by-law, and shall be in accordance with the Ontario Municipal Board (OMB File PL060606 and related files) approved proposal.

## 12.4.8 Development Standards: Mainstreet Commercial Area

12.4.8.1. Detailed development standards respecting size, scale, setbacks, height, and other matters will be set out in the implementing zoning by-law, having regard for the following:

- a. All development within the **Mainstreet Commercial** designation will be subject to the urban design policies of this Plan, as outlined in the Kleinburg-Nashville Heritage Conservation District Study and Plan, and the development standards as set out in the implementing zoning by-law; and,
- b. The scale of development within the **Mainstreet Commercial** designation shall be

in keeping with the policies and guidelines established by the Kleinburg-Nashville Heritage Conservation District Study and Plan and will complement the character and scale of existing buildings, and shall:

- i. generally not exceed a maximum achievable building height of 9.5 metres;
- ii. have a maximum achievable Floor Space Index within the range of 0.2 to 1.0, depending upon the lot frontage, depth, proposed use, site constraints and standards established by the Zoning By-law; and,
- iii. have consideration for the scale, massing and use of existing development on abutting or adjacent properties.

#### **12.4.9 Development Standards: Kleinburg Core Area**

12.4.9.1. Detailed development standards applicable to the predominantly residential uses within the Kleinburg Core Area will be established in the City's Comprehensive Zoning Bylaw 1-88 with particular regard to lot frontages, setbacks, building height, landscaping and with the intent of maintaining the village scale and character of development.

#### **12.4.10 Urban Design - General**

12.4.10.1. Achieving quality design is recognized as an important objective for the Kleinburg Core area. Outlined below is a set of comprehensive design policies which apply to the Core Area. The design policies set out general criteria for the development of public and private lands within the Core Area and include:

- a. the provision of a clearly defined public realm;
- b. predictable and consistent built form in keeping with the existing scale and massing of the buildings within established commercial and residential areas;
- c. safety, comfort and accessibility in the pedestrian environment;
- d. compatibility and enhancement of the unique rural village heritage character of the community; and,
- e. a recognition and description of the standards established within the Kleinburg-Nashville Heritage Conservation District Study and Plan.

#### **Strategic Sites (Gateways)**

12.4.10.2. Strategic Sites are those properties shown on Map 12.4.A that warrant special design treatment because of their location and visibility. These sites have the potential to strengthen and improve the quality of the public realm. The treatment of the

architecture, building materials, massing and built form should reinforce their location as a gateway to the Kleinburg Core Area, a visual terminus or an important intersection.

- 12.4.10.3. The elements used to define a gateway or strategic site should be sensitive to the context of the location of the gateway or site and be designed in such a manner as to contribute to a welcoming Village core. Gateway elements may consist of any or a combination of the following:
- a. buildings of a high quality design and oriented toward the intersection;
  - b. architectural elements such as gates and walls;
  - c. special paving and lighting;
  - d. signage; and,
  - e. landscape treatment.

### **Community Corridors**

- 12.4.10.4. Nashville Rd. and Regional Road 27 are considered as important community corridors between the existing villages of Kleinburg and Nashville and linking the developed and more rural areas within the Kleinburg-Nashville area. Consideration shall be given to the landscaping and streetscaping along these corridors to ensure that the rural character and vistas of the rural and natural landscapes are retained. The visibility of development in such areas from major roads and corridors should be minimized.

## **12.4.11 Urban Design in the Kleinburg Core Area including the Mainstreet Commercial Area**

### **Public Realm**

- 12.4.11.1. The design of development should enhance and maintain a public realm (comprising public streets, lanes, parks and open space and the public use activity areas of private lands and buildings) that is consistent and compatible with the community character and contribute to a safe and attractive pedestrian environment.
- 12.4.11.2. New development and redevelopment in the Core Area shall be compatible with, and complementary to, the distinctive and historic character of the building, with particular regard for the scale, setback, spacing, massing, and architectural design. Detailed Development Standards will be set out in the implementing zoning by-law.

- 12.4.11.3. The development of public use spaces, as community focal points, in the Core Area is encouraged. These spaces should be located primarily within the **Mainstreet Commercial** area and designed in such a manner as to contribute to the “mainstreet commercial” and village environment.
- 12.4.11.4. Landscape elements including trees, planting, paving materials and architectural elements, should be utilized to complement building sites and streetscapes, reinforce the relationship between the building and the street and contribute to the visual continuity of the public realm.

### **Streets**

- 12.4.11.5. Protect existing heritage streetscapes using techniques such as variable rights-of-way, and innovative road design standards.
- 12.4.11.6. Roads and streets through and within the Village Core should maintain the historic character of the community.
- 12.4.11.7. Establish a streetscape Masterplan for all arterial streets within the community planning area as part of any planned improvements.

### **Islington Avenue**

- 12.4.11.8. In recognition of Islington Ave. as the primary entrance into Kleinburg and its importance as a public amenity area, a comprehensive streetscape design shall be developed for the public right-of-way of Islington Ave. between Major Mackenzie Dr. and the McMichael Gallery, that incorporates the following features:
  - a. traffic calming measures with particular attention to the intersection of Islington Ave. and Bindertwine Blvd.;
  - b. a consistent landscaping treatment that effectively screens the rear yards of existing residential properties backing onto Islington Ave.;
  - c. landscaped boulevards on both sides of the street that incorporate public sidewalks, landscaping, pedestrian-scaled street lighting and other streetscaping elements;
  - d. well-marked pedestrian crossing areas at identified locations; and,
  - e. In addition, the comprehensive streetscape design shall include the public right-of-way of Islington Ave. north of Nashville Road to Regional Road 27.

### **Views, Vistas and Focal Points**

- 12.4.11.9. Preserve and enhance views to natural features including valley lands, topographic features and across open spaces.
- 12.4.11.10. Provide opportunities for views of important public buildings, heritage features, open spaces, natural features, and other landmarks.
- 12.4.11.11. Provide for sites to terminate streets and view corridors wherever possible.
- 12.4.11.12. Inventory significant trees with the goal of maintaining the village character wherever possible.

### **Location of Buildings and Permitted Uses with Respect to Streets and Open Space**

- 12.4.11.13. Generally, the “front” facade of each building should be aligned parallel to the abutting street and located in close proximity to the street line.
- 12.4.11.14. Buildings fronting onto public streets should be located along the street edge.
- 12.4.11.15. Buildings on corner sites should be sited and massed toward the intersection and the abutting streets.
- 12.4.11.16. Siting and massing of new buildings should provide an appropriate degree of enclosure and continuity to the street.
- 12.4.11.17. The entrance to Commercial and Retail Uses within the **Mainstreet Commercial** designation will be grade-related with access directly to the public sidewalk. Below grade entrances to commercial uses will be discouraged.
- 12.4.11.18. The implementing zoning by-law will provide details for building setbacks, massing and relationship to the public realm.

### **Pedestrian Environment**

- 12.4.11.19. To promote safety and security in public places, including streets, parks and open space, parking and public use areas, the following measures are encouraged:

- a. the design and siting of new buildings should provide opportunities for visual overlook and ease of physical access to adjacent streets, parks, open space and public use areas;
- b. direct views to parks, open spaces and public use areas should be provided from the adjoining streets;
- c. appropriate lighting, visibility and opportunities for informal surveillance should be provided for all walkways, parking lots, garages and outdoor amenity areas; and,
- d. a consistent level of streetscape design, incorporating such elements as appropriate paving, planting, fencing, street furniture, lighting and signage should be provided to ensure ease of access for the pedestrian and the enjoyment of public streets and other outdoor spaces.

### **Building Compatibility**

- 12.4.11.20. Where redevelopment of wide lots or consolidation of adjacent properties is proposed, the built form on the property shall reflect the historic scale and pattern of built form of the village and the adjacent and neighbouring properties. Subject to the provisions of the Zoning By-law, more than one building may be developed on the property in order to achieve a consistent scale and pattern of development.
- 12.4.11.21. New development will be compatible with adjacent and neighbouring development by ensuring that the siting and massing of new buildings does not result in negative impacts on adjacent properties, particularly with regard to overlook, shadows, wind and other environmental and compatibility factors.

### **Building Heights**

- 12.4.11.22. New buildings, additions to buildings or the redevelopment of existing sites in the Core Areas of Kleinburg or Nashville shall generally not exceed a maximum height of 9.5 metres above finished grade.

### **Architectural Design**

- 12.4.11.23. Building facades should be articulated to provide visual interest using such elements as porches, bays, canopies, balconies, solariums and other architectural features.
- 12.4.11.24. Siting, massing and scale of mixed-use buildings in the Core Area that front onto Islington Ave., and Nashville Rd. in the Core Area, will reflect building types typically

found on a traditional “rural Ontario Main Street” and should be representative of the architectural heritage of 19th and early 20th century Kleinburg.

### **Building Entrances**

- 12.4.11.25. Primary building entrances should be clearly visible and located on a public street frontage.
- 12.4.11.26. Access to primary building entrances, should be convenient and direct with minimum changes in grade and should be accessible to people who are mobility impaired.
- 12.4.11.27. The implementing zoning by-law may require a minimum percentage of the building frontage to be restricted to commercial use only, in order to facilitate continuous retail activity at grade.

### **Parking and Servicing Areas**

- 12.4.11.28. The provision of underground parking shall be encouraged for mixed-use developments within the **Mainstreet Commercial** area to reduce the impact of surface parking and to assist in providing at-grade amenity areas, and to encourage a suitable relationship to the public street.
- 12.4.11.29. Where it is not feasible to locate parking below grade, parking should be located to the rear of principal buildings subject to the preservation of significant trees.
- 12.4.11.30. Landscaping, fencing, architectural elements and other appropriate screening treatments should be provided to reduce the impact of at grade parking and service areas and contribute to the visual continuity of the public realm, ensuring that safety and security measures - for both parking lot users and the public on adjacent streets, are maintained.
- 12.4.11.31. No parking within the **Mainstreet Commercial** area will be permitted between the main building(s) and a public street.
- 12.4.11.32. Pursuant to the provisions of the Planning Act and the Municipal Act the City may establish a parking levy for the purpose of providing and maintaining off-site parking:
  - a. Monies raised through cash-in-lieu of parking shall be placed in a special reserve

fund, separate from general revenues, dedicated to managing existing public parking resources and/or establishing new parking facilities;

- b. In determining the actual cost of providing a parking space, and the appropriate fee to be levied for each space, the City may create a formula that considers the cost of land, cost of construction of a space and associated accesses, cost of landscaping, cost of maintenance, and the use of a multiplier for different types of development; and,
- c. In order to promote the adaptive re-use (conversion and renovation) of existing heritage homes within the **Mainstreet Commercial** designation, the use of a multiplier to reduce the cost of providing a parking space to a landowner may be considered.

12.4.11.33. The City's preferred course of action in responding to applications that propose to provide fewer parking spaces than established by the City's Comprehensive Zoning By-law is as follows:

- a. Calculate parking required, as per the City's Comprehensive Zoning By-law;
- b. Review applicant's/owner's site plan to determine the number of parking spaces to be provided;
- c. If it is determined that the proposed site plan does not provide the required number of parking spaces, then a Parking Generation Assessment will be required from the applicant/owner;
- d. Based on the findings of the Parking Generation Assessment, a variance may be supported for the difference between the requirements of the City's Comprehensive Zoning By-law and the Parking Generation Assessment.
- e. If the proposed number of parking spaces is less than the number of parking spaces required by the City's Comprehensive Zoning By-law and less than number of parking spaces recommended in the Parking Generation Assessment, then the applicant/owner would be required to enter into an agreement with the City pursuant to Section 40 of the Planning Act to provide cash-in-lieu of parking for the difference.

12.4.11.34. When reviewing applications for variances to the minimum required parking standards in the City's Comprehensive Zoning By-law, in addition to items normally considered, the City shall also require the following items to be submitted by an applicant/landowner:

- a. A Parking Generation Assessment; and,

- b. A heritage property assessment which includes an analysis of existing landscape and tree conditions, the impact the proposed parking area will have on these site conditions and other characteristics that contribute to the character of the Kleinburg-Nashville Heritage Conservation District and any existing buildings.

12.4.11.35. The heritage property assessment shall:

- a. Be prepared by a qualified heritage professional; and,
- b. Describe the impact of proposed development on the existing building(s), landscape and streetscape.

12.4.11.36. The landscape and tree analysis shall:

- a. Be prepared by a qualified horticulturalist, landscape architect, arborist or forester, depending on the feature being assessed;
- b. Identify and assess the current location and condition of all trees, shrubs and other natural and man made features on the site. The assessment of annuals and other bedding plants is not required; and,
- c. Consider the long term health of the natural feature and provide comment on the impact of additional parking on the health and protection of the identified vegetation.

12.4.11.37. The parking generation assessment shall contain a detailed description of the proposed and existing uses, and describe characteristics of the site, such as but not limited to total floor area of the building, total floor area of each use contained within the building, number of employees, design capacity of building and seating capacity and any other item identified by the City. The review of the proposed use(s) shall include analysis and recommendations based on a review of uses of a similar type and scale within the community and in other Heritage Conservation Districts.

### **Signage**

12.4.11.38. The City shall control signage within the Kleinburg Core area through the provisions of the Sign By-law, the Urban Design Guidelines and the Heritage Conservation District Plan and Guidelines.

- 12.4.11.39. The City shall undertake a review of the Sign By-law to ensure that there are suitable provisions for use in the Kleinburg – Nashville Heritage Conservation District. The review shall pay specific attention to the policies and guidelines contained within the Kleinburg-Nashville Heritage Conservation District Study and Plan.

### **12.4.12 Design Considerations - Area A**

- 12.4.12.1. Notwithstanding policies 12.4.11.20 through 12.4.11.22, for the lands identified on Map 12.4.A as **Area A**, the maximum height shall be 9 metres, contained in 2-storeys and having the scale of a single residential building on all east elevations of the building, facing Islington Avenue. The rear of the building will be permitted a maximum building height of 12 metres, contained in 3-storeys (west elevation). A transition to 3-storeys may be permitted at the side of the building where the change in grade permits. These development details are more specifically depicted in the implementing zoning by-law and the OMB approved proposal (OMB File PL060606 and related files).
- 12.4.12.2. **Area A** shall contain a 7.5 metre buffer from the staked top-of-bank, which shall form part of the valley system and shall be zoned OS1 Open Space Conservation Zone. An additional 2.5 metre landscaped buffer shall be provided adjacent to the Open Space Conservation Zone, and be used for no other purposes than landscaping.

### **12.4.13 Transportation**

- 12.4.13.1. The City shall continue to ensure that Islington Ave and Nashville Rd. (east of Regional Rd. 27) function as pedestrian friendly village main streets.
- 12.4.13.2. Traffic management measures may be established to regulate traffic on Islington Ave. particularly in the A.M. peak.
- 12.4.13.3. The City shall consider the establishment of traffic calming features such as a traffic circle at the intersection of Islington Ave. and Bindertwine Ave. in response to increasing traffic levels and the continuing need for traffic calming.

### **12.4.14 Community Design Guidelines**

- 12.4.14.1. It shall be the policy of Council to encourage the maintenance of the general character of the Kleinburg Core area through the sensitive design of building forms and facades, street design, scale and siting of buildings, fencing and the sensitive use of materials

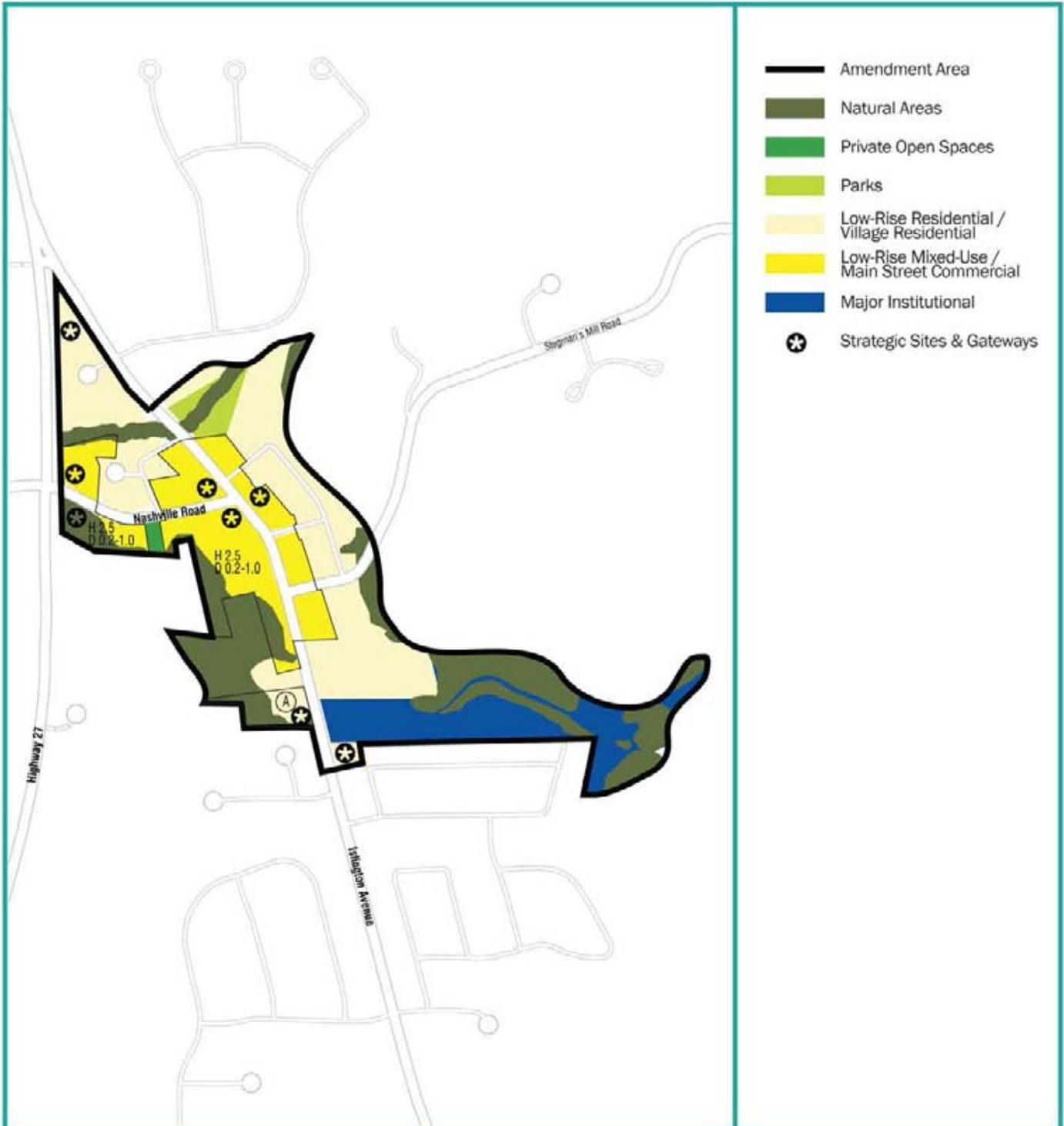
and landscaping, for any development in the Core area. Council will consult with representatives of the Kleinburg-Nashville community during the implementation.

- 12.4.14.2. Council may initiate and adopt design guidelines.
- 12.4.14.3. Zoning applications for commercial or mixed-use development within the **Mainstreet Commercial** area, or the remainder of the Kleinburg Core Area shall be supported by the following information:
  - a. a planning justification report outlining in detail how the proposed development conforms with:
    - i. the policies of this plan and in particular the policies respecting Urban Design and Core Areas;
    - ii. the Kleinburg-Nashville Heritage Conservation District Study and Plan; and
    - iii. any other study or by-law that is empowered or recommended by the provisions of this Plan or the Kleinburg-Nashville Heritage Conservation District Study and Plan, and any study, plan or regulation that may be undertaken in the future provided that it is adopted by Council;
  - b. a preliminary site plan including:
    - i. preliminary building architectural elevations;
    - ii. relationship to adjacent buildings;
    - iii. relationship of the proposed development to the street;
    - iv. facilities for public sidewalks and pedestrian amenities along the street or to adjacent parks or open space areas;
    - v. residential amenities for any residential units which may be proposed;
  - c. the following supplemental items may be required:
    - i. a traffic and parking report establishing the requirements of the proposed development and the adequacy of the proposed facilities;
    - ii. a vegetation report identifying any significant vegetation on site and how it is to be protected and integrated into the proposed development;
    - iii. an environmental/open space report identifying how the site is to be integrated with any adjacent open space or valley land; and,
    - iv. a preliminary report, prepared by a qualified heritage architect with respect to architectural design features and consistency with adjacent development, with particular regard to the Kleinburg-Nashville Heritage Conservation District Study and Plan.

### 12.4.15 Interpretation

- 12.4.15.1. The locations of and boundaries between the land use designations shown on the Schedules are approximate, except where they coincide with arterial roads, railway lines, or other clearly defined physical features. Minor adjustments will not require an amendment to this Plan as long as the intent of its policies is maintained.
- 12.4.15.2. The location of the by-pass, arterial collector and primary road system is approximate, and minor adjustments will not require an amendment to this Plan provided that the intent of its policies is maintained.
- 12.4.15.3. All statistics, with the exception of density requirements may vary within 5% as long as the intent of the policies of this Plan is maintained.
- 12.4.15.4. Notwithstanding the above, for **Area A**, all statistics contained in the Ontario Municipal Board approved development proposal (OMB File PL060606 and related files) shall be adhered to.

▼ Map 12.4.A: Kleinburg Core



## 12.5 Adult Entertainment Uses

### 12.5.1 Adult Entertainment Parlors

- 12.5.1.1. Adult entertainment parlors, as defined in Section 154 of the Municipal Act, and body rub parlors, as defined in Section 154 of the Municipal Act, are prohibited in the City of Vaughan with the following exceptions:
- a. they are limited to the following locations:
    - i. a portion of the lands located east of Keele Street and south of Langstaff Road, being part of Lots 7, 8, 9, and 10, Concession 3, as identified on Map 12.5.A; and,
    - ii. a portion of the lands located south of Doughton Road and Killaloe Road, and east of Maplecrete Road, being part of Lots 4 and 5, Concession 4, as identified in Map 12.5.A;
  - b. adult entertainment parlours shall be located at least 100 metres from an arterial road or Provincial highway, and shall not be located on lots abutting such roadways;
  - c. any adult entertainment parlour shall be located at least 1000 metres from any other adult entertainment parlour;
  - d. an adult entertainment parlour shall be located at least 500 metres from any lands designated in the Official Plan for residential purposes, and any residential use. This restriction shall not apply to the two existing farm dwellings located on the west side of Jane Street within Lot 4, Concession 5; and,
  - e. an adult entertainment parlour shall be located only in a building where the adult entertainment parlour is the sole occupant of the building.

### 12.5.2 Body Rub Parlors

- 12.5.2.1. Body rub parlors, as defined in Section 154 of the Municipal Act, are prohibited in the City of Vaughan with the following exceptions:
- a. they are to the following locations:
    - i. a portion of the lands located east of Keele Street and south of Langstaff Road, being part of Lots 8 and 9, Concession 3; as identified on Map 12.5.A; and,
    - ii. a portion of the lands located south of Killaloe Road and east of Costa Road, being parts of Lots 4 and 5, Concession 4, as shown on Map 12.5.A;
  - b. a body rub parlour may not exceed 150 square metres in Gross Floor Area; and,
  - c. a body rub parlour is restricted to a maximum of one per lot, and when in a multi-unit building is also restricted to a maximum of 15% of the Gross Floor Area of the multi-unit building.

▼ Map 12.5.A: Adult Entertainment Uses



## 12.6 Vaughan Mills Mall

### 12.6.1 General

12.6.1.1. Notwithstanding policy 9.2.1.1 the use of land within the Vaughan Mills Mall area shall be per the policies below for the Vaughan Mills Mall District and the Commercial District, shown on Map 12.6.A.

### 12.6.2 Mall District

12.6.2.1. The **Mall District**, shown on Map 12.6.A, shall be used for an enclosed mall and shall:

- a. have a minimum gross floor area (GFA) of 100,000 square metres; and,
- b. be permitted to contain a full range of commercial uses including retail, service commercial, entertainment and restaurant uses subject to the limitations established herein.

12.6.2.2. The commercial uses established within the **Mall District** shall be subject to the following policies:

- a. All uses shall be collectively permitted only within a fully enclosed shopping mall, except for limited outside display, storage and sales accessory to a permitted use, outdoor seating areas associated with restaurant uses and uses for the entertainment and recreation of mall patrons, subject to standards set out in the implementing zoning by-law;
- b. A maximum of 10%, not including the food court, of the commercial floor area (C.F.A.) of the mall shall be provided in commercial units having an area of 185 square metres C.F.A, or less;
- c. A minimum of eight (8) commercial units shall be provided which have a minimum commercial floor area of 1,850 square metres which cumulatively have a minimum area of 35% of the commercial floor area of the mall; and,
- d. Department stores and supermarkets shall not be permitted.

### 12.6.3 Commercial District

12.6.3.1. Commercial uses shall be permitted on those lands designated **Commercial District** on Map 12.6.A.

12.6.3.2. Permitted Uses within the **Commercial District** are existing commercial uses, retail stores for the buying, leasing and exchanging of goods and services, restaurants, banks, and business and professional offices, hotels, convention centres accessory

to hotels, and places of entertainment. Office building uses shall be subject to policies 12.6.3.7 and 12.6.3.8 below.

- 12.6.3.3. The **Commercial District** on Map 12.6.A shall be developed in the form of individual buildings or groupings of buildings.
- 12.6.3.4. The first occupancy of any commercial uses in this designation shall not occur prior to the building permit issuance for the enclosed shopping centre within the Vaughan Mills Mall Area.
- 12.6.3.5. Uses which require outdoor storage and display or merchandise such as auto, boat and recreational vehicle sales, may be permitted in the southerly **Commercial District** area, adjacent to the east-west collector road, as shown on Map 12.6.A.
- 12.6.3.6. A maximum of three automobile service stations or gas bars may be permitted provided that no more than one such use shall be located on any single municipal road frontage and shall not be located at the intersection of Jane Street and Rutherford Road.
- 12.6.3.7. The maximum Commercial Floor Area of all commercial uses located within the Vaughan Mills Area, with the exception of offices, hotels and accessory convention centres, shall be 160,000 square metres.
- 12.6.3.8. Office building uses shall only be permitted in the area shown as Office Precinct on Map 12.6.A. Office building uses shall not collectively exceed a total C.F.A. of approximately 10,000 square metres and shall have maximum building heights of five storeys. The implementing Zoning By-law shall provide the standards required to implement these policies.
- 12.6.3.9. Notwithstanding the foregoing, the lands located at the northwest corner of the intersection of Jane Street and the east-west collector road opposite Locke Street, as shown on Map 12.6.A as **Area A**, shall be subject to the following policies:
  - a. The lands shall be zoned with the Holding Symbol (H) in accordance with Section 36 of the Planning Act. A by-law to remove the Holding Symbol (H) shall not be enacted until such time as:

- i. Draft Plan of Subdivision 19T-98V10 has been registered and the east-west collector road leading from the partial interchange at Highway 400 to Jane Street to intersect with Jane Street at Locke Street has been constructed and dedicated as a public highway;
  - ii. a Site Plan Agreement for the development of the subject lands has been registered;
  - iii. the Owner shall have ceased all operations and demolished all buildings and structures; and,
  - iv. the Owner shall have provided a Record of Site Condition, based on the studies of a qualified environmental consultant and the completion of any necessary remediation work, which indicates that the entire site is free of environmental hazard and meets the Ministry of the Environment (MOE) Guidelines for Industrial and Commercial Uses. The record of site condition shall be acknowledged by MOE;
- b. Notwithstanding the above, a by-law to remove the Holding Symbol (H) from the portion of the lands located at the northwest corner of Jane Street and the new east-west collector road (opposite Locke Street), as defined in the implementing zoning by-law, may proceed in advance of the removal of the Holding Symbol for the entire **Area A** shown on Map 12.6.A. The by-law to remove the Holding Symbol (H) for this area shall not be enacted until such time as:
- i. Policy 12.6.3.9.a.i. above shall have been fulfilled and 12.6.3.9.a.iv. above shall have been fulfilled as it applies to the defined area;
  - ii. The Owner shall have ceased all operations and demolished all buildings and structures within the defined area; and,
  - iii. A Site Plan Agreement or Site Plan Letter of Undertaking, whichever is in effect, has been registered for the lands within the defined area from which the Holding Symbol (H) is to be removed and Vaughan Council has concurrently approved a conceptual plan for the remaining portion of the subject lands which demonstrates that the remaining lands can be developed in a manner which is functional, compatible with adjacent development and visually attractive;
- c. In addition to the General Commercial uses permitted in policy 12.6.3.2 hotels, convention centres accessory to hotels and places of entertainment shall also be permitted.

- d. Uses permitted in policy 12.6.3.5 shall also be permitted adjacent to the east-west collector road, as shown on Map 12.6.A, provided that no such use shall be located within 150 metres of Jane Street;
- e. The maximum Commercial Floor Area permitted shall be 6,400 square metres. This limitation shall be reflected in the implementing zoning by-law. The maximum Commercial Floor Area may be exceeded without amendment to this plan, subject to an amendment to the zoning by-law based on market study which specifies the supportable commercial floor area for the site. Such market study shall be to the satisfaction of the City of Vaughan.
- f. The Urban Design Guidelines and Master Landscaping Plan approved by Vaughan Council on April 26, 1999 shall form the basis for the submission, review and approval of the site development application(s) for this site;

#### **12.6.4 Urban Design Policies**

- 12.6.4.1. The City shall approve Urban Design Guidelines and a Master Landscaping Plan prior to the registration of the implementing draft plan of subdivision. The Guidelines and Master Landscaping Plan will guide developers, builders and the municipality in achieving appropriate zoning and design standards for the various elements which will create the Vaughan Mills Mall Area.
- 12.6.4.2. The City shall ensure that all development within the lands identified on Map 12.6.A is consistent with the Urban Design Guidelines and the Master Landscaping Plan.
- 12.6.4.3. The Vaughan Mills Mall Area shall be developed in accordance with the following urban design objectives and principles:
  - a. To enhance the setting and desirability of the development as a major urban centre in the City and as an important tourist destination. The development shall establish a distinctive, high quality urban environment defined by the architectural treatments and materials used in the component buildings, and the landscaping treatment;
  - b. To provide for a distinctive and co-ordinated overall landscape theme and architectural style through the approval and implementation of urban design guidelines and an overall landscape masterplan;

- c. To ensure that development located adjacent to Rutherford Road shall be pedestrian and street related with pedestrian access located in the Rutherford Road building elevations, connected to the sidewalk system in the road allowance;
- d. To provide that parking be limited between Rutherford Road and any building and any building which abuts the Rutherford Road frontage;
- e. To require that buildings along Rutherford Road shall exhibit a street related retail presence and a “four frontage” building design which serves to animate the street;
- f. To provide safe pedestrian linkages from the surrounding arterial road system to the central commercial mall development; and,
- g. To provide for safe, efficient and convenient vehicular access to all components of the development in a manner that is pedestrian friendly.

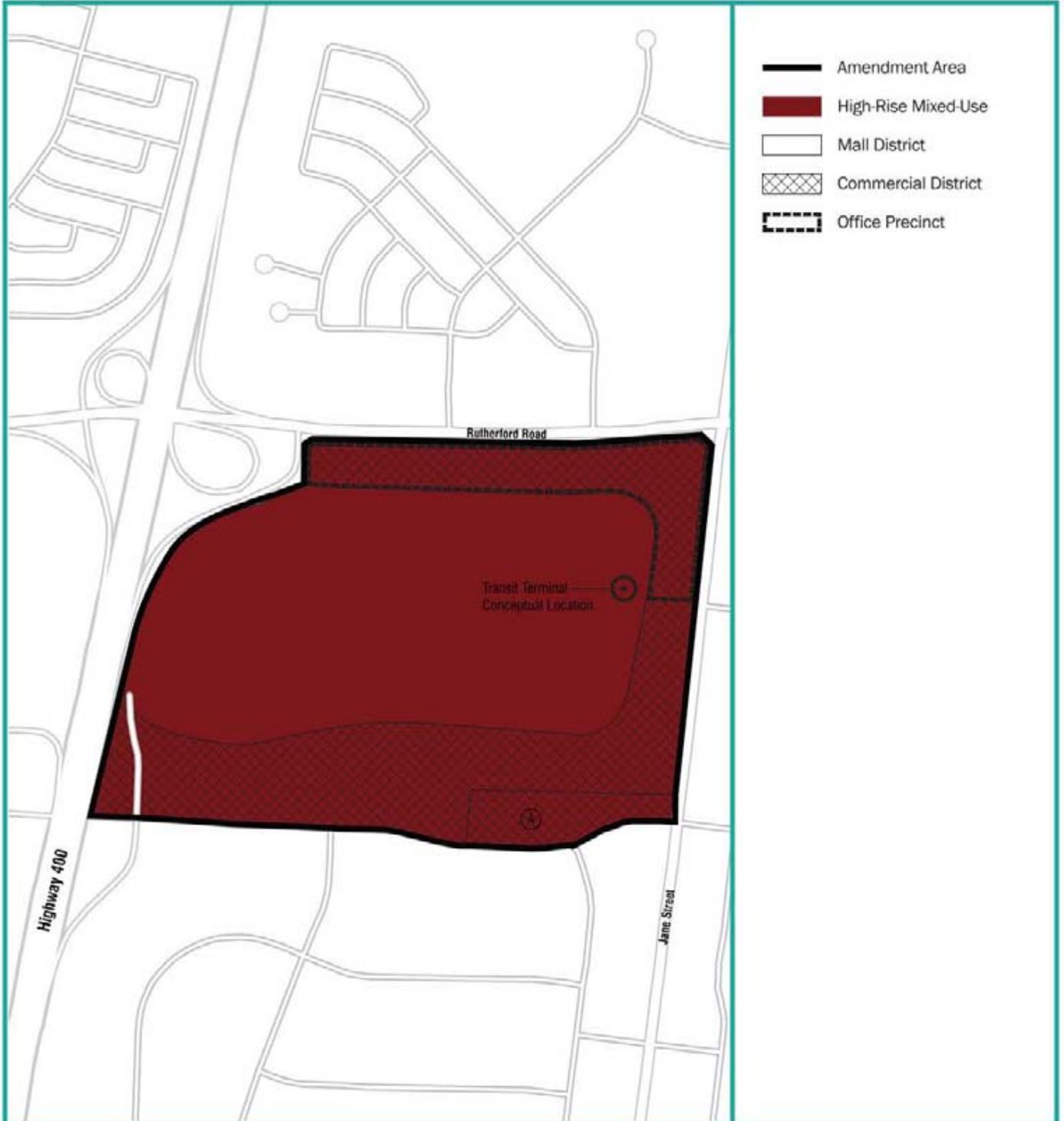
## 12.6.5 Transportation Policies

12.6.5.1. Council shall be satisfied, in consultation with the Region of York and the Ministry of Transportation, that prior to development proceeding, satisfactory arrangements are in place to ensure the completion of the following road improvements. The nature and timing of these road improvements may be modified with the consent of the affected road authorities without amendment to this plan:

- a. A new partial interchange with Highway No. 400 between Jane Street and Langstaff Road including a northbound off-ramp and a southbound on-ramp connecting to a new collector road crossing Highway No. 400, ultimately linking the west side of Highway No. 400 with Jane Street opposite Locke Street and a connection to the southbound on-ramp be provided for traffic originating on the west side of Highway No. 400;
- b. A collector road connecting the new Highway No. 400 interchange, between Rutherford Road and Langstaff Road, with Jane Street, opposite Locke Street. Subject to final design, the status of this collector road may be upgraded to a Minor Arterial having a minimum right of way width of 30 metres. The final width will be determined during the review of the implementing draft plan of subdivision application or site development application and the increase in width to 30 metres will not require an amendment to this plan.

- c. A link connecting Edgeley Boulevard to Rutherford Road as a future 23m Minor Collector, permitting public access across the site. A direct north-south public road connection through the site is not anticipated during the life of the mall, however, should the mall redevelop at some time in the future, then the extension of Edgeley Boulevard to Rutherford Road from the south edge of Lot 14, as a public road, will be examined. In the interim, the City will secure public and transit access (if required) to the mall's internal circulation system to allow traffic to flow north and southbound to and from Rutherford Road. The internal circulation system shall be designed to a standard satisfactory to the City;
- d. A five lane ramp cross-section at the Highway No. 400 northbound off-ramp at its intersection with Rutherford Road;
- e. Rutherford Road widened to a basic six lanes between the Highway No. 400 westbound to southbound on-ramp and the east side of Jane Street, with left turn lanes at all intersections and exclusive right turn lanes at both intersections with Rutherford Road between Jane Street and Highway No. 400. The widening of Rutherford Road to six lanes may need to be extended further west to Weston Road. The exact limits of this road improvement will need to be determined in an Environmental Assessment Study;
- f. Dual northbound left turn lanes at the intersections with Rutherford Road between Jane Street and Highway No. 400, and traffic signals will also be required at the two intersections to Rutherford Road;
- g. Improvements to the intersection of Rutherford Road and Jane Street; and,
- h. Intersection improvements (including turning lanes and traffic signals) will also be required at the intersections of Jane Street/Locke Street and Jane Street and the new direct access to the Mall. The direct access to the transit facility from Jane Street shall be dedicated to the City of Vaughan as a public highway and designed to the satisfaction of the City of Vaughan and the Region of York.

▼ Map 12.6.A: Vaughan Mills Mall



## 12.7 North East Quadrant of Weston Road and Major Mackenzie Drive

### 12.7.1 General

12.7.1.1. Notwithstanding policy 9.2.1.1, the subject lands have been further delineated as Village District and Commercial District, shown on Map 12.7.A. A Village Promenade has also been identified on Map A within the Village District.

### 12.7.2 Policies Applicable Throughout The Subject Lands

12.7.2.1. Notwithstanding policy 9.2.1.1, the following uses shall be permitted:

- a. Business and Professional Office;
- b. Service Commercial uses, including Personal Service, Eating Establishment excluding a Drive-through Facility, Place of Entertainment, Bank and Financial Institution, Regulated Health Professional and Veterinary Clinic uses;
- c. Senior's Retirement, Nursing Home, or Long-term Care Facility uses up to a maximum of 100 units per hectare and, notwithstanding policy 9.2.1.5, 6 storeys, in height and subject to the following:
  - i. in exchange for public benefits such as, but not limited to:
    - A. underground parking;
    - B. increased on-site open space;
    - C. greater parkland dedication and/or facilities within the Village Core;
    - D. community and cultural facilities including community meeting rooms and halls, library space, theatres or similar facilities; and,
    - E. public transit facilities;
  - ii. and provided they can demonstrate conformity with the following criteria:
    - A. separation from Low-Rise Residential areas;
    - B. stepping down in height and density to adjacent residential and mixed-use areas;
    - C. massing complementary to 3 to storey buildings;
    - D. located on sites readily accessible and oriented to the Highway 400 corridor;
    - E. substantial high quality landscaped open space;
    - F. compatibility with the village scale and appearance of development;
    - G. appropriate relationship to the street and the pedestrian realm;
    - H. extent of public benefits required as set out in i) above;
    - I. demonstration through a traffic impact study that transportation capacity needs can be met;

- J. a shadow study to illustrate the potential impact on adjacent public or private realms; and,
    - K. concurrent submission and review of a site plan application;
  - d. Institutional, Civic and Community uses, including Libraries, Theatres, Arts and Cultural Centres, Day Care Centres and Government Services;
  - e. Places of Worship; and,
  - f. Urban Square, which is an area of land featuring hard and/or soft landscaping and/or architectural elements.
- 12.7.2.2. Notwithstanding policy 9.2.2.2, the following building types are permitted, pursuant to Section 9.2.3:
- a. Low-Rise buildings;
  - b. Mid-Rise buildings; and,
  - c. Public and Private Institutional buildings.
- 12.7.2.3. Development within the subject lands area shall be undertaken on a comprehensive basis, and shall provide an overall design to achieve proper access, internal traffic circulation, adequate parking, substantial landscaped area, and the desired urban design characteristics of the built form.
- 12.7.2.4. Development shall be related and scaled to pedestrian traffic, as well as being characterized by high quality design standards.
- 12.7.2.5. The applicable development standards shall be established in the implementing zoning by-law to provide for a second storey component along the Village Promenade, a portion of which must be functional.
- 12.7.2.6. There shall be significant landscaping, as well as pedestrian walkway connections, within the surface parking areas to create an attractive environment for pedestrians.
- 12.7.2.7. A combination of built form and landscaping shall be provided along Weston Road and Major Mackenzie Drive in order to screen surface parking and for pedestrian comfort.
- 12.7.2.8. There shall be significant landscaping to screen surface parking and provide an attractive street edge along all public streets.

- 12.7.2.9. The primary elevations for buildings located along Weston Road or Major Mackenzie Drive shall be prominent. A minimum of 50% of the main entrances for the buildings located along Weston Road or Major Mackenzie Drive shall be visible from Weston Road or Major Mackenzie Drive. Buildings may have more than one main entrance.

### **12.7.3 Policies Applicable to the Village District**

- 12.7.3.1. The Village District Area is the location of the most compact development form within the subject lands. It is intended to be an area of mixed-use retail commercial and office development, combining high order retail uses and eating establishments. Residential uses appropriately integrated into the area are also permitted.
- 12.7.3.2. In addition to the uses listed in 12.7.2.1, the following additional uses are permitted:
- a. retail commercial uses, including Pharmacy and Retail Store uses, but excluding retail commercial uses with a Gross Floor Area exceeding 5,750 square metres in a single unit as part of a building or a stand-alone building; and,
  - b. multi-unit residential uses, excluding townhouses.
- 12.7.3.3. The lands within the Village District area may accommodate development up to a maximum total Gross Floor Area (GFA) of 17,000 square metres.
- 12.7.3.4. To create the desired pedestrian oriented character and environment along the Village Promenade, the primary elevations of all buildings shall provide main entrances facing the Promenade, where a minimum of two (2) stand alone buildings shall be a minimum of two (2) storeys in height within the Village Promenade and all buildings shall be located close to the Promenade edge. The applicable development standards shall be established in the implementing zoning by-law.
- 12.7.3.5. Buildings shall have upgraded elevations (facades) where the building elevation is oriented towards Vellore Park Avenue, i.e., “turn the corner” at the eastern terminus of the Village Promenade, to mark the eastern gateway entrance to the Promenade.
- 12.7.3.6. Notwithstanding policy 9.2.1.5, the maximum building height along the Village Promenade frontage shall not exceed four (4) storeys.

- 12.7.3.7. Sidewalks shall be provided along the Village Promenade in order to allow for an active pedestrian environment. All buildings along the Promenade shall provide retail and/or other commercial uses at-grade.
- 12.7.3.8. The portion of the Village Promenade shown as “Pedestrian Only Promenade” on Map 12.7.A shall be accessible only by pedestrians. Vehicular traffic shall be prohibited.
- 12.7.3.9. A landscaped area at the north-east intersection of Weston Road and Major Mackenzie Drive, shown as an “Urban Square” on Map 12.7.A shall be provided to identify the gateway entrance to the Village Core Promenade. Built form shall frame the “Urban Square” in order to mark the gateway to the Promenade.
- 12.7.3.10. Parking for uses within the Village District shall be provided outside of the Pedestrian Only Promenade. Parking may be permitted within the areas shown as “Promenade Area ‘A’” on Map 12.7.A.
- 12.7.3.11. Parking for the uses within the Village District may be provided within the area designated as Commercial District between Weston Road and Vellore Park Avenue (extension), as shown on Map 12.7.A for the purposes of developing an attractive pedestrian-oriented Village Promenade.

#### **12.7.4 Policies Applicable to the Commercial District**

- 12.7.4.1. Within the Commercial District, larger scale retail commercial uses shall be located away from Weston Road and Major Mackenzie Drive, with smaller-scale retail commercial uses located closer to Weston Road and Major Mackenzie Drive.
- 12.7.4.2. In addition to the uses listed in 12.7.2.1, within the Commercial District the following additional uses are permitted:
  - a. Retail Commercial uses, including one Department Store which may include an accessory Autobody Repair Shop, Supermarket, Retail Stores and Pharmacy uses located within a Department Store; and,
  - b. Residential uses and compatible institutional uses, subject to policy 12.7.4.3, and provided such uses are limited to the Commercial District lands located between Weston Road and Vellore Park Avenue (extension).

- 12.7.4.3. The following shall apply to residential uses permitted within the Commercial District, as per policy 12.7.4.2.b:
- a. the permitted building types for residential uses shall be Detached Houses, Semi-Detached Houses and Townhouses, pursuant to Section 9.2.3;
  - b. they shall have a minimum residential density on any site of 17 units per hectare, and a maximum residential density on any site not exceeding 40 units per residential hectare;
  - c. the area included in the calculation of residential density shall include local and primary roads and the land for the residential units, but shall exclude all other lands; and
  - d. the maximum building height shall be two and one-half (2 1/2,) storeys.
- 12.7.4.4. The lands within the Commercial District area may accommodate development up to a maximum Gross Floor Area (GFA) of 5,500 square metres east of Vellore Park Avenue (extension) and up to a maximum GFA of 11,000 square metres west of Vellore Park Avenue (extension).
- 12.7.4.5. No individual Department Store or stand alone building shall be permitted with a GFA in excess of 17,500 square metres.
- 12.7.4.6. Within the block fronting onto Major Mackenzie Drive, buildings shall be located along the public street edge, in particular, along Vellore Park Avenue (extension) and Cityview Boulevard where the buildings shall provide attractive facades facing Vellore Park Avenue (extension) and Cityview Boulevard. The applicable development standards shall be established in the implementing zoning by-law.
- 12.7.4.7. To create the desired pedestrian oriented character and environment along Major Mackenzie Drive and Cityview Boulevard, the primary elevations of all buildings (a minimum of two (2) stand alone buildings) at the north-west intersection of Major Mackenzie Drive and Cityview Boulevard shall provide main entrances facing Major Mackenzie Drive and Cityview Boulevard. The applicable development standards shall be established in the implementing zoning by-law.
- 12.7.4.8. There shall be sufficient fencing and/or landscaping located along the northern boundary of the subject lands to provide a buffer between the commercial, residential community and park uses to the north.

- 12.7.4.9. Parking for the uses within the Commercial District area may be provided within the Village District, outside of the Village Promenade.
- 12.7.4.10. Outdoor storage and display of merchandise may be permitted for a Department Retail Store, but only in locations where their exposure will not detract from the general character of the area, and provided such uses and storage display areas are properly landscaped.

### **12.7.5 Urban Design - Policies Applicable to All Areas**

- 12.7.5.1. Where parking abuts a public street, there shall be significant landscaping to screen the parking from the street.
- 12.7.5.2. Service and loading/unloading areas shall not face the Village Promenade, and shall be screened from the street.
- 12.7.5.3. All elevations shall have a high quality of architectural design.
- 12.7.5.4. Buildings should “turn” the corner, i.e., they should have articulated facades towards driveways and public streets.
- 12.7.5.5. Setbacks from Major Mackenzie Drive and Weston Road should be substantially landscaped in order to define the public street edge and provide safety and amenity for pedestrians where built form is not located along the public street edge.
- 12.7.5.6. Parking, service and loading/unloading areas should occur off of internal driveways to the side or rear of the buildings.
- 12.7.5.7. Transformers should be located within buildings.
- 12.7.5.8. The visual impact of surface parking areas shall be mitigated with significant landscaping and pavement treatments including low walls, landscape materials, trees and lighting throughout the parking lots and along the edges.

### **Major Mackenzie Drive and Weston Road**

- 12.7.5.9. Within the Subject Lands, Weston Road and Major Mackenzie Drive shall reflect their role as major arterial transportation corridors providing transit service to the mixed-use commercial development within the Subject Lands.
- 12.7.5.10. The treatment of the streetscape along these arterial roads shall provide a mix of built form and substantial landscaping to provide for pedestrian comfort and direct access to the Village Promenade.
- 12.7.5.11. Entrances to the Subject Lands area shall be demarcated as gateways through a combination of built form and landscaping.

### **12.7.6 Urban Design – Policies Applicable to the Village District**

- 12.7.6.1. The intent of the urban design policies is to ensure that the built form, public realm and streetscaping are consistent with the objectives for the Subject Lands. The urban design policies complement and are integrated with the land use policies. Built form policies specify the preferred relationship of buildings to the street through policies governing building heights, massing, frontage and setbacks.
- 12.7.6.2. The preparation of Urban Design and Architectural Design Guidelines, prior to the approval of a Site Development application, shall be to the satisfaction of the City, and shall address, but not be limited to, the following:
  - a. Development to be in accordance with a comprehensive design scheme to be approved by the City, laying out the general orientation, configuration, massing and scale of major buildings and structures, and parking areas, together with access points, traffic circulation, pedestrian circulation, landscaping and buffering;
  - b. Detailed design guidelines respecting the proposed pedestrian promenade include building design guidelines, primary building entrance materials, streetscaping, signage, lighting, street furniture, and lay-by parking within the vehicular access portion of the Village Promenade;
  - c. Detailed design guidelines respecting the location and design of partial or outdoor storage, merchandise display areas and loading/unloading areas;
  - d. Building setbacks, minimum and maximum heights, compatible lighting, visual screening, landscaping, planting and/or fencing between commercial and residential areas; and,

- e. Guidelines respecting signage, including but not limited to, the design, size and amount of signage, and limiting the impact of sign related lighting on adjacent land uses.

### **Built Form and Pedestrian Realm**

- 12.7.6.3. The ground floor coverage of buildings within the Village District should be maximized.
- 12.7.6.4. A second storey shall be provided for a minimum of two (2) buildings located along the Village Promenade.
- 12.7.6.5. Buildings along the Village Promenade shall be sited and designed to provide interest and comfort at the ground level for pedestrians.
- 12.7.6.6. Buildings along the Village Promenade shall be located close to the sidewalk/ pedestrian promenade edge and a sidewalk/pedestrian promenade shall be provided to comfortably accommodate pedestrians, streetscaping elements, landscaping and outdoor patios. No building shall be located further than three (3) metres from the edge of the Village Promenade.
- 12.7.6.7. Buildings shall frame the pedestrian gateway to the Village Promenade through the Urban Square located at the north-east intersection of Weston Road and Major Mackenzie Drive.
- 12.7.6.8. Buildings shall frame the access points to the Village Promenade, where the Promenade intersects at Weston Road, as well as at where it intersects with Vellore Park Avenue.
- 12.7.6.9. Buildings along the Village Promenade should be designed to exhibit a “Village” character with articulated roofs and canopies for weather protection.
- 12.7.6.10. Buildings along the Village Promenade shall be sited and organized so that they are parallel to the Promenade and appropriately turn the corners.
- 12.7.6.11. Buildings along the Village Promenade shall contain an appropriate amount of main entrances facing onto the Promenade to provide for an active pedestrian-friendly

environment. The applicable development standards requiring main entrances for a portion of the Village Promenade shall be established in the implementing zoning by-law.

- 12.7.6.12. The zoning by-law shall establish a minimum proportion of frontage along the Village Promenade which shall be occupied by buildings.
- 12.7.6.13. The zoning by-law shall establish a minimum proportion of the length of the Village Promenade that shall be faced with a building wall of a minimum of two (2) storeys in height.
- 12.7.6.14. The depths of buildings located within the Village Promenade shall not exceed 50 metres, in order to encourage street-related development.
- 12.7.6.15. Pedestrian and bicycle connections shall be provided to the Village Promenade from all public streets.
- 12.7.6.16. Buildings located along the Village Promenade should have the highest level of architectural detailing and distinct architectural appearance. Variation in three-dimensional elements, such as cornices, window trims and entrances can be used to create a dynamic façade image, as opposed to a monotonous and repetitive façade along the Promenade.
- 12.7.6.17. Buildings located adjacent to the public streets shall provide attractive facades facing the streets with direct pedestrian connections to entrances.

### **Parking, Service and Loading/Unloading Areas**

- 12.7.6.18. Parking, servicing and loading/unloading areas should have the least possible impact on the Village District Promenade streetscape and Urban Squares in the Village District, and shall utilize significant landscaping to screen these uses from the street.

### **The Village Promenade**

- 12.7.6.19. The Village Promenade shall reflect its role and function to provide vehicular access as well as a high level of pedestrian amenity.

- 12.7.6.20. Street trees, as well as a pedestrian weather protection system, including design elements such as awnings and canopies, shall be provided along the Village Promenade and adjacent to the entrances of all buildings.
- 12.7.6.21. There shall be consistency in street furniture and lighting standards along the Village Promenade which displays a “Village” character.
- 12.7.6.22. Building walls for units facing the Village Promenade shall provide a “Village” character.
- 12.7.6.23. Barrier-free building design of buildings, streets and publicly accessible spaces shall be provided.

#### **Parking and Driveways**

- 12.7.6.24. Parking shall not be permitted within the Village Promenade except for within the area identified as “Promenade Area ‘A’” as shown on Map 12.7.A. Parking shall be parallel parking adjacent to the sidewalks.

#### **Loading/Unloading Areas**

- 12.7.6.25. There shall be no loading requirement for buildings located within the Village Promenade. Where loading/unloading is provided, these areas cannot face a street unless they can be adequately screened from view and loading areas cannot face the Village Promenade.

#### **Outdoor Storage and Refuse Areas**

- 12.7.6.26. No outdoor storage is permitted within the Village District.
- 12.7.6.27. All refuse areas, i.e., garbage and recycling, shall be located internally within a building.

### **12.7.7 Urban Design – Policies applicable to the Commercial District Built Form and Pedestrian Realm**

- 12.7.7.1. Larger scale buildings within the Commercial District area shall be sited away from Major Mackenzie Drive and Weston Road.
- 12.7.7.2. Smaller scale buildings within the Commercial District area shall be sited close to Major

Mackenzie Drive and Weston Road, and generally oriented to face Vellore Park Avenue and Cityview Boulevard, in order to create an inviting pedestrian friendly streetscape.

- 12.7.7.3. Internal driveways through the Commercial District area shall be laid out in a grid network pattern with pedestrian connections provided through the parking areas.
- 12.7.7.4. Buildings shall frame the access points to the Commercial District area at Weston Road, Vellore Park Avenue and Cityview Boulevard.
- 12.7.7.5. Pedestrian and bicycle connections shall be provided to the main entrances of buildings from all public streets.
- 12.7.7.6. All buildings shall have a high level of architectural detailing and articulation, in particular larger scale retail uses.

### **Cityview Boulevard and Vellore Park Avenue**

- 12.7.7.7. Vellore Park Avenue and Cityview Boulevard shall provide vehicular access, as well as a high level of pedestrian amenity, at the entrances to the Commercial District.
- 12.7.7.8. Street trees, as well as substantial landscaping, should provide pedestrian comfort and screen surface parking areas.
- 12.7.7.9. There shall be consistency in street furniture and lighting standards.
- 12.7.7.10. Building walls for units facing Cityview Boulevard and Vellore Park Avenue shall be attractive and articulated to provide for pedestrian comfort.
- 12.7.7.11. Barrier-free building design of buildings, streets and publicly accessible spaces shall be provided.

### **Parking and Driveways**

- 12.7.7.12. Pedestrian walkways and landscaped islands shall be provided to break up the surface parking areas and ensure adequate pedestrian linkages between buildings and public streets.

### **Loading/Unloading Area**

- 12.7.7.13. Loading shall be provided in accordance with the applicable zoning regulations, with the exception of eating establishment uses, which shall not be required to provide loading spaces. Where loading is provided, these areas are not permitted in any yard facing a street, unless they can be adequately screened from view, and in no case shall face the Village Promenade.

### **Outdoor Storage and Refuse Areas**

- 12.7.7.14. No outdoor storage is permitted within the Commercial District, with the exception of the outdoor storage and display of goods related to a retail nursery accessory to a Department Store.

## **12.7.8 Sustainability Community Objectives**

- 12.7.8.1. Sustainable community objectives be implemented through neighbourhood designs that supports cycling and walking, ensures neighbourhood connectivity to the broader community, and provides transit opportunities, water and energy efficiencies, energy alternatives and green building design and site development.

## **12.7.9 Road Network**

- 12.7.9.1. Development within the Village District and Commercial District areas will include the construction of Vellore Park Avenue from its existing terminus at the northern boundary of the District Centre to connect with Major Mackenzie Drive to the south. The proposed right-of-way for the extension of Vellore Park Avenue shall be a minimum of 23 metres.
- 12.7.9.2. The internal driveways within the Village District and Commercial District areas are organized in a grid pattern in order to facilitate efficient vehicular and pedestrian circulation and provide a network that can evolve into a more dense urban form over time. In order to preserve this grid network pattern, no development is permitted to occur within the areas shown as streets or private driveways on Map 12.7.A. The private driveways shown on Map 12.7.A are conceptual, with their final locations to be determined through the Site Plan Approval process.
- 12.7.9.3. Roundabouts or other similar traffic-calming or traffic control measures may be implemented for the private driveways, subject to evaluation at the site plan stage.

12.7.9.4. Individual direct access to any arterial public street shall be discouraged.

### **12.7.10 Pedestrian and Bicycle System**

12.7.10.1. The development of all public streets shall include safe, convenient and attractive facilities for pedestrians and cyclists.

12.7.10.2. An adequate supply of secure bicycle parking shall be provided near transit stops, in high activity areas and open space areas.

12.7.10.3. The Pedestrian Only Promenade within the Village Promenade shall be restricted to pedestrian-only access.

12.7.10.4. Appropriate pedestrian and bicycle access shall be provided wherever a greenway system traverses a commercial centre, to the satisfaction of the City.

### **12.7.11 Parking**

12.7.11.1. The sharing of parking in mixed-use developments located in either the Village District or Commercial District areas is encouraged, subject to evaluation by the City.

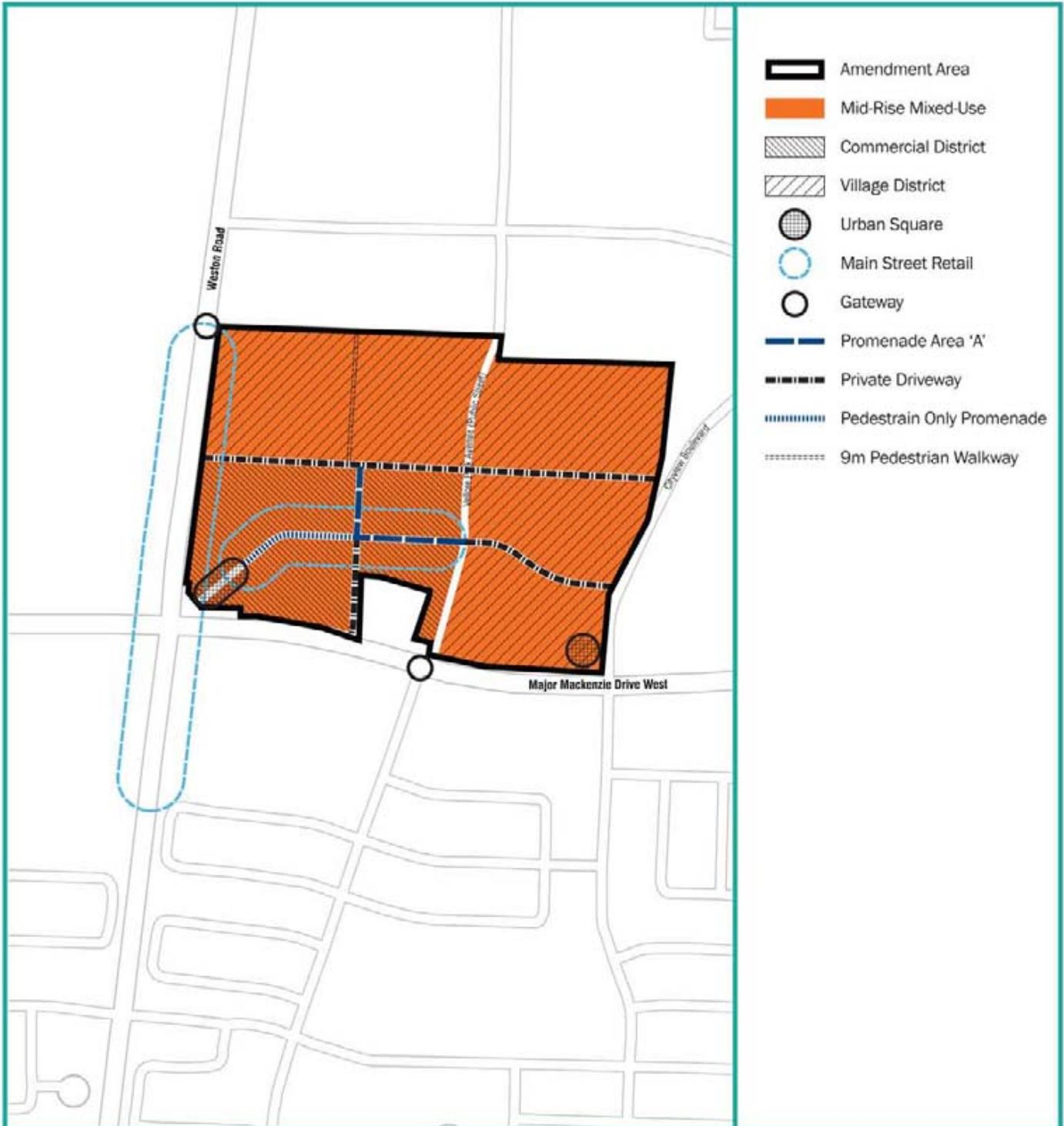
12.7.11.2. In order to provide for additional gross floor area within the Village District as well as to provide additional landscaped open space, a reduction to the current parking standards may be applied. These standards may be applied in a zoning by-law for the Village District and/or Commercial District areas.

### **12.7.12 Implementation**

12.7.12.1. The Owner is encouraged to contribute their proportionate share towards the provision of major community and infrastructure facilities such as schools, parks, greenways, roads and road improvements, external services and stormwater management facilities. Property owners will be required to enter into one or more agreements as a condition of development approval, providing for the equitable distribution of the costs of the land and community facilities. The Trustee for Block 33 West shall provide the City with a letter indicating that the Owner has fulfilled all cost sharing and other obligations of the Block 33 West Developers' Group Agreement.

- 12.7.12.2. The Holding Symbol “(H)” shall be used to facilitate coordinated development with the surrounding land uses and shall not be removed from the Village District and Commercial District areas on Map 12.7.A until such time that the following conditions are addressed for the Subject Lands or (Phase) thereof, to the satisfaction of the City:
- a. The approval of a site plan for the Subject Lands or portion (Phase) thereof;
  - b. The determination of the future land uses, including the disposition of the road network respecting the southerly extension of Zachary Place and the disposition of the north/south traversing 9 metres pedestrian walkway, and transition between the zoned residential land for Plan of Subdivision 19T-00V03 (Lormel Developments [Weston] Inc. and Ozner Corporation [South]) and the retail commercial uses proposed for the Village District and Commercial District area;
  - c. The determination of the future land uses, including the transition between the zoned residential land for Plan of Subdivision 19T-03V08 (Argento Developments Inc.) and the retail commercial uses proposed for the Commercial District area;
  - d. The provision of agreements/land transfer for the elementary school block and neighbourhood park block lands; and,
  - e. The Owner shall provide the City with an updated Block Plan for Block 33 West showing the associated land uses and statistics for approval by Vaughan Council.

▼ Map 12.7.A: North East Quadrant of Weston Road and Major Mackenzie Drive



## 12.8 Block 61 West - Nashville Heights

### 12.8.1 Introduction

- 12.8.1.1. The area indicated as the Amendment Area on Map 12.8.A shall be known as Nashville Heights.
- 12.8.1.2. The population estimate for Nashville Heights is 8,000 people.
- 12.8.1.3. The phasing for Nashville Heights, if any, shall be addressed at the Block Plan Stage and/or Draft Plan of Subdivision Stage, based on the availability of the water supply and sanitary services for Nashville Heights.
- 12.8.1.4. Growth shall be staged and phased, based on the availability of the water supply and sanitary services for the Nashville Heights Neighbourhood.
- 12.8.1.5. The area included in the calculation of Residential Density for Nashville Heights shall be based on a net residential hectare, which includes the lands for local and primary roads, the lands for dwelling units, and stormwater management pond facilities, but excluding lands associated with protected natural features such as woodlots, valleylands and wetlands and the Nashville Cemetery. Density shall be calculated on a Block Plan basis, and the Draft Plans of Subdivision and Site Plans shall conform.

### 12.8.2 Low-Rise Residential

- 12.8.2.1. The Plan recognizes the **Low-Rise Residential** Areas within Nashville Heights in the Nashville Community, where development is to be compatible with the scale and character of the existing community. The **Low-Rise Residential** Areas are predominantly residential in character with a range of lot sizes and building forms. Urban design and sustainability policies shall be implemented to create pedestrian friendly neighbourhoods that support walking, cycling and transit; park and greenway systems; and elementary schools, as well as connect to the broader community.
- 12.8.2.2. Notwithstanding policy 9.2.1.1 the permitted uses in **Low-Rise Residential** Areas shall include:
  - a. residential units;
  - b. schools;
  - c. parks and open spaces;
  - d. home occupation; and,
  - e. private home daycare.

- 12.8.2.3. Notwithstanding policy 9.2.1.6 the designated **Low-Rise Residential** Areas shall permit a residential density ranging from a minimum of 15 units per net residential hectare to a maximum of 25 units per net residential hectare.
- 12.8.2.4. The following building types are permitted in **Low-Rise Residential** Areas pursuant to Section 9.2.3:
- a. Detached Houses;
  - b. Semi-Detached Houses;
  - c. Townhouses; and,
  - d. Low-Rise buildings.
- 12.8.2.5. Notwithstanding policy 9.2.1.5, the maximum building height is four (4) storeys.
- 12.8.2.6. The location of buildings with three (3) or more residential units within the **Low-Rise Residential** designation shall be in accordance with the following criteria:
- a. adjacent to more intensive land uses;
  - b. fronts or be adjacent to an arterial road or a primary road;
  - c. mitigates noise sources such as arterial roads or highways;
  - d. requires the submission of a sun/shadow study;
  - e. compatible with the surrounding land uses; and,
  - f. provides an appropriate transition with the adjacent land uses including, but not limited to, the tiering of buildings (vertical and horizontal setbacks at the upper storeys, angular planes from the property lines, and placement of buildings to minimize shadow impacts and micro-climate conditions), buffering and landscaping.
- 12.8.2.7. Prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, that the following matters, but not limited to, shall be addressed through the Block Plan process and shall include:
- a. Block Plan Report and Block Plan Land Use Schedule;
  - b. Master Environmental and Servicing Plan;
  - c. Phase I Environmental Site Assessment, and where required, Phase II Environmental Site Assessments;
  - d. Transportation Analysis/Study;

- e. Travel Demand Management Plan/Study;
- f. Traffic/Transit Management and Sidewalk/Walkway Master Plan;
- g. Pedestrian and Bicycle Master Plan;
- h. Noise and Vibration Impact/Mitigation Report;
- i. Woodlot/Tree Inventory and Terrestrial Resources Mitigation/Restoration Report;
- j. Parks and Open Space Plan;
- k. Sustainability Report;
- l. Development Phasing/Infrastructure Staging Plan;
- m. Archaeological Assessment/Cultural Heritage Resource Assessment; and,
- n. Urban Design Guidelines.

12.8.2.8. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a registered Plan of Subdivision, that the following matters, but not limited to, shall be refined through the Block Plan process:

- a. the delineation of land uses;
- b. the delineation and design of the road network;
- c. the limits of development;
- d. the proposed realignment of the watercourses;
- e. the delineation and design of the stormwater management pond facilities; and,
- f. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans.

12.8.2.9. Sustainable community objectives shall be implemented through neighbourhood designs that support cycling and walking within and external to the community, including connectivity to transit facilities; ensure neighbourhood connectivity to the broader community; provide transit opportunities; provide water and energy efficiencies, and energy alternatives; and support green building design and site development, to the satisfaction of the City.

12.8.2.10. As a condition of development approval, valley and stream corridors determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.

- 12.8.2.11. Wooded areas in and adjacent to valley and stream corridors shall be maintained in a natural undisturbed state due to their environmental sensitivity, aesthetics, proper flood and erosion control and other functions, and that the appropriate buffers be provided, in consultation with the Toronto and Region Conservation Authority, to the satisfaction of the City. As a condition of development approval, wooded areas determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.
- 12.8.2.12. The residential uses within the **Low-Rise Residential** designation shall be serviced by full municipal services including water supply and sanitary sewers, and shall require the formal allocation of water supply and sewage servicing capacity by Council.
- 12.8.2.13. Development within the **Low-Rise Residential** designated areas shall be subject to Architectural Control. All development shall proceed in accordance with the Council approved Architectural Design Guidelines.

### 12.8.3 Mid-Rise Residential

- 12.8.3.1. Development in **Mid-Rise Residential** Areas within Nashville Heights is generally located along arterial, collector and primary roads. The **Mid-Rise Residential** Areas are predominantly residential, with a range of lot sizes and building forms, including buildings up to a maximum of ten (10) storeys in height, and provide a transition to the surrounding **Low-Rise Residential** Areas. Urban design and sustainability policies shall be implemented to create pedestrian friendly neighbourhoods that support walking, cycling and transit; park and greenway systems; and elementary schools, as well as connect to the broader community.
- 12.8.3.2. Notwithstanding policy 9.2.1.6, the designated **Mid-Rise Residential** Areas shall permit a residential density ranging from a minimum of 25 units per net residential hectare to a maximum of 150 units per net residential hectare.
- 12.8.3.3. Notwithstanding policy 9.2.1.1 the permitted uses within **Mid-Rise Residential** Areas shall include:
- a. residential units,
  - b. hospice associated with a hospital or other regulated medical health care/support facility;

- c. schools;
  - d. parks and open spaces;
  - e. home occupation; and,
  - f. private home daycare.
- 12.8.3.4. The following building types are permitted in **Mid-Rise Residential** Areas pursuant to Section 9.2.3:
- a. Detached Houses;
  - b. Semi-Detached Houses;
  - c. Townhouses;
  - d. Stacked Townhouses;
  - e. Low-Rise buildings;
  - f. Mid-Rise buildings; and,
  - g. Public and Private Institutional Buildings.
- 12.8.3.5. Notwithstanding policy 9.2.1.5, the maximum building height is ten (10) storeys.
- 12.8.3.6. The location of buildings with five (5) or more storeys within the **Mid-Rise Residential** designation shall be in accordance with the following criteria:
- a. directly abut Huntington Road;
  - b. mitigates noise sources such as arterial roads or highways;
  - c. requires the submission of a sun/shadow study; and,
  - d. provides an appropriate transition with the adjacent land uses including, but not limited to, the tiering of buildings (vertical and horizontal setbacks at the upper storeys, angular planes from the property lines, and placement of buildings to minimize shadow impacts and micro-climate conditions), buffering and landscaping.
- 12.8.3.7. Prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, that the following matters, but not limited to, shall be addressed through the Block Plan process include:
- a. Block Plan Report and Block Plan Land Use Schedule;
  - b. Master Environmental and Servicing Plan;
  - c. Phase I Environmental Site Assessment, and where required, a Phase II

- Environmental Site Assessment;
- d. Transportation Analysis/Study;
- e. Travel Demand Management Plan/Study;
- f. Traffic/Transit Management and Sidewalk/Walkway Master Plan;
- g. Pedestrian and Bicycle Master Plan;
- h. Noise and Vibration Impact/Mitigation Report;
- i. Woodlot/Tree Inventory and Terrestrial Resources Mitigation/Restoration Report;
- j. Parks and Open Space Plan;
- k. Sustainability Report;
- l. Development Phasing/Infrastructure Staging Plan;
- m. Archaeological Assessment/Cultural Heritage Resource Assessment; and,
- n. Urban Design Guidelines.

- 12.8.3.8. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a Plan of Subdivision, that the following matters, but not limited to, shall be refined through the Block Plan process:
- a. the delineation of land uses;
  - b. the delineation and design of the road network;
  - c. the limits of development;
  - d. the proposed realignment of the watercourses;
  - e. the delineation and design of the stormwater management pond facilities; and,
  - f. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans.

- 12.8.3.9. Sustainable community objectives shall be implemented through neighbourhood designs that support cycling and walking within and external to the community, including connectivity to transit facilities; ensure neighbourhood connectivity to the broader community; provide transit opportunities; provide water and energy efficiencies, and energy alternatives; and support green building design and site development, to the satisfaction of the City.

- 12.8.3.10. As a condition of development approval, valley and stream corridors determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.

- 12.8.3.11. Wooded areas in and adjacent to valley and stream corridors shall be maintained in a natural undisturbed state due to their environmental sensitivity, aesthetics, proper flood and erosion control and other functions, and that the appropriate buffers be provided, in consultation with the Toronto and Region Conservation Authority, to the satisfaction of the City. As a condition of development approval, wooded areas determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.
- 12.8.3.12. The residential uses within the **Mid-Rise Residential** designation shall be serviced by full municipal services including water supply and sanitary sewers, and shall require the formal allocation of water supply and sewage servicing capacity by Council.
- 12.8.3.13. Development within the **Mid-Rise Residential** designated areas shall be subject to Architectural Control. All development shall proceed in accordance with the Council approved Architectural Design Guidelines.

#### **12.8.4 Mid-Rise Mixed-Use Area A and Area B**

- 12.8.4.1. The **Mid-Rise Mixed-Use** Areas, which includes **Area A** and **Area B** within Nashville Heights, includes development that is generally located along arterial, collector and primary roads. The **Mid-Rise Mixed-Use** Areas A and B are generally residential, with a range building forms, including buildings up to a maximum of ten (10) storeys in height, and limited commercial uses in a building that includes residential dwelling units, provided the commercial uses and residential uses are not on the same floor. Urban design and sustainability policies shall be implemented to create pedestrian friendly neighbourhoods that support walking, cycling and transit; park and greenway systems; and elementary schools, as well as connect to the broader community.
- 12.8.4.2. Notwithstanding policy 9.2.1.6, The designated **Mid-Rise Mixed-Use Area A** and **Mid-Rise Mixed-Use Area B**, and **Mid-Rise Mixed-Use Area B** within the Major Mackenzie Drive Alignment Special Study Area shall permit a residential density for **Area A** ranging from a minimum of 25 units per net residential hectare up to a maximum of 150 units per net residential hectare, and shall permit a residential density for **Area B** ranging from a minimum of 35 units per net residential hectare up to a maximum of 150 units per net residential hectare.

- 12.8.4.3. Notwithstanding policy 9.2.1.1 the permitted uses shall include:
- a. residential units;
  - b. retail store;
  - c. personal service shop;
  - d. business or professional office;
  - e. hospice associated with a hospital or other regulated medical health care/support facility;
  - f. day nursery;
  - g. private home daycare;
  - h. schools;
  - i. parks and open spaces;
  - j. home occupation; and,
  - k. commercial uses (i.e., retail store, personal service shop, and business or professional office uses) provided the use is in a building that includes residential dwelling units, and provided the commercial uses and residential uses are not on the same floor, and the gross floor area for the commercial use(s) shall be determined in the implementing zoning by-law.
- 12.8.4.4. The following building types are permitted in **Mid-Rise Mixed-Use Area A** and **Area B** pursuant to Section 9.2.3:
- a. Townhouses;
  - b. Stacked Townhouses;
  - c. Low-Rise buildings;
  - d. Mid-Rise buildings; and,
  - e. Public and Private Institutional Buildings.
- 12.8.4.5. Notwithstanding policy 9.2.1.5, the maximum height of the buildings within the **Mid-Rise Mixed-Use Area A** and **Mid-Rise Mixed-Use Area B** designations shall not exceed a height of ten (10) storeys.
- 12.8.4.6. The location of buildings with five (5) or more storeys within the **Mid-Rise Mixed-Use Area A** and **Mid-Rise Mixed-Use Area B** designations, shall be in accordance with the following criteria:
- a. directly abuts Huntington Road or Major Mackenzie Drive;
  - b. mitigates noise sources such as arterial road or highways;

- c. requires the submission of a sun/shadow study; and,
- d. provides a transition to the detached, semi-detached, street townhouse, block townhouse and multiple dwellings (to not exceed the permitted maximum density for the **Mid-Rise Mixed-Use A** and **Mid-Rise Mixed-Use Area B** designations), which may include, but not limited to the tiering of buildings (vertical and horizontal setbacks at the upper storeys, angular planes from the property lines, and placement of buildings to minimize shadow impacts and micro-climate conditions), buffering and landscaping.

12.8.4.7. The automobile service station or gas bar uses shall be permitted within the **Mid-Rise Mixed-Use Area A** and **Mid-Rise Mixed-Use Area B** and **Mid-Rise Mixed-Use Area B** within the Major Mackenzie Drive Alignment Special Study Area designation subject to the following criteria:

- a. allow only the sale of automobile fuel, oil, lubricant and other related products, and the minor provision of repair and maintenance service;
- b. may provide a convenience retail store, to not exceed a maximum gross floor area of 280 square metres, which may include a convenience eating establishment with a drive-through to not exceed 25% of the convenience retail store's gross floor area;
- c. prohibit a drive-through associated with a convenience eating establishment or a car wash to be located between a building and a street, or between a building and a highway and the associated highway egress and ingress ramps;
- d. regulate the pump island and carwash locations, and outside storage, if permitted, in the implementing zoning by-law;
- e. may require a noise study for car washes abutting a Residential or a Mixed-Use Area;
- f. automobile service station or gas bar uses shall be developed pursuant to an approved site plan, which demonstrates an appropriate lot and/or block size and configuration, proper site access, internal traffic circulation, adequate parking, substantial landscaped areas and appropriate urban design characteristics that will ensure compatibility with adjacent lands;
- g. automobile service station or gas bar buildings and sites shall have a design, appearance and scale that shall be consistent with the existing and intended character of the surrounding community, with consideration given to the aesthetic design;

- h. limit the number of automobile service stations or gas bars to not exceed a maximum of one at the intersection of one at the intersection of Huntington Road or Major Mackenzie Drive and a collector road, or Huntington Road or Major Mackenzie Drive and a primary road, and not to exceed a maximum of two automobile service stations or gas bars for the Nashville Heights Community; and,
- i. design criteria shall be established in the Urban Design Guidelines and Architectural Control Guidelines for automobile service station or gas bar, and carwash buildings, as well as the provision of a minimum separation distance.

12.8.4.8. Lands designated **Mid-Rise Mixed-Use Area A**, **Mid-Rise Mixed-Use Area B** and **Mid-Rise Mixed-Use Area B** within the Major Mackenzie Drive Alignment Special Study Area shall be subject to the following:

- a. Prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, that the following matters, but not limited to, shall be addressed through the Block Plan process and shall include:
  - i. Block Plan Report and Block Plan Land Use Schedule;
  - ii. Master Environmental and Servicing Plan;
  - iii. Phase I Environmental Site Assessment, and where required, a Phase II Environmental Site Assessment;
  - iv. Transportation Analysis/Study;
  - v. Travel Demand Management Plan/Study;
  - vi. Traffic/Transit Management and Sidewalk/Walkway Master Plan;
  - vii. Pedestrian and Bicycle Master Plan;
  - viii. Noise and Vibration Impact/Mitigation Report;
  - ix. Woodlot/Tree Inventory and Terrestrial Resources Mitigation/Restoration Report;
  - x. Parks and Open Space Plan;
  - xi. Sustainability Report;
  - xii. Development Phasing/Infrastructure Staging Plan;
  - xiii. Archaeological Assessment/Cultural Heritage Resource Assessment; and,
  - xiv. Urban Design Guidelines;
- b. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a registered Plan of

Subdivision, that the following matters, but not limited to, shall be refined through the Block Plan process:

- i. the delineation of land uses;
  - ii. the delineation and design of the road network;
  - iii. the limits of development;
  - iv. the proposed realignment of the watercourses;
  - v. the delineation and design of the stormwater management pond facilities; and,
  - vi. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans;
- c. The final uses within the Major Mackenzie Drive Alignment Special Study Area, including the delineation of land uses, valley and stream corridors, road network and stormwater management facilities shall coincide with:
- i. the Highway 427 Environmental Assessment (EA) Corridor Study Area by the Ministry of Transportation, which will identify the preferred corridor and terminus for the north expansion of Highway 427;
  - ii. the Western Vaughan Transportation Individual Environmental Assessment (IEA), which will identify the alignment of Major Mackenzie Drive, including the interchange of the future north expansion of Highway 427 and Major Mackenzie Drive, and the preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line;
  - iii. the City-wide Vaughan Transportation Master Plan, and have regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which will explore the overall layout and alignment of the collector and local road network, including road connectivity to the western portion of Vaughan and mid-block collector and community local road connectivity, and the Huntington Road realignment; and,
  - iv. the York Region Transportation Master Plan Update;
- d. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a registered Plan of Subdivision, that the following matters for the Major Mackenzie Drive Alignment

Special Study Area, but not limited to, shall be refined, in consultation with, the Ministries of Municipal Affairs and Housing and/or Transportation, York Region, and Toronto and Region Conservation Authority, to the satisfaction of the City:

- i. the delineation of land uses;
  - ii. the delineation and design of the road network;
  - iii. the limits of development;
  - iv. the proposed realignment of the watercourses;
  - v. the delineation and design of the stormwater management pond facilities;  
and,
  - vi. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans;
- e. The Subdivision Owner(s) shall enter into an agreement, prior to any Draft Plan of Subdivision approval, which shall state that the pre-sales of lots and/or blocks for dwelling units, within the Major Mackenzie Drive Alignment Study Area affected by the Environmental Assessments, will not be permitted for the lands, until:
- i. an Environmental Assessment has identified a preferred alignment for Major Mackenzie Drive, the extension of Highway 427, the associated grade separation of the CP Railway Line, and the preferred location for Huntington Road and the collector roads intersecting with Major Mackenzie Drive; and,
  - ii. York Region will consider a phased or partial release of the lots and/or blocks, when the Environmental Assessment or a Feasibility Study (undertaken by the City of Vaughan and/or the Subdivision Owner(s)) has identified, in consultation with York Region, to the satisfaction of the City, that particular lots and/or blocks are no longer impacted by the proposed alignment and grade separation of Major Mackenzie Drive;
- f. Sustainable community objectives shall be implemented through neighbourhood designs that support cycling and walking within and external to the community, including connectivity to transit facilities; ensure neighbourhood connectivity to the broader community; provide transit opportunities; provide water and energy efficiencies, and energy alternatives; and support green building design and site development, to the satisfaction of the City;

- g. As a condition of development approval, valley and stream corridors determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances; and,
- h. Wooded areas in and adjacent to valley and stream corridors shall be maintained in a natural undisturbed state due to their environmental sensitivity, aesthetics, proper flood and erosion control and other functions, and that the appropriate buffers be provided, in consultation with the Toronto and Region Conservation Authority, to the satisfaction of the City. As a condition of development approval, wooded areas determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.

12.8.4.9. The residential uses within the **Mid-Rise Mixed-Use Area A**, **Mid-Rise Mixed-Use Area B** and **Mid-Rise Mixed-Use Area B** located within the Major Mackenzie Drive Alignment Special Study Area” designation shall be serviced by full municipal services including water supply and sanitary sewers, and shall require the formal allocation of water supply and sewage servicing capacity by Council.

12.8.4.10. Development within the **Mid-Rise Mixed-Use Area A**, **Mid-Rise Mixed-Use Area B** and **Mid-Rise Mixed-Use Area B** within the Major Mackenzie Drive Alignment Special Study Area designated areas shall be subject to Architectural Control. All development shall proceed in accordance with the Council approved Architectural Design Guidelines.

## 12.8.5 Mid-Rise Mixed-Use Area C

12.8.5.1. The Plan recognizes the **Mid-Rise Mixed-Use Area C** within Nashville Heights, where development is generally located along arterial, collector and primary roads. The **Mid-Rise Mixed-Use Area C** allows for residential, with a range building forms, including buildings up to a maximum of ten (10) storeys in height, mixed-use residential-commercial and commercial uses to serve the local community. Urban design and sustainability policies shall be implemented to create pedestrian friendly neighbourhoods that support walking, cycling and transit; park and greenway systems; and elementary schools, as well as connect to the broader community.

- 12.8.5.2. Notwithstanding policy 9.2.1.6 The designated **Mid-Rise Mixed-Use Area C** and **Mid-Rise Mixed-Use Area C** within the Major Mackenzie Drive Alignment Special Study Area shall permit a residential density ranging from a minimum of 35 units per net residential hectare to a maximum of 150 units per net residential hectare.
- 12.8.5.3. Notwithstanding policy 9.2.1.1, the permitted uses shall include:
- a. residential uses permitted in the **Mid-Rise Mixed-Use Area B** designation subject to the policies for the **Mid-Rise Mixed-Use Area B** designation;
  - b. supermarket;
  - c. pharmacy;
  - d. retail store;
  - e. eating establishment;
  - f. eating establishment, convenience;
  - g. eating establishment, take-out;
  - h. eating establishment, convenience with drive-through;
  - i. banks or financial institution; and,
  - j. business or professional office.
- 12.8.5.4. Notwithstanding policy 9.2.1.5, the maximum height of the buildings within the Mixed-Use Residential-Commercial C designation shall not exceed a height of ten (10) storeys;
- 12.8.5.5. The commercial uses shall be permitted within the **Mid-Rise Mixed-Use Area C** designation subject to the following criteria:
- a. development of **Mid-Rise Mixed-Use Area C** shall be in accordance with a comprehensive design scheme, approved by the City, which shall provide for the development and redevelopment of the area, and shall lay out the general orientation and configuration of major buildings and structures, and parking areas, together with access points, traffic circulation, pedestrian circulation, landscaping and buffering;
  - b. outdoor storage and display of merchandise may be permitted, but only in locations where their exposure will not detract from the general character of the area, and provided such uses and storage display areas are properly landscaped;
  - c. building setbacks, compatible lighting, visual screening, planting and/or fencing shall be provided between commercial uses and residential uses; and,
  - d. appropriate provision shall be made for pedestrian and bicycle trails wherever the Greenway System traverses a commercial centre.

- 12.8.5.6. The automobile service station or gas bar uses shall be permitted within the **Mid-Rise Mixed-Use Area C** designation subject to the following criteria:
- a. allow only the sale of automobile fuel, oil, lubricant and other related products, and the minor provision of repair and maintenance service;
  - b. may provide a convenience retail store, to not exceed a maximum gross floor area of 280 square metres, which may include a convenience eating establishment with a drive-through to not exceed 25% of the convenience retail store's gross floor area;
  - c. prohibit a drive-through associated with a convenience eating establishment or a car wash to be located between a building and a street;
  - d. regulate the pump island location, and outside storage, if permitted, in the implementing zoning by-law;
  - e. may require a noise study for car washes abutting a Residential or a Mixed-Use Area;
  - f. automobile service station or gas bar uses shall be developed pursuant to an approved site plan, which demonstrates an appropriate lot and/or block size and configuration, proper site access, internal traffic circulation, adequate parking, substantial landscaped areas and appropriate urban design characteristics that will ensure compatibility with adjacent lands;
  - g. automobile service station or gas bar buildings and sites shall have a design, appearance and scale that shall be consistent with the existing and intended character of the surrounding community, with consideration given to the aesthetic design;
  - h. limit the number of automobile service stations or gas bars to not exceed a maximum of one at the intersection of one at the intersection of Major Mackenzie Drive and a collector road, or Major Mackenzie Drive and a primary road, and not to exceed a maximum of two automobile service stations or gas bars for the Nashville Heights Community; and,
  - i. design criteria shall be established in the Urban Design Guidelines and Architectural Control Guidelines for automobile service station or gas bar, and carwash buildings, as well as the provision of a minimum separation distance.

- 12.8.5.7. The drive-through facility for an eating establishment, convenience with drive-through shall be subject to the following criteria:
- a. the appropriateness of permitting an eating establishment, convenience with drive-

- through shall be determined at the implementing zoning by-law stage;
- b. prohibit a drive-through associated with an eating establishment, convenience with drive-through to be located between a building and a street; and,
  - c. design criteria shall be established in the Urban Design Guidelines and Architectural Control Guidelines for drive-through facilities, which shall include provisions for the location, screening and landscaping of the drive-through facilities.

12.8.5.8. Lands designated **Mid-Rise Mixed-Use Area C** and **Mid-Rise Mixed-Use Area C** within the Major Mackenzie Drive Alignment Special Study Area shall be subject to the following:

- a. Prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, that the following matters, but not limited to, shall be addressed through the Block Plan process and shall include:
  - i. Block Plan Report and Block Plan Land Use Schedule;
  - ii. Master Environmental and Servicing Plan;
  - iii. Phase I Environmental Site Assessment, and where required, a Phase II Environmental Site Assessment;
  - iv. Transportation Analysis/Study;
  - v. Travel Demand Management Plan/Study;
  - vi. Traffic/Transit Management and Sidewalk/Walkway Master Plan;
  - vii. Pedestrian and Bicycle Master Plan;
  - viii. Noise and Vibration Impact/Mitigation Report;
  - ix. Woodlot/Tree Inventory and Terrestrial Resources Mitigation/Restoration Report;
  - x. Parks and Open Space Plan;
  - xi. Sustainability Report;
  - xii. Development Phasing/Infrastructure Staging Plan;
  - xiii. Archaeological Assessment/Cultural Heritage Resource Assessment; and
  - xiv. Urban Design Guidelines;
  
- b. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a registered Plan of Subdivision, that the following matters, but not limited to, shall be refined through

the Block Plan process:

- i. the delineation of land uses;
  - ii. the delineation and design of the road network;
  - iii. the limits of development;
  - iv. the proposed realignment of the watercourses;
  - v. the delineation and design of the stormwater management pond facilities;  
and,
  - vi. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans;
- c. The final uses within the Major Mackenzie Drive Alignment Special Study Area, including the delineation of land uses, valley and stream corridors, road network and stormwater management facilities shall coincide with:
- i. the Highway 427 Environmental Assessment (EA) Corridor Study Area by the Ministry of Transportation, which will identify the preferred corridor and terminus for the north expansion of Highway 427;
  - ii. the Western Vaughan Transportation Individual Environmental Assessment (IEA), which will identify the alignment of Major Mackenzie Drive, including the interchange of the future north expansion of Highway 427 and Major Mackenzie Drive, and the preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line;
  - iii. the City-wide Vaughan Transportation Master Plan, and have regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which will explore the overall layout and alignment of the collector and local road network, including road connectivity to the western portion of Vaughan and mid-block collector and community local road connectivity, and the Huntington Road realignment; and,
  - iv. the York Region Transportation Master Plan Update;
- d. Prior to draft approval of a Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not within a registered Plan of Subdivision, that the following matters for the Major Mackenzie Drive Alignment Special Study Area, but not limited to, shall be refined, in consultation with, the

Ministries of Municipal Affairs and Housing and/or Transportation, York Region, and Toronto and Region Conservation Authority, to the satisfaction of the City:

- i. the delineation of land uses;
  - ii. the delineation and design of the road network;
  - iii. the limits of development;
  - iv. the proposed realignment of the watercourses;
  - v. the delineation and design of the stormwater management pond facilities;  
and,
  - vi. the delineation and design of internal and perimeter transit services and related amenities consistent with the current YRT/Viva Service standards and service plans;
- e. The Subdivision Owner(s) shall enter into an agreement, prior to any Draft Plan of Subdivision approval, which shall state that the pre-sales of lots and/or blocks for dwelling units, within the Major Mackenzie Drive Alignment Study Area affected by the Environmental Assessments, will not be permitted for the lands, until:
- i. an Environmental Assessment has identified a preferred alignment for Major Mackenzie Drive, the extension of Highway 427, the associated grade separation of the CP Railway Line, and the preferred location for Huntington Road and the collector roads intersecting with Major Mackenzie Drive; and,
  - ii. York Region will consider a phased or partial release of the lots and/or blocks, when the Environmental Assessment or a Feasibility Study (undertaken by the City of Vaughan and/or the Subdivision Owner(s)) has identified, in consultation with York Region, to the satisfaction of the City, that particular lots and/or blocks are no longer impacted by the proposed alignment and grade separation of Major Mackenzie Drive;
- f. Sustainable community objectives shall be implemented through neighbourhood designs that support cycling and walking within and external to the community, including connectivity to transit facilities; ensure neighbourhood connectivity to the broader community; provide transit opportunities; provide water and energy efficiencies, and energy alternatives; and support green building design and site development, to the satisfaction of the City;

- g. As a condition of development approval, valley and stream corridors determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances; and,
- h. Wooded areas in and adjacent to valley and stream corridors shall be maintained in a natural undisturbed state due to their environmental sensitivity, aesthetics, proper flood and erosion control and other functions, and that the appropriate buffers be provided, in consultation with the Toronto and Region Conservation Authority, to the satisfaction of the City. As a condition of development approval, wooded areas determined to be retained and protected shall be conveyed to the Toronto and Region Conservation Authority or the City, free of all cost and encumbrances.

12.8.5.9. The residential uses within the **Mid-Rise Mixed-Use Area C** and **Mid-Rise Mixed-Use Area C** within the Major Mackenzie Drive Alignment Special Study Area designation shall be serviced by full municipal services including water supply and sanitary sewers, and shall require the formal allocation of water supply and sewage servicing capacity by Council.

12.8.5.10. Development within **Mid-Rise Mixed-Use Area C** and **Mid-Rise Mixed-Use Area C** within the Major Mackenzie Drive Alignment Special Study Area shall be subject to Architectural Control. All development shall proceed in accordance with the Council approved Architectural Design Guidelines.”

## 12.8.6 Parks

12.8.6.1. Preferred locations for Neighbourhood Parks are identified conceptually on Map 12.8.A and shall be defined in more detail at subsequent stages in the planning process based on the criteria herein.

12.8.6.2. Where possible, parks are to be located to provide maximum linkage between the open space valley system and the community. This helps to ensure that the valley system remains visible and accessible to the community as a whole and not isolated through rear lotting of residential development.

- 12.8.6.3. Neighbourhood Parks shall meet the following criteria:
- a. should be a minimum of 0.8 hectares to 2.5 hectares in size;
  - b. should be a shape that supports their intended use (predominantly square or rectangular);
  - c. intended predominantly for the 'close to home' needs of residents, especially children, older adults and for less organized recreational activities;
  - d. to be located within a five minute walking distance;
  - e. tableland required and adjacent to local schools where possible;
  - f. sites should be highly visible and have good street frontage (50% of park perimeter); and,
  - g. where possible and practical, be linked into an overall open space and community greenway system.
- 12.8.6.4. Greenways, as shown on Map 12.8.B, shall include but not be limited to the following characteristics:
- a. provide long linear public spaces which act as linking corridors between parks, open spaces, environmental features and other significant land forms;
  - b. ideally should be a minimum of 25 metres wide however, precise boundaries and configuration shall be established in the Draft Plan, Subdivision or Block Plan;
  - c. shall utilize existing significant tree hedgerows, natural features and land form character;
  - d. incorporate ecosystem based corridor linkages between neighbourhood and district parks, woodlands and other environmentally sensitive areas;
  - e. include public trails/bicycle paths, seating areas, signage and lighting;
  - f. should be integrated with the Inter-regional Trail system to form expansive community open space links; and,
  - g. should be located along the TransCanada PipeLines Limited right-of-way for the Nashville Heights community on Map 12.8.A.
- 12.8.6.5. Lands for park purposes shall be provided at a ratio of 5% of the gross land area or 1 hectare for each 300 dwelling units or a combination of 5% and 1 ha per 300 units, whichever yields the greater provided for in Section 42(1) of the Planning Act. Commercial development shall require conveyance of 2% of the land for parks purposes as provided for in Section 42(1) of the Planning Act.

- 12.8.6.6. Gross land area is defined as the total land area within a plan minus valley lands and/or woodlands. If the valley lands are used for any servicing, a portion of that valley shall be included in the 5% parkland calculation.
- 12.8.6.7. Lands utilized for stormwater management facilities shall generally not be considered as any part of the parkland dedication requirement provided via the Planning Act. Where a quantity stormwater management facility located adjacent to a park is sized and designed to accommodate active recreational facilities, the City may include up to 50% of the land occupied by the recreational facility as part of the required parkland dedication pursuant to the Planning Act. Stormwater management ponds shall be designed in a natural and curvilinear form incorporating appropriate trees and shrubs. To the extent possible, such facilities shall have regard for the pedestrian and bicycle system and greenway corridors and designed in a manner to enhance their aesthetic appeal.
- 12.8.6.8. The City shall require dedication of parkland rather than cash-in-lieu unless it can conclusively be demonstrated that no significant requirement for such parkland will be present for the foreseeable future.

## **12.8.7 General**

- 12.8.7.1. The lands identified as Nashville Heights on Map 12.8.A shall be planned to achieve a minimum density of 50 residents and jobs per hectare.
- 12.8.7.2. Development of the Nashville Heights community shall require a comprehensive review through the Block Plan process, which includes Master Environmental and Servicing Plan (MESP) process, and the undertaking of Environmental Assessments for the Western Vaughan Transportation Individual Environmental Assessment and Highway 427 Environmental Assessment Corridor Study for the lands within the Major Mackenzie Drive Alignment Special Study Area, which may result in the phasing of development. The Block Plan is to be reviewed with regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008. The Block Plan process requires that prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, the following

reports, but are not limited to the following, shall be submitted for the review and approval, in consultation with York Region, the Toronto and Regional Conservation Authority, and any other commenting agency, to the satisfaction of the City. All the reports shall include the lands between the west side of Huntington Road, Nashville Road, Major Mackenzie Drive and the future Highway 427 right-of-way, as well as the proposed site for a community centre/park, which must include a facility fit design to be approved by the City, in their analysis, including land use concept plans.

- a. Block Plan Report, and Block Plan Land Use Schedule with supporting land use detail (land uses by area, densities, housing mix, population, ownership, participating/non-participating owners, etc.);
- b. A Master Environmental and Servicing Plan shall adequately address the following:
  - i. matters including, but not limited to: stormwater management, hydrology, hydrogeology, ecology (natural heritage), geotechnical, sustainable development, green design and green space connections to features within and beyond the Nashville Heights community, baseline conditions, predicted changes, mitigation and any compensation, to be reviewed and approved by the City in consultation with Toronto and Region Conservation Authority (TRCA); and should include how the principles and/or recommendations of, but not limited to the following, Sub-section 2.1.2 of the Provincial Policy Statement (PPS), Humber River Watershed Plan, including the Humber Watershed Terrestrial Natural Heritage System (TNHS) and the City of Vaughan's natural heritage system (that is currently being developed) are addressed in order to guide the development of the Nashville Heights Community. The Terms of Reference for the Master Environmental and Servicing Plan (MESP) shall be reviewed and approved by the City and TRCA prior to the initiation of the MESP;
  - ii. the boundaries of the features, including the requisite buffers, be staked and delineated, in consultation with the TRCA and Owner, to the satisfaction of the City;
  - iii. as a minimum, the valley and stream corridor boundaries be 10 metres inland from the greater of the following: physical top of the valley feature; stable top of bank, where geotechnical concerns exist; limits of flooding on the property in a Regulatory Storm Event; predicted meander belt; and limits of significant vegetation which is contiguous with the valley/stream corridor;

- iv. the valley and stream corridors, and tableland woodlots contiguous with the valley and stream corridors shall be designated in an appropriate open space designation, and shall be conveyed to the City or TRCA;
- v. where a Natural Heritage System has been defined for the area, these lands should also be designated in an appropriate open space designation, and be conveyed to the City or TRCA;
- vi. The Humber River Watershed Plan (June 2008) results indicated the potential for significant changes to flood flows and potential risks to downstream areas, therefore, an updated hydrologic study is required to evaluate the effects on flooding of proposed new developments and to confirm the level of stormwater control needed before permitting urban type land uses beyond those identified in existing official plans. The hydrologic study will be completed on a watershed scale and will be directed by the TRCA;
- vii. As the hydrologic study for the watershed will not be completed until 2011 or 2012, in order to allow the development to proceed in the Nashville Heights Community, the MESP should consider a reasonable conservative outcome of the hydrologic study which may require new development to detain and over-control the entire runoff volume from the Regional Storm event. As an approach to resolve the stormwater management criteria on an interim basis, the MESP should identify methods and designate areas of sufficient size to contain stormwater management facilities of a size and volume to perform this function, with the understanding that smaller facilities may be required once the hydrologic study is complete. The sizing of temporary stormwater management facilities in the MESP shall be consistent with current standards and the approval of other stormwater management facilities in OPA #601, and should be supported with analysis and calculations conducted that demonstrate that over-control of the Regional Storm event volume can be achieved, to the satisfaction of the City in consultation with the TRCA;
- viii. As the final alignments of Highway 27 and Major Mackenzie Drive have yet to be determined and have been protected through the identification of the Major Mackenzie Drive Special Study Area, except for temporary stormwater management ponds and public infrastructure and uses, no development shall be permitted in this area until such time as the alignments of Highway 427 and Major Mackenzie Drive are determined and it has

been demonstrated that additional lands within the area are not required for stormwater management control. Notwithstanding this approach, the MESP shall consider the implementation of stormwater management techniques throughout the entire Block Plan that contribute to achieving this overall goal;

- ix. Except for the creation of lots, blocks or other parcels that are to be used for stormwater management and other public uses, the hydrologic study for the watershed must be completed prior to the registration of any draft plan of subdivision or site development application for the lands as shown on Map 12.8.A, and identified as the Major Mackenzie Drive Alignment Special Study Area. All draft plans of subdivision or site plans shall be redline revised, if needed, to reflect the conclusions of the hydrologic study with accompanying reports to be submitted to the satisfaction of the TRCA;
- c. The Phase I Environmental Site Assessment, and where required, the Phase II Environmental Site Assessment shall be in accordance with the following:
  - i. Prior to final approval of any Draft Plan of Subdivision or Site Plan, and prior to initiation of grading or topsoil stripping, the Owner shall submit the Environmental Site Assessment (ESA) Phase I Report, and if required, Phase II ESA Report and the Remedial Action Plan for the lands within the Draft Plan of Subdivision or Site Plan, in accordance with the Ontario Regulation 153/04, "Soil, Ground Water and Sediment Standards" for Use Under Part XV.1 of the Environmental Protection Act. In addition, for park blocks and open space blocks, a Phase II Environmental Site Assessment (ESA) report shall be carried in accordance with the "Guideline Phase II Environmental Assessment, Proposed Parkland, City of Vaughan" and submitted to the City for review and approval. Documented proof of the satisfactory registration of the Record of Site Condition (RSC) with the Environmental Site Registry (ESR) of the Ministry of Environment (MOE), which includes the acknowledgement from MOE and a signed copy of the RSC by a Qualified Person, has to be submitted to the City for review and approval;
  - d. Environmental Impact Statement;
  - e. Transportation Analysis/Study and Travel Demand Management Plan/Study shall adequately address, in consultation with, the Ministries of Municipal Affairs and Housing and/or Transportation and the Region of York, to the satisfaction the City, the following details for:

- i. the proposed **Mid-Rise Residential, Mid-Rise Mixed-Use Area A, Area B, and Area C** at the north and south sides of the intersection of the proposed realigned Major Mackenzie Drive and Huntington Road intersection;
- ii. the lands for the community centre/parks with respect to the future Highway 427 extension;
- iii. the potential realignment of Huntington Road and Nashville Road;
- iv. the potential east-west mid-block road connections extending westerly to Highway 50 be explored to establish a local and regional road network connectivity;
- v. the potential mid-block crossing of the CP Railway be explored for the purpose of a pedestrian/cycling connections between the Nashville Heights and Humber Trails Neighbourhoods;
- vi. the right-of-way widths/dimensions and standards;
- vii. the realignment of Huntington Road, located north of Major Mackenzie Drive, east of the existing Huntington Road, will be connected in a continuous manner to the existing Huntington Road. The new realignment of Huntington Road shall be up to a 36 metre right-of-way (ROW); and,
- viii. The section of Huntington Road, beyond the new realignment shall be protected for up to a 36 metre ROW. This ROW width and any potential realignment options beyond the boundary of the Kleinburg-Nashville Community Plan area, shall be determined through the environmental assessment process or a subsequent transportation study as required through the Block Plan process, to the satisfaction of the City and Region;
- f. Traffic/Transit Management and Sidewalk/Walkway Master Plan;
- g. Pedestrian and Bicycle Master Plan;
- h. Noise and Vibration Impact/Mitigation Report;
- i. Woodlot/Tree Inventory and Terrestrial Resources Mitigation/Restoration Report;
- j. Parks and Open Space Plan;
- k. Development Phasing/Infrastructure Staging Plan;
- l. Sustainability Report;
- m. Archaeological Assessment/Cultural Heritage Resource Impact Assessment, in accordance with the City of Vaughan's Strategy for the Maintenance and Preservation of Significant Heritage Buildings, approved by Council in June 2005, shall:
  - i. undertake an assessment of all properties of cultural heritage value or

interest within Nashville Heights, which is to be provided by to the satisfaction of the Recreation and Culture Department - Cultural Services Section. Such properties would include, but not necessarily be limited to, 10,671 Huntington Road, Nashville Cemetery and 10,395 Huntington Road; and,

- ii. include in the Cultural Heritage Resource Impact Assessment that as a condition of approval for any Draft Plan of Subdivision or Site Development Application, where the lands were not in a Plan of Subdivision, involving the Nashville Heights lands that the development of new lots under the Plan of Subdivision process will require the Owner to preserve, restore and incorporate significant heritage structures in their plan, and letters of credit be posted in the amount of the cost equal to the re-construction of the heritage structure where a significant heritage building is destroyed or demolished without City approval, or repair the heritage building to preserve the heritage structure;
- n. Urban Design Guidelines which are to include specific transition related guidelines to address:
  - i. the relationship of low-rise residential areas (i.e., detached and semi-detached dwelling units) with mid-rise residential and mid-rise mixed-use areas; and,
  - ii. the automobile service station or gas bar, and carwash uses' transition and buffer to residential uses; and,
- o. Architectural Design Guidelines.

12.8.7.3. Prior to the approval of any Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, for the Nashville Heights lands, and prior to initiation of grading or topsoil stripping, the Owner shall submit the Environmental Site Assessment (ESA) Phase I Report, and if required, Phase II ESA Report and the Remedial Action Plan for the lands within the Draft Plan of Subdivision or Site Plan, in accordance with the Ontario Regulation 153/04, "Soil, Ground Water and Sediment Standards" for Use Under Part XV.1 of the Environmental Protection Act. In addition, for park blocks and open space blocks, a Phase II Environmental Site Assessment (ESA) report shall be carried in accordance with the "Guideline Phase II Environmental Assessment, Proposed Parkland, City of Vaughan" and submitted to the City for review and approval. Documented proof of the satisfactory registration of the Record of Site Condition (RSC) with the Environmental

Site Registry (ESR) of the Ministry of Environment (MOE), which includes the acknowledgement from MOE and a signed copy of the RSC by a Qualified Person, has to be submitted to the City for review and approval.

- 12.8.7.4. All development within Nashville Heights shall be on full municipal water supply and sanitary sewers. Water supply and sewage servicing capacity has not been allocated beyond the current approved population within the Kleinburg-Nashville Community Plan. Formal allocation of water supply and sewage servicing capacity will be required by Council.
- 12.8.7.5. The City is currently undertaking the Kleinburg-Nashville Local Water and Wastewater Servicing Strategy Master Plan Class Environmental Assessment (EA) Study for the Kleinburg-Nashville Community to identify servicing infrastructure improvements in order to accommodate the planned and proposed growth in the Kleinburg-Nashville Community. The City is also undertaking the Storm Drainage and Stormwater Management Master Plan Study, in support of the on-going City-wide Official Plan Review, which is to recommend the final stormwater management scheme for the City, including the Nashville Heights lands. Prior to draft approval of a Draft Plan of Subdivision Application or approval of any Site Development, where the lands were not in a registered Plan of Subdivision Application, should the Nashville Heights Community be serviced prior to the completion of Kleinburg-Nashville Water and Wastewater Servicing Strategy Master Plan Class Environmental Assessment (EA) Study for the Kleinburg-Nashville Service Area, the following, which should be reflected in the Master Environmental and Servicing Plan, are required:
- a. A Comprehensive Sanitary Sewer System Capacity Analysis (addressing infrastructure improvements external to the Nashville Heights Block 61 West Area) be prepared to address the availability of both conveyance and treatment capacity at the Block Plan stage should the servicing of this area proceed prior to the completion of the Kleinburg-Nashville Water and Wastewater Servicing Strategy Master Plan Class Environmental Assessment (EA) Study for the Kleinburg-Nashville Service Area, and the final sanitary servicing scheme for the Nashville Heights Block 61 West Area shall conform to the recommended preferred servicing scenario as identified in the City's Final Master Plan Class Environmental Assessment Report;
  - b. A Comprehensive Water Supply System Analysis (addressing infrastructure

improvements external to the Nashville Heights Block 61 West Area) be prepared at the Block Plan stage should the servicing of this area proceed prior to the completion of the Kleinburg-Nashville Water and Wastewater Servicing Strategy Master Plan Class Environmental Assessment (EA) Study for the Kleinburg-Nashville Service Area, and the final water supply servicing scheme for the Nashville Heights Block 61 West Area shall conform to the recommended preferred servicing scenario as identified in the City's Final Master Plan Class Environmental Assessment Report; and,

- c. A final stormwater management scheme for the Nashville Heights Block 61 West Area shall conform to recommended preferred stormwater management scenario as identified in the City's Storm Drainage and Stormwater Management Master Plan.

12.8.7.6. The southern portion of the Nashville Heights community has been designated "Major Mackenzie Drive Alignment Special Study Area" which includes the **Mid-Rise Mixed-Use**, and **Natural Areas** and identifies the Stormwater Management Pond Facility as shown on Map 12.8.A. These lands are in a "Special Study Area" in order for the land uses in this area to be reviewed further by the Province, Region, Toronto and Region Conservation Authority, and City, once the transportation issues have been finalized. The final uses within the Major Mackenzie Drive Alignment Special Study Area, including the delineation of land uses, valley and stream corridors, road network and stormwater management facilities, for the lands within the Major Mackenzie Drive Alignment Special Study Area coincide with the following and prior to draft approval of a Draft Plan of Subdivision Application or Site Development Application, where the lands were not in a registered Plan of Subdivision, and shall be refined in the Block Plan process and Plan of Subdivision process:

- a. the Highway 427 Environmental Assessment (EA) Corridor Study Area by the Ministry of Transportation, which will identify the preferred corridor and terminus for the north expansion of Highway 427;
- b. the Western Vaughan Transportation Individual Environmental Assessment (IEA), which will identify the alignment of Major Mackenzie Drive, including the interchange of the future north expansion of Highway 427 and Major Mackenzie Drive, and the preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line;
- c. the City-wide Vaughan Transportation Master Plan, and have regard for the

components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which will explore the overall layout and alignment of the collector and local road network, including road connectivity to the western portion of Vaughan and mid-block collector and community local road connectivity, and the Huntington Road realignment; and,

- d. the York Region Transportation Master Plan Update.

- 12.8.7.7. The Owner of lands with in the Major Mackenzie Drive Alignment Special Study, shown on Map 12.8.A shall enter into an agreement with the City of Vaughan, prior to Draft Plan of Subdivision approval, which shall be registered on title and apply to the applicable lots and/or blocks, committing the Owner not to enter into any agreements of purchase and sale with end-users for the subject lands until such time as:
  - a. an Environmental Assessment has identified a preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line, in conjunction with York Region, to the satisfaction of the City of Vaughan, and the preferred location for Huntington Road and the collector roads intersecting with Major Mackenzie Drive; and,
  - b. York Region will consider a phased or partial release of the lots and blocks identified above, when the Environmental Assessment or a Feasibility Study (undertaken by the City and/or the Owner) has identified, to the satisfaction of York Region, that particular lots and blocks are no longer impacted by the proposed alignment and grade separation of Major Mackenzie Drive.
  
- 12.8.7.8. Prior to final approval of the any Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, the Owner shall carryout a feasibility study for a pedestrian crossing through the CP Railway right-of-way between the Nashville Heights and Humber Trails Neighbourhoods to the satisfaction of the City. The Owner shall agree in the subdivision agreement to carry out the recommendations of the approved feasibility study to the satisfaction of the City. The Owner shall also include a warning statement for all prospective buyers in the Plan detailing the potential construction of a pedestrian crossing through the CP Railway right-of-way within the Draft Plan of Subdivision, to the satisfaction of the City.

## **12.8.8 Sustainability**

- 12.8.8.1. Sustainable community objectives shall be implemented through neighbourhood designs that support cycling and walking within and external to the community, including connectivity to transit facilities; ensure neighbourhood connectivity to the broader community; provide transit opportunities; provide water and energy efficiencies, and energy alternatives; and support green building design and site development, to the satisfaction of the City.
- 12.8.8.2. The location and widths of all valley and stream corridors, as shown on Map 12.8.A for Nashville Heights, is approximate and may change without requiring an amendment to this Plan. The final location and widths of the valley and stream corridors will be determined during the Block Plan approval process through the preparation of a Master Environmental and Servicing Plan (MESP). The MESP will be prepared in conjunction with the Toronto Region Conservation Authority and any other commenting agencies, to the satisfaction of the City. Adjustments to land use designations shall be permitted without amendment to this Plan in order to accommodate the final alignment and widths of the valley and stream corridors.
- 12.8.8.3. The valley and stream corridors must be studied from both an ecological and hydrological perspective, including connectivity to recharge areas, to determine their function and the implications of potential modifications.
- 12.8.8.4. If a realignment is required, it must incorporate an appropriate corridor width that includes all relevant factors including, but not limited to, flooding, meander belt, erosion limits and buffers. The design of the new valley and stream corridor(s) must be appropriate for the biophysical conditions and should replicate and enhance the existing functions. The realignment must be in balance with the overall development plan and fit into an appropriate post development natural system, and must be integrated into the overall open space system through neighbourhood design.”

## **12.8.9 Transportation**

- 12.8.9.1. The transportation network is illustrated on Map 12.8.B.
- 12.8.9.2. Primary roads shall have a 23 metre right-of-way. Local roads shall generally have an 18.5 metre to 20 metre right-of-way.

- 12.8.9.3. The number of east-west and north-south primary and collector roads, and the right-of-way dimensions and geometric standards for the primary and collector roads shall be established in consideration with the conclusions and recommendations of the City-wide Transportation Master Plan and Block Transportation Studies and have regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which shall be finalized at the Block Plan stage for the Nashville Heights community.
- 12.8.9.4. The number of east-west and north-south local roads, and the right-of-way dimensions and geometric standards for the local roads shall be established in consideration with the conclusions and recommendations of the City-wide Transportation Master Plan and Block Transportation Studies and have regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which shall be finalized at the Block Plan stage for the Nashville Heights community.

#### **12.8.10 CP Intermodal Yard and Railway Line**

Any development adjacent to the CP Railway right-of-way shall comply with environmental and safety standards addressing rail noise, vibration and safety issues. The following policies shall apply:

- 12.8.10.1. All proposed development within 2000 metres of the CP Intermodal Yard and / or 300 metre of a railway right-of-way shall be required to undertake noise studies, to the satisfaction of the City in consultation with the Ministry of the Environment and CP Rail and shall undertake appropriate measures to mitigate any adverse effects from noise that were identified.
- 12.8.10.2. All proposed development within 75 metres of a railway right-of-way may be required to undertake vibration studies, to the satisfaction of the City in consultation with the MOE and CP Rail and shall undertake appropriate measures to mitigate and adverse effects from vibration that were identified.
- 12.8.10.3. All proposed development adjacent to railways shall ensure that appropriate safety measures such as setbacks, berms and security fencing are provided, to the satisfaction of the City in consultation with the appropriate railway.

- 12.8.10.4. Generally the preferred form of noise and vibration mitigation is to increase distance from the rail right-of-way through berming and noise attenuation fence and the establishment of a parallel service road.
- 12.8.10.5. Through an Environmental Assessment process, a future Go Station may be established in the vicinity of the intersection of Major Mackenzie Drive and the CP Railway. An Urban Design Plan may be prepared for the lands in proximity to a future GO Station, which demonstrates compatibility with the surrounding community, and achieves the sustainability community objectives with an overall design and density supportive of a future GO Station.
- 12.8.10.6. Prior to final approval of the any Draft Plan of Subdivision Application or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, the Owner shall carryout a feasibility study for a pedestrian crossing through the CP Railway Line right-of-way between the Nashville Heights and Humber Trails Neighbourhoods to the satisfaction of the City. The Owner shall agree in the subdivision agreement to carry out the recommendations of the approved feasibility study to the satisfaction of the City. The Owner shall include a warning statement in all Offers of Purchase and Sale, or Lease for all lots/blocks for all prospective buyers detailing the potential construction of a pedestrian crossing through the CP Railway right-of-way within the Draft Plan of Subdivision Application or Site Development Application proposal, to the satisfaction of the City.
- 12.8.10.7. Should the feasibility study determine that a pedestrian crossing may be provided, the location and type of linkage (e.g., overpass or underpass) that addresses, but not limited to barrier-free requirements and height clearances for potential electrification of the railway corridor are to be explored by the Subdivision Owners(s), in consultation with the Ministry of the Environment and CP Railway, to the satisfaction of the City.
- 12.8.10.8. A warning clause shall be included in all Offers of Purchase and Sale, or Lease for all lots/blocks for all prospective buyers for any Draft Plan of Subdivision Application or Site Development Application, stating that that a potential future GO Train Commuter Station and Local Commuter Bus Station, and associated infrastructure may be located on properties at the south-east intersection of Major Mackenzie Drive and the CP Railway, in of Part of Lot 20, Concession 9, City of Vaughan, to the satisfaction of the City.

- 12.8.10.9. A warning clause shall be included in all Offers of Purchase and Sale, or Lease, and registered on title for all lots/blocks for all prospective buyers for any Draft Plan of Subdivision Application or Site Development Application, stating that a potential Provincial controlled access highway maybe located on lands west of Huntington Road, north of Major Mackenzie Drive.

### **12.8.11 Servicing**

- 12.8.11.1. As the City is currently undertaking the Local Water and Wastewater Servicing Strategy Master Plan Class Environmental Assessment (EA) Study for the Kleinburg-Nashville Community to identify servicing infrastructure improvements (i.e., water supply and sanitary sewer services), including the land for an elevated water tank, required to support the build out of the planned and proposed developments in the Kleinburg-Nashville Community, including the Humber Trails and Nashville Heights Neighbourhood, the lands shall be zoned with the Holding Symbol “(H)”. Prior to the removal of the Holding Symbol “(H)” from any Residential Zone for any lot and/or block, the water supply and sewage servicing capacity shall be identified and allocated by the Council of the City of Vaughan.

### **12.8.12 TransCanada PipeLine**

A high pressure natural gas pipeline within its right-of-way crosses the Community Plan area as indicated on Map 12.8.A to this Plan. Any development within 200 metres of this facility may affect its safety and integrity.

- 12.8.12.1. Any excavation, blasting or movement of heavy equipment within 30 metres of the right-of-way shall receive prior approval by TransCanada Pipeline.
- 12.8.12.2. TransCanada Pipeline shall be circulated on any development application within 200 metres of its right-of-way.
- 12.8.12.3. All development shall be setback a minimum of 10 metres from the limits of the right-of-way for all permanent structures and excavations.
- 12.8.12.4. The City shall consider, through the Urban Design Plan the incorporation of the TransCanada Pipeline easement into an environmental or pedestrian open space linkage.

- 12.8.12.5. The Owners of the Draft Plan of Subdivision and/or Site Development Applications shall enter into an agreement with TransCanada PipeLines Limited for the purpose of recoating the gas pipeline at road crossings at the Owner's/Developer's expense, and providing concrete slabs over the TransCanada PipeLines Limited and Enbridge pipelines for the length of the right-of-way that is located in the boulevard or under the road at the Owner's/Developer's expense.
- 12.8.12.6. The number of crossings; the signage on the right-of-way in a number, location and form; the types of trees and form of landscaping that can be planted on the right-of-way, and their depth and location; the number of utility crossings and their location; the notification of future purchasers of the existence of high pressure pipelines and appropriate ingress and egress over properties; and the provision of fencing or other means of identifying the limit of the right-of-way; are to be in consultation with TransCanada PipeLines Limited, to the satisfaction of the City.
- 12.8.12.7. The design, costs, agreements and any other item required to satisfy the provisions in policy 12.8.12.6, will be finalized by agreement between the Owner(s) and TransCanada PipeLines Limited or its successors to be negotiated during the Subdivision approval process. Adjustments to the provisions in policy 12.8.12.6 with respect to the requirements and responsibilities associated with the protection and enhancement of the TransCanada pipeline corridor may occur without requiring an amendment to this Plan provided that they are agreed to by TransCanada PipeLines Limited.

### **12.8.13 Holding Symbol “(H)” Environmental Assessments/Transportation Master Plans.**

- 12.8.13.1. The implementing zoning by-law(s) for the Major Mackenzie Drive Alignment Special Study Area, as shown on Map 12.8.A, may zone any lot and/or block within the Major Mackenzie Drive Alignment Special Study Area with the Holding Symbol “(H)”. Prior to the removal of the Holding Symbol “(H)”:
- a. the Highway 427 Environmental Assessment (EA) Corridor Study Area by the Ministry of Transportation will identify the preferred corridor and terminus for the north expansion of Highway 427 for the lands within the Major Mackenzie Drive Alignment Special Policy Area;
  - b. the Western Vaughan Transportation Individual Environmental Assessment (IEA), will identify the alignment of Major Mackenzie Drive, including the interchange

of the future north expansion of Highway 427 and Major Mackenzie Drive, and the preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line for the lands within the Major Mackenzie Drive Alignment Special Policy Area;

- c. the City-wide Vaughan Transportation Master Plan, will have regard for the components, policies and issues identified in the Kleinburg Focused Area Study report and Terms of Reference approved by Council on March 31, 2008, which will explore the overall layout and alignment of the collector and local road network, including road connectivity to the western portion of Vaughan and mid-block collector and community local road connectivity, and the Huntington Road realignment;
- d. the York Region Transportation Master Plan Update, will identify Regional road network requirements; and,
- e. the determination by an Environmental Assessment of a preferred alignment for Major Mackenzie Drive and the associated grade separation of the CP Railway Line, in consultation with CP Railway, the Region and any other commenting agencies, to the satisfaction of the City of Vaughan, and a preferred alignment for Huntington Road, and York Region will consider a phased or partial release of the lots and/or blocks, when the Environmental Assessment or a Feasibility Study (undertaken by the City of Vaughan and/or the Subdivision Owner) is identified, in consultation with York Region, to the satisfaction of the City, that particular lots and/or blocks are no longer impacted by the proposed alignment and grade separation of Major Mackenzie Drive.

#### **12.8.14 Holding Symbol “(H)” - Servicing Allocation**

- 12.8.14.1. The implementing zoning by-law(s) for Nashville Heights as shown on Schedule “A” may zone any lot and/or block with the Holding Symbol “(H)”. Prior to the removal of the Holding Symbol “(H)” from any lot and/or block within Nashville Heights:
  - a. the water supply and sewage servicing capacity shall be identified and allocated by the Council of the City of Vaughan.

### **12.8.15 Approval of Phase 1 Environmental Site Assessment, and where required, a Phase II Environmental Site Assessment.**

- 12.8.15.1. Prior to the approval of any Draft Plan of Subdivision or approval of any Site Development Application, where the lands were not in a registered Plan of Subdivision, for the Nashville Heights lands, and prior to initiation of grading or topsoil stripping, the Owner shall submit the Environmental Site Assessment (ESA) Phase I Report, and if required, Phase II ESA Report and the Remedial Action Plan for the lands within the Draft Plan of Subdivision or Site Plan, in accordance with the Ontario Regulation 153/04, "Soil, Ground Water and Sediment Standards" for Use Under Part XV.1 of the Environmental Protection Act. In addition, for park blocks and open space blocks, a Phase II Environmental Site Assessment (ESA) report shall be carried in accordance with the "Guideline Phase II Environmental Assessment, Proposed Parkland, City of Vaughan" and submitted to the City for review and approval. Documented proof of the satisfactory registration of the Record of Site Condition (RSC) with the Environmental Site Registry (ESR) of the Ministry of Environment (MOE), which includes the acknowledgement from MOE and a signed copy of the RSC by a Qualified Person, has to be submitted to the City for review and approval.

### **12.8.16 Approval of Zoning By-law**

- 12.8.16.1. Applicable development standards shall be established in the implementing zoning by-law.

### **12.8.17 Region of York**

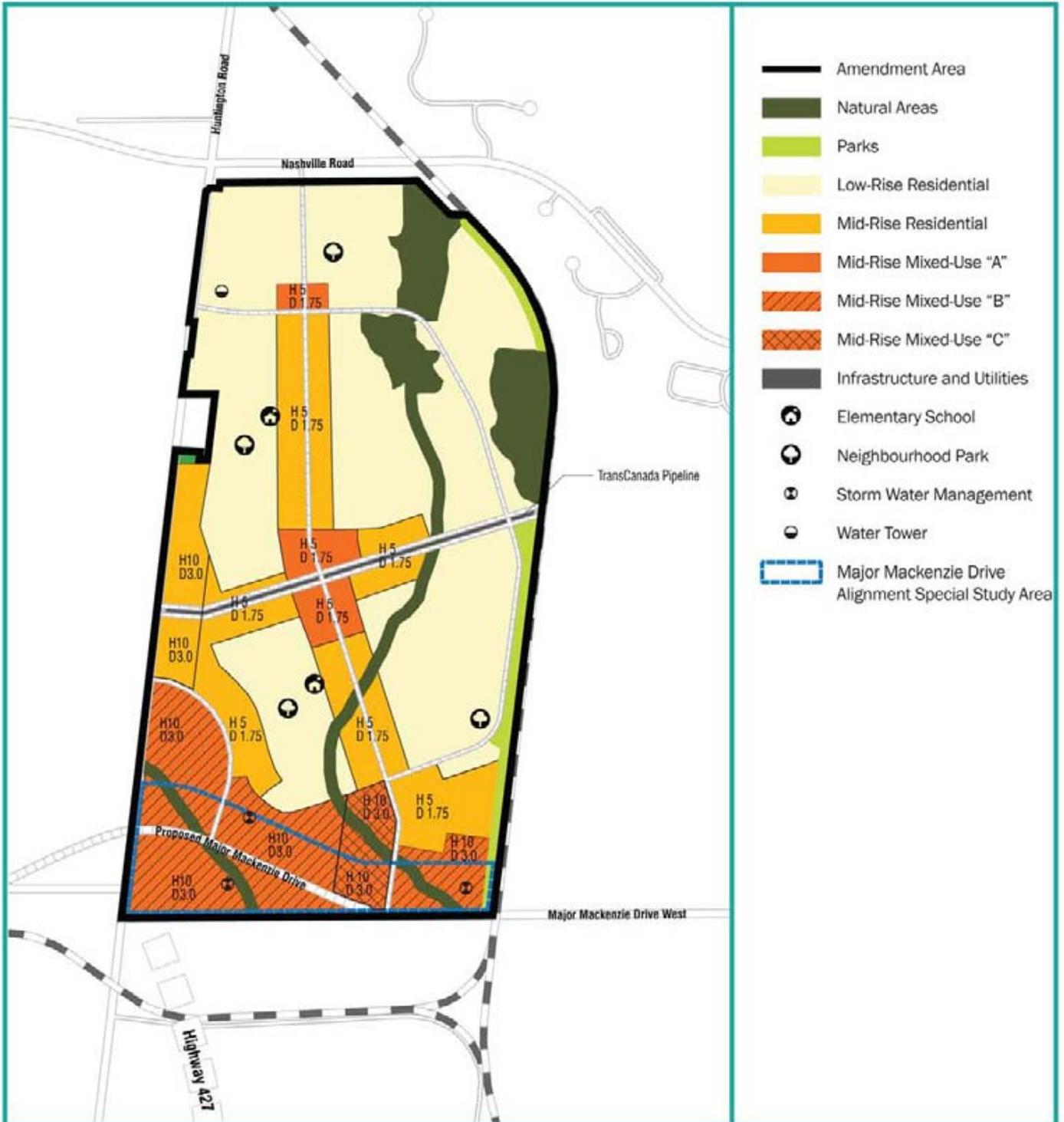
- 12.8.17.1. All requirements of the Regional Municipality of York (York Region) shall be satisfied.

### **12.8.18 Interpretation**

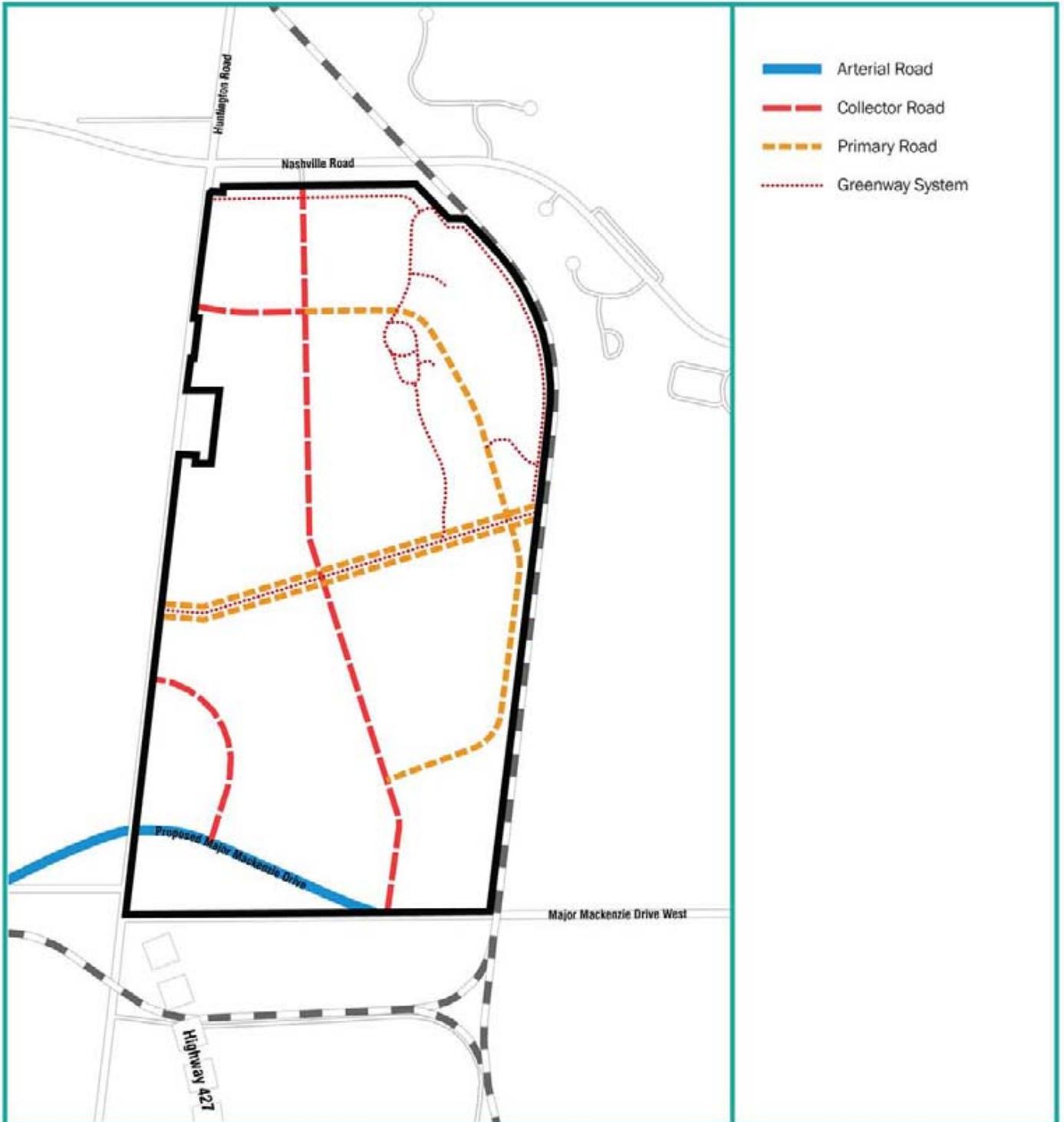
- 12.8.18.1. The locations of and boundaries between the land use designations shown on the Schedules to this Plan are approximate, except where they coincide with arterial roads, railway lines, or other clearly defined physical features. Minor adjustments will not require an amendment to this Plan as long as the intent of its policies is maintained.
- 12.8.18.2. The location of the by-pass, arterial collector and primary road system is approximate, and minor adjustments will not require an amendment to this Plan provided that the intent of its policies is maintained.

- 12.8.18.3. All statistics, with the exception of density requirements may vary within 5% as long as the intent of the policies of this Plan is maintained.
- 12.8.18.4. The text and maps and Table A contained herein constitute the Kleinburg-Nashville Community Plan. Photographs, drawings and graphics contained in the document are intended to be illustrative and are provided for information only.
- 12.8.18.5. Notwithstanding policies 12.8.18.1 and 12.8.18.2, adjustments to the delineation of the land uses, road network, valley and stream corridor, and stormwater management facilities for the Major Mackenzie Drive Alignment Special Study Area as a result of determining the final alignment of Major Mackenzie Drive through the Highway 427 Environmental Assessment (EA) Corridor Study Area, Western Vaughan Transportation Individual Environmental Assessment (IEA), City-wide Vaughan Transportation Master Plan and York Region Transportation Master Plan Environmental Assessment Update, will not require an amendment to this Plan provided the intent of the policies are maintained.
- 12.8.18.6. Notwithstanding policies 12.8.18.1 and 12.8.18.2, the following shall apply:
- a. the grade separation between the road and rail systems may be provided as needed at arterial road/railway intersections, without an amendment to this Plan;
  - b. the grade separation between the road and controlled access highway may be provided as needed at arterial road/highway, collector road/highway or primary road/highway intersections, without an amendment to this Plan; or,
  - c. the grade separation between the off-street pedestrian system and the road system and the CP Railway Line may be provided, without an amendment to this Plan.
- 12.8.18.7. The location and widths of all valley and stream corridors, as shown on Map 12.8.A for Nashville Heights, is approximate and may change without requiring an amendment to this Plan.

▼ Map 12.8.A: Block 61 West - Nashville Heights - Land Use



▼ Map 12.8.B: Block 61 West - Nashville Heights - Transportation Network



## 12.9 Yonge Street Corridor in Thornhill

### 12.9.1 Thornhill Vision and Principles

12.9.1.1. The Thornhill-Yonge Corridor, shown on Map 12.9.A, is expected to become a vibrant, mixed-use area. It will be developed as a higher order transit corridor and is an appropriate location for the promotion of intensification in order to bring vitality to the area, to promote residential, commercial and employment growth and to support transit use. The Vision for the Thornhill-Yonge Street Corridor is characterized by:

- a. High quality, beautiful, inspiring and memorable places;
- b. A vibrant and mixed-use main street;
- c. A predominance of commercial / retail uses along Yonge Street;
- d. An attractive, high quality, pedestrian friendly, transit supportive streetscape;
- e. Differing scales of development including transit-supportive mid-rise intensification and small scale infill projects to complement existing developments and heritage assets;
- f. Protection for, and enhancement of heritage resources and their environs;
- g. New public open spaces / plazas and enhancements and connections to the total open space system;
- h. Building height and massing transition to ensure compatibility with neighbourhood uses and heritage assets; and,
- i. Organized access and parking to the rear of commercial and mixed-use properties.

12.9.1.2. To achieve this vision, there are a number of key principles that should be considered in reviewing all public and private sector development decisions, as follows:

- a. Transit is an important catalyst for positive change, renewal and reinvestment along the Yonge Street Corridor;
- b. Create a true mixed-use, vibrant main street by encouraging people to live in proximity to the Yonge Street Corridor;
- c. Create a pedestrian friendly, high quality and distinctive streetscape on Yonge Street;
- d. Utilize every development opportunity to repair gaps in the urban fabric and enhance the urban character of the street;
- e. Create new public open spaces / plazas on the side streets as important places for meeting and gathering;
- f. Safeguard, expand and diversify the commercial retail sector;
- g. Promote a connected open space system;

- h. Protect and strengthen core assets;
- i. Promote a high quality of design excellence; and,
- j. Organize shared parking and promote the development of a connected system of rear laneways to free up Yonge Street for people, parks, buildings and activity.

## 12.9.2 Land Use Policies: Low-Rise Mixed-Use

12.9.2.1. Notwithstanding policy 9.2.1.1, the following policies shall apply to the area identified as **Low-Rise Mixed-Use** on Map 12.9.A.

12.9.2.2. Within the area identified as **Low-Rise Mixed-Use** on Map 12.9.A, mixed-use retail/residential or retail/office development is encouraged. New development with frontage on Yonge Street and/or along any plaza/public open space frontage is required to include ground floor, grade-related commercial uses. Residential uses at grade are not permitted along these frontages.

12.9.2.3. Ground floor, grade-related commercial uses are permitted on sites with frontage on Secondary and/or Local Streets. Residential uses are also permitted at grade along these frontages.

12.9.2.4. The following uses are permitted within the **Low-Rise Mixed-Use** area:

- a. Residential units;
- b. Retail uses;
- c. Service uses;
- d. Offices;
- e. Banks and financial institutions;
- f. Health and fitness uses;
- g. Institutional facilities including community facilities and government services;
- h. Day care centres;
- i. Commercial schools; and,
- j. Restaurants.

12.9.2.5. Development permitted at grade along the frontages of Yonge Street, and/or the frontages of any plaza/public open space, uses shall be limited to:

- a. Retail uses;
- b. Restaurants;

- c. Service uses;
  - d. Offices;
  - e. Banks and financial institutions;
  - f. Sports, health and fitness recreational uses; and,
  - g. Institutional facilities including community facilities and government services.
- 12.9.2.6. The following uses are not permitted:
- a. Retail stores and restaurants requiring individual premises in excess of 1,000 square metres;
  - b. Free-standing, single storey buildings containing retail uses, restaurants, bank and financial institutions, health and fitness uses, or commercial schools;
  - c. Drive-through facilities of any kind;
  - d. Gas bars/service stations, either stand alone or associated with any permitted use; and,
  - e. Any use that requires outdoor storage, excluding small scale sidewalk display areas for retail uses
- 12.9.2.7. The **Low-Rise Mixed-Use** designation applies to properties fronting Yonge Street within the boundaries of the Thornhill Heritage Conservation District, but which do not contain designated or listed heritage buildings. Within this designation, new low-rise, mixed-use development is encouraged.
- 12.9.2.8. The following building types are permitted in the **Low-Rise Mixed-Use** area, pursuant to Section 9.2.3:
- a. Townhouses;
  - b. Stacked Townhouses;
  - c. Low-Rise buildings; and,
  - d. Public and Private Institutional buildings.
- 12.9.2.9. Notwithstanding policy 9.2.1.5, the maximum building height within the **Low-Rise Mixed-Use** designation shall be 5 storeys or 17.0 metres, subject to the following additional policies:
- a. In all new development, the ground floor height shall be a minimum of 4.0 metres;
  - b. The maximum permitted height shall only be permitted adjacent to Yonge Street. Away from the Yonge Street frontage, building heights shall transition from the

maximum height to the heights of the surrounding residential fabric, in accordance with sub-policy d. below;

- c. On sites that abut or include a listed or designated heritage building, the height of new buildings adjacent to the heritage building transition down to one storey above the height of the heritage building; and,
- d. Appropriate building heights and height transition regulations will be established by the City through the rezoning process, in conformity with the policies of this Site Specific Policy and consistent with the Council endorsed Thornhill Yonge Street Study (2005).

12.9.2.10. Notwithstanding policy 12.9.2.9, within the **Low-Rise Mixed-Use** designation, a minimum height of 2.5 storeys, or 8.0 metres is required for all new development;

12.9.2.11. Notwithstanding policy 9.2.1.6, the maximum net density shall be in the range of 1.5 or 2.0 Floor Space Index (F.S.I.)

12.9.2.12. Notwithstanding the forgoing for the lands identified on Map 12.9.A as the Thornhill Public School the uses shall be limited to the following institutional uses:

- a. Elementary public and separate schools;
- b. Secondary schools;
- c. Private schools;
- d. Religious institutions;
- e. Day-care centres;
- f. Community centres;
- g. Voluntary associations;
- h. Branch colleges;
- i. Libraries;
- j. Police stations; and,
- k. Fire halls.

### 12.9.3 Heritage Main Street Area

12.9.3.1. Notwithstanding the policies of Section 12.9.2 for the underlying **Low-Rise Mixed-Use** designation, certain lands in the Secondary Plan area have been identified as a Heritage Main Street Area and are subject to the following policies.

- 12.9.3.2. The primary intent of the Heritage Main Street area, shown on Map 12.9.A, is the protection and adaptive re-use of existing heritage buildings.

The permitted uses on lands within the Heritage Main Street area are limited to:

- a. Retail uses;
  - b. Service uses;
  - c. Offices;
  - d. Banks and financial institutions;
  - e. Hotels and bed and breakfast inns;
  - f. Institutional uses, including community facilities and government services compatible with and complementary to the planned function of the designation;
  - g. Full service restaurants; and,
  - h. Residential uses on upper floors only, subject to ensuring that the planned function of the lands is maintained, and that the location is appropriate.
- 12.9.3.3. Drive-through uses of all kinds shall not be permitted within the 'Heritage Main Street' designation.
- 12.9.3.4. Notwithstanding policy 12.9.2.8 the following building types are permitted in the Heritage Main Street area, pursuant to Section 9.2.3:
- a. Low-Rise buildings; and,
  - b. Public and Private Institutional buildings.
- 12.9.3.5. Notwithstanding policy 12.9.2.9, within the Heritage Main Street designation, the maximum height of any building shall be 3 storeys, or 9.0 metres, subject to compatibility with existing heritage structures.
- 12.9.3.6. Notwithstanding policy 12.9.2.10, a minimum height of 2.5 storeys or 8.0 metres is required for all new development.
- 12.9.3.7. Notwithstanding policy 12.9.2.11, the maximum site density shall be 0.75 FSI.

## **12.9.4 Land Use Policies: Low-Rise Residential**

- 12.9.4.1. Notwithstanding policies 9.2.1.1, the following policies apply to lands designated **Low-Rise Residential** on Map 12.9.A.

- 12.9.4.2. In **Low-Rise Residential** areas the permitted uses shall be residential uses.
- 12.9.4.3. In addition to the uses listed in policy 12.9.4.2, lands designated **Low-Rise Residential** on Centre Street east of Brooke Street, within the village of Thornhill Heritage District, may also be used for business and professional offices, and / or retail commercial uses not exceeding 167.2 square metres in total gross floor area.
- 12.9.4.4. The Detached House building type is permitted in the **Low-Rise Residential** area, pursuant to policy 9.2.3.1.
- 12.9.4.5. Notwithstanding the floor space restriction in policy 12.9.4.3, the lands known as 46 Centre Street (Martin House) being Part of Lot 31, Concession 1, may be developed for business and professional offices, and / or retail commercial uses up to a maximum of 282 square metres of net commercial floor area. The implementing zoning by-law shall specify the exact amount of net commercial floor area, not exceeding 282 square metres, subsequent to Council's approval of a detailed site plan.
- 12.9.4.6. The appropriateness of all commercial uses shall be evaluated at the time of consideration of any implementing zoning by-law. The impact of any commercial uses on adjacent residential areas shall be minimized by appropriate site design. If the impact of a commercial use or combination of uses on a residential area cannot be minimized so as to make it compatible with the residential area, it shall not be permitted.
- 12.9.4.7. In its consideration of applications for commercial development and redevelopment, Council shall evaluate all applications on the basis of the following criteria:
- a. The retention of existing historical buildings shall be encouraged to as great an extent as possible;
  - b. The preservation of mature landscape elements shall be encouraged;
  - c. The creation or preservation of a landscape buffer zone between residential and commercial shall be provided using existing mature trees wherever possible;
  - d. Parking areas shall be designed to reduce the visual impact of the automobile and shall provide safe parking areas separated from pedestrian areas. Design criteria to be considered are:
    - i. Discourage parking areas visible from Yonge Street and Centre Street;
    - ii. Landscaping in parking area or shielding parking areas;

- iii. Architectural elements to reduce the visual impacts of the automobile such as building design, porte cocheres, fences, etc.;
- iv. The type and texture of paved areas;
- v. Connections for common rear lot laneways and parking areas; and,
- vi. Joint or mutual access points to major roads;
- e. Service areas and garbage storage areas shall be shielded to as great an extent as possible, by means of landscaping or architectural elements;
- f. Entrances should encourage pedestrian access from Yonge Street;
- g. Pedestrian systems within the site such as mews, courtyards, lanes, etc., will be encouraged;
- h. Pedestrian linkways between commercial sites will be encouraged either by means of connections to the street or by direct connections to adjacent sites;
- i. Setbacks shall be judged by the compatibility of the buildings with neighbourhood structures and where feasible shall be aligned with neighbouring buildings; and,
- j. Exterior lighting shall be designed in consideration of the following criteria:
  - i. Lighting of parking areas shall promote safety while not impacting on adjacent areas;
  - ii. Lighting systems which use incandescent light source shall be encouraged; and,
  - iii. Lighting standards used on site shall be compatible with the historic character of the Village.

### **12.9.5 Land Use Policies: Public Open Space / Plaza Designation**

12.9.5.1. Within the Thornhill Yonge Street Corridor, there are specific properties identified with a **Public Open Space Plaza** designation. It is the intent of this Secondary Plan that these sites accommodate a significant public open space component as part of the redevelopment proposal. The size, location and design of the required amenity space shall be subject to the satisfaction of the City, in accordance with the urban design policies of this Secondary Plan.

### **12.9.6 Policies Applicable to Area A**

12.9.6.1. Notwithstanding the foregoing, the following policies apply to the lands located north of Arnold Avenue, south of Centre Street on the west side of Yonge Street identified as **Area A** on Map 12.9.A (7584, 7586, 7588, 7590, 7592, 7594, 7596, 7598, 7600, 7602, 7604, 7610 and 7616 Yonge Street):

- a. New development proposed for the lands identified as **Area A** having frontage on Yonge Street is required to include ground floor at-grade commercial uses.
- b. Residential dwelling units, residence suites and uses accessory thereto (except for a lobby) at-grade are not permitted along the Yonge Street frontage.
- c. The primary intent for the use of the lands shown on Map 12.9.A containing the existing heritage building known as the Robert Cox House is the protection and adaptive re-use of the existing heritage building.
- d. The following uses are permitted for the lands identified as **Area A** on Map 12.9.A unless otherwise stated:
  - i. Apartment Dwelling, subject to the following:
    - A. a maximum of 225 residence suites; or,
    - B. a maximum of 150 dwelling units; or,
    - C. a combination thereof not to exceed a total of 225 residence suites/ dwelling units (subject to a maximum of 150 dwelling units);
  - ii. Retail Commercial Uses;
  - iii. Service Uses;
  - iv. Business or Professional Office;
  - v. Banking or Financial Institution;
  - vi. Club or Health Centre;
  - vii. Day Nursery; and,
  - viii. Eating Establishment Uses;
- e. The following uses shall be permitted at-grade along the Yonge Street frontage for the lands identified as **Area A** on Map 12.9.A:
  - i. Retail Commercial Uses;
  - ii. Service Uses;
  - iii. Eating Establishment Uses;
  - iv. Business or Professional Office;
  - v. Banking or Financial Institution; and,
  - vi. Club or Health Centre, subject to a maximum of 25% of the building frontage along Yonge Street and a maximum of 250 square metres of gross floor area to be allocated to such use;
- f. The following uses are not permitted for the lands identified as **Area A** on Map 12.9.A;
  - i. Retail Stores, Service Uses, Club or Health Centre Uses and Eating Establishment Uses requiring individual premises in excess of 1,000 square metres;

- ii. Free standing, single storey buildings containing retail uses, restaurants, banking and financial institutions, club or health centres and or commercial schools. This shall not prevent the use of the Robert Cox house for Retail or Service Commercial uses;
  - iii. Drive-Through facilities of any kind;
  - iv. Gas Bars / Service Stations, Public Garage or associated uses, either stand alone or associated with any permitted uses;
  - v. Any use that requires outdoor storage, excluding small scale sidewalk display areas; and,
  - vi. Residential uses referenced in policy 12.9.6.1.d.i to be located within the Robert Cox House;
- g. The building height for new development, not including the Robert Cox House, within **Area A** shall be a maximum of 5 storeys and to a maximum height of 17 metres, subject to the following additional policies:
- i. With respect to new development having frontage on Yonge Street for the lands identified as **Area A** on Map 12.9.A, the building height can be increased by one-storey to a maximum of 6 storeys and a maximum of 20.6 metres, provided the 6th storey is stepped-back a minimum of 1.5 metres from the building frontage along Yonge Street for at least 75% of that frontage and stepped back a minimum of 3.5 metres from the Robert Cox House;
  - ii. With respect to new development on the lands designated identified as **Area A** on Map 12.9.A to the west of the existing Robert Cox House the building height shall be a maximum of 4 storeys and a maximum of 13.5 metres;
  - iii. In all new development, the ground floor height shall be a minimum of 4.0 metres as measured from the average finished grade along Yonge Street from top of floor slab to top of floor slab;
  - iv. Appropriate building heights and height transition regulations will be established by the implementing zoning by-law;
  - v. A minimum building height of 2.5 storeys or 8 metres is required for all new development; and,
  - vi. All new development within **Area A** shall be subject to site plan control, and shall conform to the Urban Design policies of this Plan;
- h. The maximum net site density on the lands identified as **Area A** shall not exceed 2.0 Floor Space Index (F.S.I.);

- i. Buildings shall be oriented toward public streets in order to clearly define the public realm, to create a consistent street wall and to create attractive retail and commercial environments for pedestrians;
- j. Buildings and their primary entrances and elevations shall be sited to address public streets and sidewalks along Yonge Street;
- k. Any new building construction within identified as **Area A** on Map 12.9.A shall occupy a minimum of 70 percent of the buildable street frontage. Buildable street frontage shall mean the horizontal distance measured from side lot line to side lot line that is not encumbered by any required interior or exterior side yard, or any natural or cultural feature that would otherwise preclude urban development;
- l. When buildings are related to and located at the street, they shall not be separated from the street by either parking areas or driveway aisles;
- m. All commercial uses located on the ground floor occupying the Yonge Street frontage shall have windows and primary commercial entrances fronting onto the public street system. In recognition of the role that windows play in providing a visual connection between the building and the outside environment, it is important that appropriate glazing materials be used. Glazing materials shall generally allow for the transmission of light and views between the inside and outside, and opaque (i.e. spandrel glass) shall not comprise more than 25% of the total window area;
- n. Notwithstanding 12.9.6.1.g.i above, an additional 1.5 metre setback to create a recessed building frontage segment of approximately 6-12 metres is permitted;
- o. Building mass should reflect a linked series of pavilion type buildings defined by recessed connector building segments. Where any development exceeds 50 metres of continuous building frontage, a pedestrian connection to the rear lot commercial parking area shall be required. Each pedestrian connection may be privately owned, but must be accessible to the public at all times. For development along the Yonge Street primary commercial frontage a maximum of 65 metres of continuous building frontage is permitted;
- p. The City shall require an enhanced Streetscape Zone to be provided by the proponent along Yonge Street as follows:
  - i. A minimum building setback of 1.8 metres is required from the right-of-way of Yonge Street. The required 1.8 metres building setback and any building setback in excess of 1.8m adjacent to the Yonge Street right-of-way shall be considered an enhanced Streetscape Zone and shall be developed in a manner consistent with the Streetscape Improvement Program identified

- within the Council adopted Thornhill Yonge Street Study (2005); and
- ii. Setbacks from Arnold Avenue right-of-way shall be generally consistent with existing development setbacks and shall be determined by the City through the zoning of the property;
  - q. The provisions of private landscaping and pedestrian-scaled lighting, when not provided in the public right-of-way, is required at the interface between new development and the Streetscape Zone and between buildings and any parking areas;
  - r. Landscape requirements for new development shall be determined at the site plan approval stage;
  - s. Commercial and residential parking shall be provided at the rates specified in the implementing zoning by-law. Above grade, structured parking is not permitted. Residential parking, except for visitors and handicapped spaces, shall not be permitted at or above grade. Commercial parking is permitted in underground parking structures and in surface parking lots, subject to the following policies:
    - i. Surface parking areas shall be permitted provided said areas are located to the rear of the lot;
    - ii. Parking is not permitted between a public right-of-way and the building face on any public street;
    - iii. Trees and other landscape features shall be employed to visually break up large expanses of surface parking and to screen the view of parking lots from public streets; and,
    - iv. Parking facilities for bicycles should be provided in residential and commercial developments;
  - t. Direct vehicular access onto Yonge Street shall be limited to two one-way driveways for access and egress respectively;
  - u. Private lanes and internal driveways are important components of the overall circulation system. Connectivity of the rear driving laneways facilitates traffic movement and shall be encouraged. High quality design, material and execution should be employed in these areas;
  - v. The visibility of any loading, garbage storage and other service areas from any public street or abutting residential community shall be effectively screened. Garbage receptacles/storage shall be provided within the building. Loading and other service areas located outside of the building shall be determined by the City at the time of consideration of the Site Development Application;

- w. Signage shall be incorporated into the building, and consistent in design with the village character and the specific guidelines provided in the Thornhill Vaughan Heritage Conservation District Plan (2007);
- x. Street and pathway lighting shall be consistent in design with the village character and the specific guidelines provided in the Thornhill Vaughan Heritage Conservation District Plan (2007); and,
- y. Sustainable community objectives shall be implemented through building design that: supports cycling and walking; ensures the connectivity of the lands designated **Area A** as shown on Map 12.9.A to the broader neighbourhood and community; promotes transit opportunities, water/energy efficiencies, energy alternatives, and “green” building design (LEED standards) and site development.

12.9.6.2. For the portions of **Area A** that are designated **Low-Rise Residential**, the minimum lot dimensions shall be 18.2 metres by 54.0 metres.

### 12.9.7 Urban Design Policies

- 12.9.7.1. Buildings shall be oriented towards public streets in order to clearly define the public realm, to create a consistent street wall and to create attractive retail and commercial environments for pedestrians. Buildings and their primary entrances and elevations shall be sited to address public street and sidewalks in the following order of priority:
- a. The Yonge Street frontage;
  - b. The corner of Yonge Street and any Secondary or Local Street intersection;
  - c. The frontage of a Secondary Street;
  - d. The corner of any Secondary Street and any Local Street; and,
  - e. The frontage of any Local Street.
- 12.9.7.2. All residential uses at ground level, where permitted, shall have their primary individual unit entrances located on the street frontage. Residential uses above the ground floor may have common access through a shared entry lobby.
- 12.9.7.3. Buildings located at street corners are encouraged to include architectural features that enhance the visual prominence and identity of these important locations within the Plan Area. Corner building entrances are encouraged at corner building locations. Development shall be in accordance with Section 3.7.2 of the Thornhill / Yonge Street Study (2005).

- 12.9.7.4. Buildings shall occupy a set percentage of the buildable street frontage, as follows:
- a. Buildings fronting Yonge Street shall occupy a minimum of 70 percent of the buildable street frontage;
  - b. Buildings fronting a Secondary or Local Street shall occupy a minimum of 50 percent of the buildable street frontage; and,
  - c. Buildable street frontage shall mean the horizontal distance measured from side lot line to side lot line, that is not encumbered by any required interior or exterior side yard, or any natural or cultural feature that would otherwise preclude urban development.
- 12.9.7.5. When buildings are related to and located at the street, they shall not be separated from the street by either parking areas or driving aisles.
- 12.9.7.6. All commercial and residential buildings shall have windows fronting onto the public street system. In recognition of the role that windows play in providing a visual connection between the building and the outside environment, it is important the appropriate glazing materials be used. Glazing materials shall generally allow for the transmission of light and views between the inside and outside and shall not be opaque.
- 12.9.7.7. Building mass should reflect a linked series of pavilion–type buildings defined by recessed connector building segments. Where any development exceeds 50 metres of continuous building frontage, a pedestrian connection to the rear lot commercial parking area shall be required. Each connection may be privately owned, but must be accessible to the public at all times. For development along the primary commercial frontage and / or for all corner pavilion segments a maximum of 30.0 metres of continuous building frontage is permitted. Development shall be in accordance with Section 3.7.2 of the Thornhill / Yonge Street Study (2005).
- 12.9.7.8. The City shall require enhanced Streetscape Zones to be provided by the proponent, as follows:
- a. A minimum building setback of 1.8 metres is required from the right-of-way of Yonge Street and any Secondary Street. The required 1.8 metres building setback and any building setback in excess of 1.8 metres adjacent to the Yonge Street or any Secondary Street right-of-way shall be considered an enhanced Streetscape Zone and shall be developed in a manner consistent with the Streetscape

Improvement Program identified within the Council adopted Thornhill Yonge Street Study (2005); and,

- b. Setback from any Local Street right-of-way shall be generally consistent with existing development setbacks and shall be determined / confirmed by the City through the rezoning process. The required building setback and / or enhanced Streetscape Zone shall be developed in a manner consistent with the Streetscape Improvement Program identified with the Council adopted Thornhill Yonge Street Study (2005).

12.9.7.9. The provision of private landscaping and pedestrian-scaled lighting, when not provided in the public right-of-way, is required at the interface between new development and the Streetscape Zone and between buildings and parking areas.

12.9.7.10. At the interface between residential and non-residential properties, the non-residential property generally must provide a landscaped buffer consisting of tightly planted coniferous plant material and fencing installed to a height of 2 metres, and higher branching deciduous trees.

12.9.7.11. Commercial and residential parking shall be provided at the rates specified in the zoning by-law. Above grade, structured parking is not permitted. Residential parking shall be provided in underground parking structures. Commercial parking is permitted in underground parking structures and in surface parking lots, subject to the following policies:

- a. Surface parking areas shall be located internally and to the rear of the lot. Parking is not permitted between the edge of the public right-of-way and the building face on any public street;
- b. Trees and other landscape features shall be employed to visually break up large expanses of surface parking and to screen the view of parking lots from public streets; and,
- c. Parking facilities for bicycles should be provided in residential and commercial developments.

12.9.7.12. Direct vehicular accesses onto Yonge Street are discouraged. All driveway access points are considered part of the public realm, and shall be located to be consistent with the Council adopted Thornhill Yonge Street Study (2005). Generally, each block face

should have no more than 2 points of access to the block connection to the internal private driveway system.

- 12.9.7.13. Private lanes and internal driveway are important components of the overall circulation system. Connectivity of rear driving laneways facilitates traffic movement and shall be encouraged. The level of connectivity, the design and configuration of these lands and driveways shall be determined by the City at the time of site plan review. High quality design, material and execution should be employed in these areas.
- 12.9.7.14. Loading, garbage storage and other service areas shall not be visible from any public street or abutting residential community. Garbage receptacles / storage shall be provided within the building. Loading and other service areas located outside the building shall be appropriately screened utilizing fencing and / or opaque landscaping, the design of which shall be determined by the City at the time of site plan review.
- 12.9.7.15. Signage shall be incorporated into the building, and consistent in design with the village character and the specific guidelines provided in the Thornhill Heritage Conservation District Plan.
- 12.9.7.16. Street, open space and pathway lighting shall be consistent in design with the village character and the specific guidelines provided in the Thornhill Heritage Conservation District Plan.

## **12.9.8 Transportation Policies**

- 12.9.8.1. It is the intent of this Secondary Plan to ensure the provision of a balanced movement system that will enable Yonge Street to perform many functions without shifting the balance to favour one form of movement over another. This section describes the components of the functional transportation system associated with the Vision to ensure that Yonge Street is able to accommodate the flow of regional traffic, but also serve as a high value pedestrian friendly mainstreet and residential address, a Rapid Bus Transitway, and an entryway to the historic district of Thornhill. The following criteria will be considered in establishing a balanced movement system within the Thornhill / Yonge Street Plan Area:
  - a. The development of a connected network of rear driving laneway occurring across multiple properties to establish a secondary movement system reducing

congestion and turning movements on Yonge Street and providing better access to commercial parking areas. Lane access will be achieved via secondary and local streets;

- b. A reduction and consolidation of driveway access points off Yonge Street further reducing congestion of Yonge and improving the safety of the pedestrian environment;
- c. The identification and special treatment of 'pedestrian priority' areas occurring at primary intersections to reinforce safe pedestrian crossing and transit access. Pedestrian priority areas are to be enhanced with special in-street paving to visibly and audibly signal to vehicular users they have entered a special area wherein pedestrians have priority over the movement of cars;
- d. The removal of pavement redundancies throughout the Thornhill / Yonge Street Corridor Plan Area by implementing minimum transitway and roadway design criteria to minimize street widening and support a balanced transit, pedestrian and vehicular movement system;
- e. The use of non-perfect road alignments across intersections to further reduce intersection redundancies and reduce pedestrian street crossing distances;
- f. The use of reduced roadway and transitway development standards to ensure efficient functional performance of the transportation system, while minimizing street widening and ensure an adequate and useable public realm within the established rights-of-ways; and,
- g. The reduction of curb radii at all intersections to improve pedestrian safety, increase available pedestrian surface waiting areas, and reduce street crossing distances.

12.9.8.2. The Thornhill Yonge Street Study (2005) establishes a Street Classification System as a basis for Urban Design Guidelines, the Streetscape Improvement Program and access and movement recommendations. The Street Classification System includes Yonge Street, Secondary and Local Streets and Private Laneways, each having a distinct and important role in accommodating pedestrian and other types of movement. The public and private street system will maximize connectivity and movement in the Plan Area, and largely define the character of the pedestrian environment. The Street Classification System is identified on Map 12.9.B.

- 12.9.8.3. Yonge Street is the only Primary Street in the Corridor Area due to its important traffic and transit roles, its role in establishing the primary address and identity for the entire neighbourhood, and its role in furthering the economic development of the neighbourhood as a commercial mainstreet. Yonge Street will be redeveloped in the short-term consistent with the recommendations provided in the Thornhill Yonge Street Study (2005) to accommodate the development of the Rapid Bus Transitway. The following policies for Yonge Street apply within the Plan Area:
- a. A +/- 36 metre public street right-of-way will be established for Yonge Street. The right-of-way will define a consistent minimum 5.2 metre Public Boulevard Pedestrian Zone. The right-of-way will be widened at key intersections to accommodate the minimum public pedestrian zone and dedicated turning lanes at key intersections as necessary. The +/- 36 metre right-of-way will occur throughout the Plan Area, excepting areas where the location of existing built form prevents right-of-way expansion;
  - b. In addition, private sector components of the streetscape pedestrian zone will include a 1.8 metre streetscape easement to be located immediately adjacent to the Yonge Street Primary Streetscape of 5.2 metres. An additional 2.5 metre streetscape easement for isolated areas, will be determined by the municipalities through the Streetscape Implementation Program. See Section 3.9.2 of the Thornhill Yonge Street Study (2005) for details respecting private streetscape easements;
  - c. Removal of utility poles and the burial of utility lines within the Thornhill / Yonge Street Corridor plan Area is required to provide for more intense forms of urban development appropriate for a higher order transit corridor, and to eliminate physical hydro pole intrusions adjacent to mid-rise redevelopment sites as well as a visual and physical intrusion within the public realm;
  - d. The provision of off-peak parallel parking within the Thornhill / Yonge Street Corridor Plan Area should be further studied by the City and Region to better support commercial development and extended hours of commercial operation. On street lay-by parking as well as off street lay-by parking should be provided close to Yonge Street on Local and Secondary Streets;
  - e. A higher quality of streetscape development is required within the Yonge Street right-of-way to support its multifunctional role as a transit, pedestrian and vehicular corridor, its economic development role as a focus for redevelopment, and its location within the Thornhill Heritage Conservation District. The streetscape improvements for Yonge Street are specifically identified in Section 3.9 of

the Thornhill Yonge Street Study (2005) and will be further refined by the municipalities;

- f. Access to individual properties from Yonge Street will be reduced and discouraged through the development of the Rapid Bus Transitway and Transitway Media, by encouraging access from the side streets and through the development of a connected network of internal drives and restoration of the public realm environment to pedestrian use; and,
- g. Existing lands within the right-of-way sight triangles with currently permit high speed island-separated right turning movements, are not required for any transportation function associated with the introduction of the Rapid Busy Transitway and should be conveyed to the City at the time of Transitway development for development as open space / plazas.

- 12.9.8.4. The Secondary Streets in the Secondary Plan Area are Arnold Avenue and Centre Street. Secondary Streets have a supporting function to Yonge Street, and generally carry higher volumes of traffic than Local Street while providing access to commercial property. These Secondary Streets serve as important east-west connections between the Plan Area and other destinations and places within Vaughan.
- 12.9.8.5. The Local Streets in the Secondary Plan Area are Old Jane Street and Thornnridge Drive and are primarily residential streets with relatively low volumes of traffic which provide for both a safe, high quality pedestrian environment and access to commercial and residential properties. The following additional policies will apply to the Local Streets within the Amendment Area.
  - a. The 1.8 metre streetscape and development setback in mixed-use areas, should change to reflect existing conditions within neighbourhood areas;
  - b. The intersection junctions of Yonge Street and local streets should be physically improved through the development of the Rapid Bus Transitway; and,
  - c. Left turning movements from Yonge Street to Jane Street and Thornnridge Drive should not be permitted in consideration of the dedicated Rapid Busy Transitway on Yonge Street.
- 12.9.8.6. Private laneways will establish a secondary movement system that will alleviate pressures on the public street system, reduce congestion on Yonge Street, and increase convenience for patrons of commercial uses on Yonge Street. In addition, the system of connected internal laneways will reduce the number of existing access points to

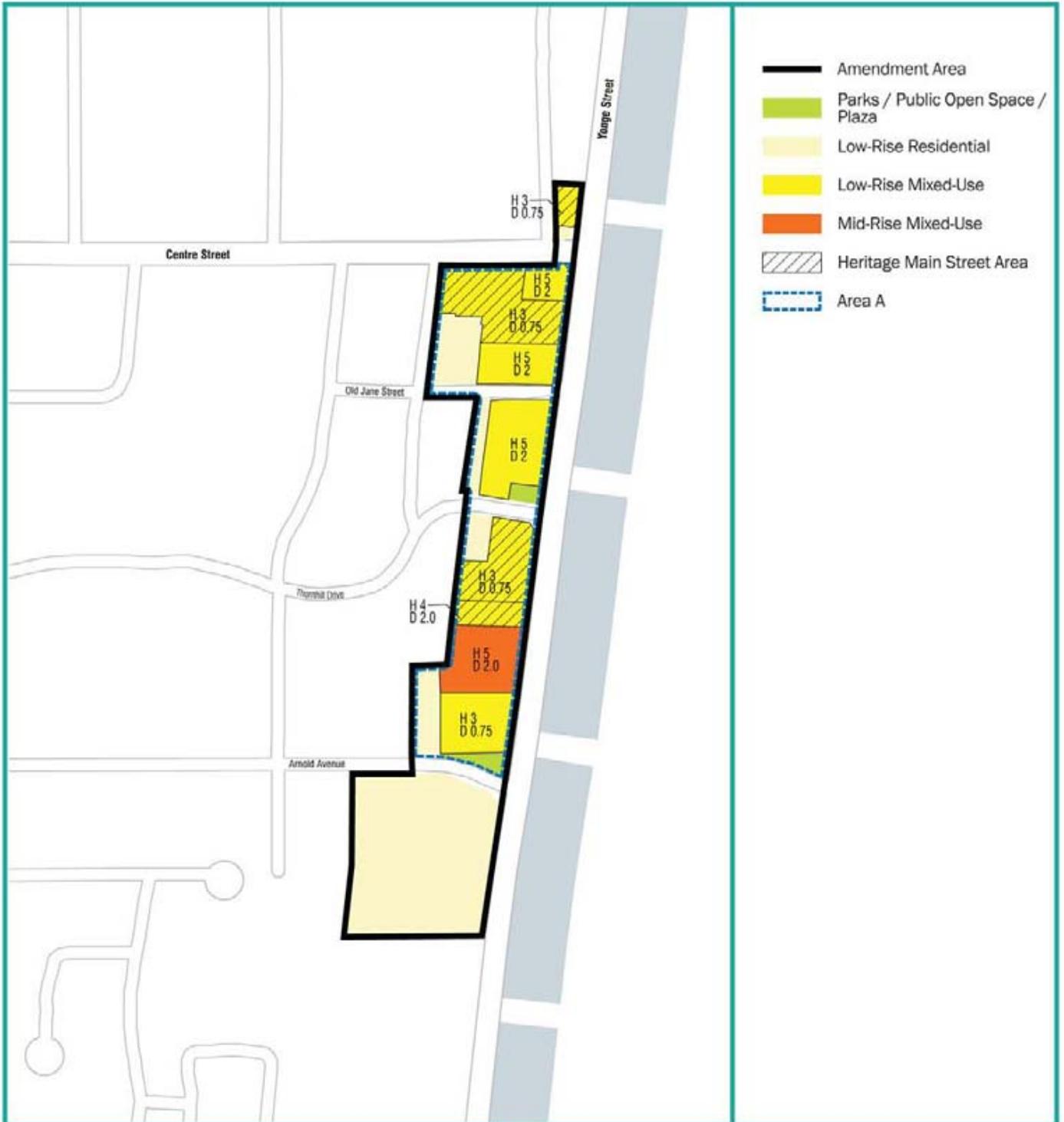
commercial properties from Yonge Street, and provide connected access to rear parking areas and buildings across multiple development parcels.

- 12.9.8.7. Enhanced Pedestrian / Vehicular Laneways and Parking Courts within the Plan Area include:
- a. Arnold Avenue to Thornridge Drive Laneway; and,
  - b. Thornridge Drive to Jane Street Laneway.
- 12.9.8.8. Within the Secondary Plan Area these ‘enhanced pedestrian / vehicular laneways’ and ‘parking courts’ form part of the internal laneway system and require special or enhanced streetscape treatment. The enhanced pedestrian vehicular laneways are to be narrow mixed pedestrian and vehicular connections of approximately 6 to 10 metres in width. The enhanced laneways are to be designed according to municipal standards, to enable conveyance to the municipality.
- 12.9.8.9. The following parking policies will apply to all new development within the Thornhill / Yonge Street Secondary Plan Area:
- a. Parking for development along Yonge Street is to be located to the rear of the blocks and access to this parking shall be from Secondary and / or Local Streets rather than Yonge Street. Above-ground parking structures are prohibited;
  - b. Maximization of available parking should be encouraged through collective commercial parking solutions, which utilize signage as opposed to fencing to define parking for specific commercial areas. The precise configuration of this system of connected internal laneways will be determined by the City on a site by site basis through the Site Plan Review process;
  - c. Residential parking is to be provided in underground parking structures, which may be access from the connected internal drive system or from the Secondary and / or Local Street system; and,
  - d. The City will consider implementing shared parking and / or reduced parking standards for private sector development projects within the Plan Area, subject to a comprehensive parking study for the entire Plan Area, prepared to the satisfaction of the City and implemented in the zoning by-law.

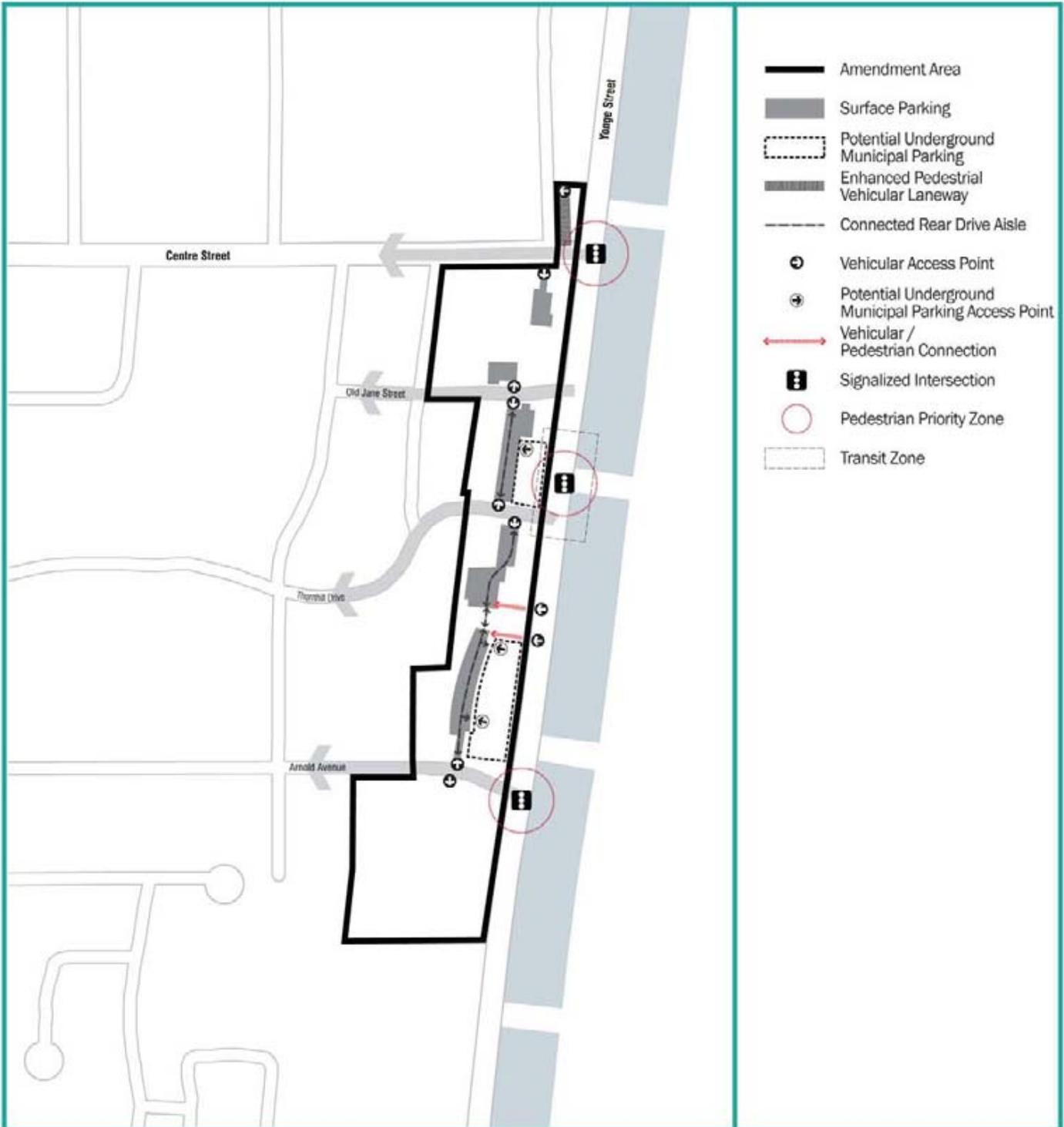
## **12.9.9 Community Improvement**

- 12.9.9.1. It is the intent of the City that the area defined as subject to the Thornhill Yonge Street Corridor, on 12.9.1, be identified as a Community Improvement Area under Section 28 of the Planning Act.

▼ Map 12.9.A: Yonge Street Corridor In Thornhill - Land Use



▼ Map 12.9.B: Yonge Street Corridor In Thornhill - Transportation Network



## 12.10 Centre Street Corridor

### 12.10.1 Goals

- 12.10.1.1. The goal of this Secondary Plan is to improve the Centre Street Corridor, shown on Map 12.10.A, as an attractive, pedestrian-friendly and transit-supportive corridor through the application of a comprehensive, co-ordinated and integrated approach to planning, transportation and urban design, based on a set of urban design policies consistently applied to new development and redevelopment.

### 12.10.2 Urban Design

- 12.10.2.1. Future development in the Centre Street Corridor will address the following urban design policies as they apply to the public realm and private lands, in order to create an attractive, safe and pedestrian-friendly environment:
- a. Buildings should have active facades including primary windows and entrances facing the street, to provide convenient accesses to pedestrians and users of public transit;
  - b. When buildings are located close to the street, the buildings should not be separated from the street by either parking areas or drive aisles;
  - c. Buildings should be sited to minimize their impact on neighbouring properties, respecting and protecting the existing scale and character of adjacent residential neighbourhoods;
  - d. Further reverse lot development (residential and/or commercial) will not be permitted along Centre Street. Where parking access is located off the street by way of an internal driveway circulation system, the units should still have a functional primary pedestrian entrance that relates to Centre Street;
  - e. The area between the building wall and the street should contribute to a positive social and visual environment on the street;
  - f. Buildings should be designed with a high level of architectural materials;
  - g. Large, blank walls and repetitive facades should be avoided. Variation in certain elements of façade treatment should be provided;
  - h. Architectural variation and animation should be encouraged and include building elements such as balconies, windows, porches, railings, entrances and awnings and attractive signage where appropriate;
  - i. Surface parking should have the least possible impact on the public realm; large surface parking areas are discouraged and if required, should be setback, screened and buffered from the street edge, with comfortable pedestrian routes provided through and around parking areas;

- j. Where the grade changes over the length or width of the site, accommodation for the grade changes, if required, should be accomplished on the site within the parking lot or within the buildings, not at the street edge;
- k. Service and loading areas are prohibited adjacent to public streets and must, at all times, be effectively screened and, if possible, fully enclosed;
- l. Access to underground parking should be appropriately integrated into the buildings it serves and located so as to minimize physical and visual impact on public sidewalks and open spaces;
- m. Garbage will be stored within the building;
- n. Urban Design Guidelines and Architectural Control Guidelines may be required by development applicants, and shall address both built form and public realm considerations to the satisfaction of the City; and,
- o. The following area-specific policies apply to properties located within the four quadrants around the Dufferin Street and Centre Street intersection
  - i. Corner sites should be defined with more intense development and high quality architecture and serve as landmarks or gateway entry points;
  - ii. Development density shall be concentrated close to the Dufferin/Centre intersection, and along the frontages of Dufferin Street and Centre Street;
  - iii. Maximum permitted building heights shall provide for the tallest buildings, including landmark buildings, to be located at the key corner locations, and such buildings shall be designed to allow stepping down of the heights from the corner back towards adjacent low-density residential neighbourhoods. The minimum height for buildings at key corner locations shall be two-storeys;
  - iv. Pedestrian and vehicle crossings at the key intersection shall be well defined;
  - v. Streetscape and street furniture shall be high quality and of unified design to clearly demarcate the gateway;
  - vi. Landowners should consolidate driveways and provide interconnected laneways, where appropriate, as sites develop or redevelop over time; and,
  - vii. Commercial and/or mixed-use buildings shall be built to within 3 metres of the front property line and residential buildings setback, with soft landscaping (up to 5 metres) from the front property line.

### 12.10.3 Land Use: Low Rise Residential

- 12.10.3.1. Notwithstanding policy 9.2.1.1, the following policies apply to the lands identified as **Low-Rise Residential** on Map 12.10.A.
- 12.10.3.2. In **Low-Rise Residential** areas the permitted uses shall be residential units.
- 12.10.3.3. In **Low-Rise Residential** areas the permitted building types shall be Detached Houses pursuant to policy 9.2.3.1.
- 12.10.3.4. The net density in **Low-Rise Residential** areas shall not exceed 22 units per ha. In addition existing single family detached dwelling units exceeding a density of 22 units per ha shall be permitted.
- 12.10.3.5. For the purposes of calculating net residential density in **Low-Rise Residential** areas, a net residential hectare shall include local streets and residential collector roads as well as the land for the dwelling units, but shall exclude parks, walkways and other public open space uses.

### 12.10.4 Land Use: Commercial Mixed-Use Area A

- 12.10.4.1. Notwithstanding policy 9.2.1.1, the following policies apply to the lands identified as **Commercial Mixed-Use A** on Map 12.10.A.
- 12.10.4.2. Permitted uses in **Commercial Mixed-Use Area A** area shall include:
- a. small scale office uses; and,
  - b. religious institutions, day care centres, schools, and libraries.
- 12.10.4.3. Notwithstanding policy 9.2.1.5 the maximum height for any building within the **Commercial Mixed-Use A** designation shall be two and one-half storeys (11 metres). The one-half storey is permitted in the roof of the building, and its floor area may be up to 50% of the GFA of the second storey. The one-half storey shall be incorporated in a peaked-roof design, with the roof having a minimum pitch of 45° and maximum of 60° off the horizontal.
- 12.10.4.4. To encourage operational compatibility any development within the **Commercial Mixed-Use A** designation will be required to proceed by way of site-specific zoning

by-law amendment, and site development applications shall have specific regard to operational matters, including but not limited to parking, lighting, loading and service areas. Also, a consolidation of individual driveways is required wherever possible within this designation.

### **12.10.5 Land Use: Commercial Mixed-Use Area B**

- 12.10.5.1. Notwithstanding policy 9.2.1.1, the lands at the northeast corner of Vaughan Boulevard and Centre Street, being Lots 92, 93, 94, 95 and 96, Registered Plan 3541, and municipally known as 1130, 1144, 1152, 1164, 1176 Centre Street and identified as **Commercial Mixed-Use Area B** on Map 12.10.A, shall be developed only in accordance with the following policies:
- a. The permitted uses include business and professional offices and commercial uses, provided that the minimum gross floor area devoted to business and professional offices shall not be less than 60% of the gross floor area of the building(s);
  - b. Notwithstanding a) above, general commercial uses such as any automotive related uses, places of amusement or entertainment, or any commercial uses requiring outside storage shall not be permitted; commercial uses permitted shall be defined in the implementing zoning by-law;
  - c. The total ground gross floor area devoted to all eating establishment uses shall be a maximum of 30% of the ground floor area of each building in this development, provided that a maximum of 30% of the total gross floor area of each building devoted to all eating establishment uses shall be developed for convenience or take-out eating establishments; and no drive-through style restaurants shall be permitted;
  - d. The maximum floor space index shall be 0.8 F.S.I.;
  - e. Substantial landscape areas shall be provided to screen the parking areas from abutting roadways, particularly on Centre Street, and to provide an attractive “campus style” setting oriented to the pedestrian streetscape and complementary to the adjacent residential neighbourhood;
  - f. The massing and conceptual design of all buildings shall be low profile, being limited to a combination of two and three storeys in building height, and shall be compatible with residential areas to the north and west; and,
  - g. Comprehensive assembly and/or development of the entire Subject Lands is required, with implementation to occur on the basis of a comprehensive zoning by-law and site plan.

- 12.10.5.2. The following building types are permitted in **Commercial Mixed-Use Area B** shown on Map 12.10.A pursuant to policies in Section 9.2.3:
- a. Employment/Industrial buildings; and,
  - b. Low-Rise buildings.

### **12.10.6 Land Use: Commercial Mixed-Use Area C**

- 12.10.6.1. Permitted uses in **Commercial Mixed-Use Area C** areas shall be existing commercial uses, retail stores for the buying, leasing and exchanging of goods and services, restaurants, banks and business and professional offices.
- 12.10.6.2. New commercial uses must be compatible with the proposed site, in terms of building design and coverage, parking provided, landscaping and buffering, and adjacent land uses. Council consideration should be given to broadening the permitted retail and service commercial uses within an implementing Zoning By-law and definitions to allow a greater range of commercial uses which reflect evolving consumer needs without imposing negative impacts on neighbouring residential areas.
- 12.10.6.3. Such uses shall only be approved consolidated in nodes in accordance with good design principles with emphasis on common internal traffic circulation. Access to internal road shall only be by service or collector roads, wherever possible.
- 12.10.6.4. Notwithstanding the above, the lands municipally known as 1453 and 1465 Centre Street, being Lots 3 and 4, Registered Plan 3668, shall be subject to the following:
- a. The Lands shall be used only for the purpose of a sit down restaurant having a maximum gross floor area of 450 square metres and containing a maximum of 100 seats;
  - b. The Lands shall not be developed until such time as full municipal services are available; and,
  - c. The Lands have been identified by the Ministry of Transportation and Communications as future Highway #407 interchange and accordingly these lands are to be utilized for the above-noted specified commercial use (i.e. restaurant) until such time as the property is required for the physical construction of Highway #407 by the Ministry of Transportation and Communications.
- 12.10.6.5. Comprehensive development plans will be required for new development in all **Commercial Mixed-Use Area C** areas and such development shall be subject to

the approval of site plans by the City through agreements or letters of undertaking, whichever is in effect.

- 12.10.6.6. No residential use shall be permitted in commercial areas other than as a direct accessory and related individual use to a permitted use.
- 12.10.6.7. Building setbacks, visual screening, planting and/or fencing shall be provided between commercial uses and adjacent residential areas, except where such areas form an integral part of medium and high density residential schemes.
- 12.10.6.8. The following building types are permitted in **Commercial Mixed-Use Area C** shown on Map 12.10.A pursuant to policies in Section 9.2.3:
  - a. Employment/Industrial buildings; and,
  - b. Low-Rise buildings.

### **12.10.7 Land Use: Commercial Mixed-Use Area D**

- 12.10.7.1. Notwithstanding policies 9.2.1.1, the following policies apply to the area identified as **Commercial Mixed-Use Area D** on Map 12.10.A.
- 12.10.7.2. A wide range of industrial, office, business and civic uses shall be permitted. No outside storage of goods or materials shall be permitted in this designation, provided that the priority uses for the area shall be offices, hotels with related hospitality and conference facilities, major corporate complexes and other prestige employment uses. The amount and type of retail commercial and personal service uses shall be limited to that which is ancillary to these priority functions and, in orientation and location, such uses shall be integrated within a development devoted to the priority uses.
- 12.10.7.3. In addition to the uses listed in policy 12.10.6.2, the following recreational and Institutional uses shall also be permitted:
  - a. community facilities;
  - b. arenas (private or public);
  - c. swimming pools (private or public);
  - d. greenway systems;
  - e. places of worship;
  - f. public health facilities;
  - g. cultural and social facilities;

- h. day care centers;
  - i. non-profit clubs and organizations; and,
  - j. Branch Colleges
- 12.10.7.4. Civic uses are also permitted. Accordingly, a new fire station may be located in the area. If the City-owned site on Dufferin Street, currently the existing Patricia Kemp Community Centre, is no longer required for civic purposes, this property may be reused for non-civic employment purposes permitted by the Prestige Area designation, without further amendment to this Plan.
- 12.10.7.5. A “service station/gas bar” use shall not be permitted within the subject lands.
- 12.10.7.6. Hydro Electric utility facilities are also a permitted use within the subject lands. If certain lands owned by Vaughan Hydro are identified as surplus to hydro needs, they may be developed for purposes permitted by the Commercial Mixed-Use designation described in policies 12.10.7.2 and 12.10.7.3, in conjunction with nearby private lands and in accordance with the applicable site-specific policies of this subsection.
- 12.10.7.7. The zoning by-law shall establish the permitted uses and development standards.
- 12.10.7.8. Vehicular access to lots abutting arterial roads and provincial highways shall generally be from the internal road network. Access to the provincial or arterial road systems shall be subject to the approval of the appropriate authority.
- 12.10.7.9. In instances where through lots are provided, buildings shall be designed so that all elevations facing a street present a “front” elevation. Loading areas are not considered appropriate in any yard facing a street. The location of loading areas will be controlled in the zoning by-law.
- 12.10.7.10. Notwithstanding policy 9.2.1.6, the lands identified as **Commercial Mixed-Use Area D** on Map 12.10.A shall be limited to a maximum overall density of 1.0 FSI.
- 12.10.7.11. Any site-specific rezoning application must be based on a Council approved comprehensive plan addressing the location of local roads, street access points, pedestrian connections, buildings, etc.

- 12.10.7.12. As a pre-requisite to the approval of development applications, comprehensive assembly or land exchanges shall be required in order to create efficient parcels to accommodate the intended development, as well as co-ordinated access to Dufferin Street for all land uses.
- 12.10.7.13. Access to all development parcels shall be consolidated by means of new public road(s) connecting the development parcels to Dufferin Street at planned signalized locations or, alternatively, in the event that comprehensive assembly makes the public road provision unnecessary, by means of private agreements, easements and/or rights-of-way to achieve the same consolidation of access points for all land uses, including Vaughan Hydro facilities.
- 12.10.7.14. The following building types are permitted in **Commercial Mixed-Use Area D** shown on Map 12.10.A pursuant to policies in Section 9.2.3:
- a. Employment/Industrial buildings; and,
  - b. Low-Rise buildings.

### **12.10.8 Land Use: Gas Station**

- 12.10.8.1. Notwithstanding policy 9.2.1.1 regarding Gas Stations, such uses are permitted in the locations identified on Map 12.10.A.
- 12.10.8.2. Notwithstanding the above, any Gas Station site designated on Map 12.10.A may be developed in accordance with the land use designation of the immediately adjoining land without amendment to this Secondary Plan.
- 12.10.8.3. Convenience Retail uses are permitted on the same site as an automobile gas bar, provided that such uses are sensitively designed and integrated on the site, and in particular, are compatible with surrounding development.
- 12.10.8.4. Outside storage shall be limited in any future implementing zoning by-laws.
- 12.10.8.5. On the property at 1030 Centre Street, where an automobile service station is located, the permitted uses shall be the sale of auto fuel, oil and lubricant and other related products, the provision of repair and maintenance services for vehicles, and the existing car wash and convenience retail use as listed on the property. Any future redevelopment of this site shall be subject to the following compatibility criteria.
- a. The site shall be developed pursuant to an approved site plan which demonstrates

proper site access, internal traffic circulation, adequate parking, substantial landscaped areas and appropriate urban design characteristics that will ensure compatibility with the adjacent lands; and,

- b. The design, appearance and scale of development on the site shall be consistent with the existing and intended character of the surrounding community, with utmost consideration given to aesthetic design.

### **12.10.9 Special Policy Area**

12.10.9.1. Lands within the Special Policy Area shall be subject to further study by the applicant.

12.10.9.2. Prior to the submission of any development proposal, the applicant shall provide a planning study, including a traffic report, public participation component, rationale for proposed density and land uses, noise study, and a height and massing study demonstrating consistency with the urban design principles of this Official Plan. The urban design, parking, and other general policies of this Official Plan, including the “Service Station/Gas Bar” policy, which in effect prohibits a service station use at the southwest corner of Dufferin and Centre Street, shall apply for any development within this designation. This Study shall provide the justification for any proposed Official Plan Amendment to redesignate the lands.

12.10.9.3. Until such Official Plan Amendment is submitted in accordance with the aforementioned Special Policy Area requirements, the current Low-Rise Residential and Gas Station designations shall apply to the southern portion of the Western Gateway.

### **12.10.10 Transportation**

12.10.10.1. That the Region of York recognize the importance of maintaining full movement access to the existing commercial centres on the north side of Centre Street between Vaughan Boulevard and New Westminster Drive, and reflect this in the planning for any transit facilities in the Centre Street Corridor between Bathurst and Dufferin Street.

12.10.10.2. Sharing of parking will be encouraged within the Secondary Plan Area, subject to evaluation by the City.

12.10.10.3. Within the Secondary Plan Area, parking underground and in structures shall be encouraged.

12.10.10.4. To assist with the reduction in large surface parking areas within the Secondary Plan, a reduction to the current parking standards for commercial development shall be applied

to all surface and/or structured parking provided. A parking standard of a minimum of 4.0 spaces/100 square metres will be used in calculating the required parking spaces for retail commercial development in any designation within the Secondary Plan.

▼ Map 12.10.A: Centre Street Corridor

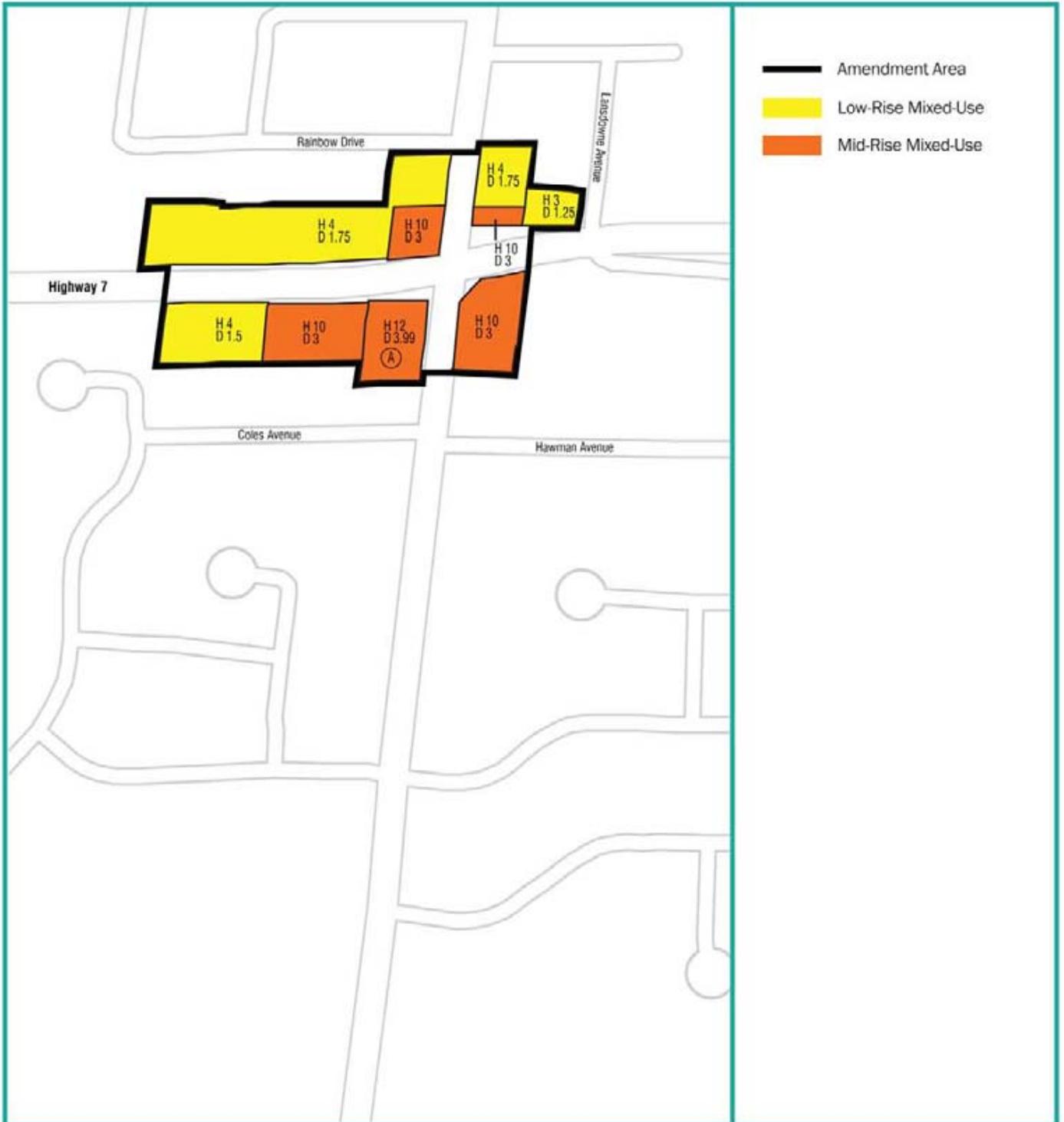


## 12.11 Kipling Avenue and Highway 7

### 12.11.1 General

- 12.11.1.1. Notwithstanding the policies of Section 9.2.1 and 9.2.2 of the Official Plan, the following policies and development criteria shall apply to the lands identified on Map 12.11.A.
- 12.11.1.2. There shall be appropriate height transition between development of the lands identified on Map 12.11.1 and adjacent sensitive land uses.
- 12.11.1.3. For lands identified as **Area A** on Map 12.11.A, the following development criteria apply:
  - a. The maximum number of dwelling units shall not exceed 120;
  - b. New development shall generally respect a 45 degree angular plane measured from the south property line. The maximum building height shall not exceed 12 storeys or 38.4 metres, which is less; stepping down to a maximum height of 4 storeys or 12.8 metres, whichever is less, towards the lands designated **Low-Rise Residential** to the south and shall be defined in the implementing Zoning by-law.
  - c. New development shall minimize shadow impacts on adjacent sensitive land uses demonstrated through the preparation of sun/shadow diagrams to the satisfaction of the City of Vaughan;
  - d. A landscape buffer area shall be provided abutting all lands designated **Low-Rise Residential**;
  - e. The ground floor of any building fronting onto Highway 7, the daylighting triangle, and approximately six (6) metres of the northerly portion of the building fronting onto Kipling Avenue shall be used for street related retail uses. Retail uses shall not be permitted for the balance of any building frontage onto Kipling Avenue; and.
  - f. Appropriate development standards shall be established in the implementing Zoning By-law.
- 12.11.1.4. For all other lands on Map 12.11.A, the following development criteria apply
  - a. New development within thirty (30) metres of a **Low-Rise Residential** designation shall not exceed a height of four storeys or 12.8 metres, whichever is less,
  - b. New development shall minimize shadow impacts on adjacent sensitive land uses demonstrated through the preparation of sun/shadow diagrams to the satisfaction of the City; and,
  - c. A landscape buffer area shall be provided abutting all lands designated **Low-Rise Residential**.

▼ Map 12.11.A: Kipling Avenue and Highway 7



## 12.12 Bathurst and Centre Street: Thornhill Town Centre

### 12.12.1 Introduction

Development of the Secondary Plan Area will implement the vision for the Thornhill Town Centre at Bathurst and Centre Streets by facilitating its ultimate development as the focal point of residential, commercial and business activity within the Thornhill community. Establishing a new “Main Street” connecting the Town Centre North and Town Centre South is expected to foster the integration of these two components into a unified whole which will be recognizable as the key ‘place’ in the Thornhill community.

Although the vision for the ultimate development of the Thornhill Town Centre north of Centre Street is expressed in terms of a mix of uses and densities, the First Phase of Development will take the form of a shopping centre with surface parking and pedestrian oriented uses along “Main Street”, flanked by high density residential development to the west.

### 12.12.2 Development Principles and Objectives of the Plan

12.12.2.1. The following principles express the fundamental premises and goals for development within the Thornhill Town Centre, identified on Map 12.12.A:

#### Growth Management

- a. Planning and development will proceed in a comprehensive and progressive manner that anticipates the needs of the forecast resident population. The objectives associated with this principle, to be achieved by the time of the ultimate development of the lands, are:
  - i. Within the Thornhill Town Centre north of Centre Street, to accommodate a concentration of residential and commercial development, including at least 600 to 900 housing units to accommodate a resident population of approximately at least 1200 - 1800 persons, and commercial development with an employee population of at least 500 - 600 persons, with the goal of achieving a resident-to-employee ratio of more than 1:1;
  - ii. To provide the Thornhill Town Centre north of Centre Street with a structure of public streets and development blocks as shown on Map 12.12.B which protects the opportunity for future development and intensification to continue consistent with the vision for the Thornhill Town Centre;
  - iii. To require all development to proceed on the basis of full services, logically extended in keeping with the availability of servicing capacity, and designed

- and built at a scale which ensures that the forecast population and employment growth estimates noted in 11.6.2.1.a.i can be accommodated;
- iv. To ensure that the municipality promotes and participates in the early development of the key public sector elements needed to achieve the vision for the Thornhill Town Centre, including the making of arrangements satisfactory to the City regarding the ultimate establishment of:
    - A. Public transit services, and associated infrastructure and facilities;
    - B. A network of public road rights-of-way, as identified on Map 12.12.B;
    - C. On-street parking on Main Street and any local roads within the Thornhill Town Centre north of Centre Street;
    - D. A pedestrian promenade with a high-quality streetscape treatment along the “Main Street’ identified on Map 12.12.B; and,
    - E. Enhanced parks/urban squares for passive recreation;
  - v. To plan well in advance, in cooperation with the Region of York, to ensure that infrastructure improvements at both the local and regional level are planned on a comprehensive basis, and built in time to address the needs of the Thornhill Town Centre;
  - vi. To provide high quality development within the Thornhill Town Centre by:
    - A. Encouraging and attracting private investment within the Thornhill Town Centre;
    - B. Encouraging a range and mix of residential and employment generating land uses; and,
    - C. Establishing simple development rules in order to encourage, rather than discourage, development that meets the objectives of this Secondary Plan;
  - vii. To ensure that the costs of private development are fully borne by, shared equitably among, and recovered from benefiting landowners;

### **Urban Form**

- b. The ultimate development of the Thornhill Town Centre shall establish an urban form with a mix of residential, commercial, institutional, parks and open space, and employment uses, concentrated and arranged in a manner which encourages travel on foot and by other modal alternatives to the car, and which fosters and facilitates public transit ridership. The objectives associated with this principle are:

- i. To enhance the Thornhill Town Centre as a focus for the Thornhill community, building on a principal “Main Street” connecting the Town Centre North and Town Centre South;
- ii. To facilitate the development of the Thornhill Town Centre in a comprehensive manner;
- iii. To promote the evolution and growth of the Thornhill Town Centre as a focal point of higher density residential and commercial development; and
- iv. To promote and encourage pedestrian activity throughout the Thornhill Town Centre and to other parts of the Thornhill Town Centre, by ensuring that development is safe, attractive, conveniently accessible, and human in scale;

### **Built Form and Public Realm**

- c. The Thornhill Town Centre will be developed in a manner that reflects the policies of this Secondary Plan as they apply to the public realm and private lands, in order to create an attractive, safe and pedestrian-friendly urban environment. The objectives associated with this principle are:
  - i. To promote high quality design within the area including streetscapes, open spaces, public buildings and infrastructure, and private buildings, in order to create a comfortable, usable, understandable, and memorable neighbourhood/community;
  - ii. To create a strong community image by enhancing the character of the built environment including building design and massing, signage, planting and streetscapes;
  - iii. To provide a sensitive transition between the concentration, mix and massing of uses and buildings within the Thornhill Town Centre and the low density community outside of the Thornhill Town Centre;
  - iv. To ensure that neighbouring developments within the Amendment Area and adjoining it are physically compatible and complementary;
  - v. To create a pedestrian-friendly environment through the layout of streets, configuration of lots, and siting of buildings and their entrances;
  - vi. To ensure that the retail commercial development on “Main Street” is planned and developed in a manner which is supportive of a street-related, pedestrian-friendly environment;
  - vii. To support the viability of transit by coordinating land use, transportation infrastructure and urban design in a mutually complementary manner

supporting and encouraging pedestrian oriented and transit-supportive development; and,

- viii. To develop attractive streetscapes through attention to the design of the public realm, built form, and the relationship between buildings, streetscapes and other public areas, based on application of the following principles:
  - A. Ensure that development adjacent to the “Main Street” is designed to establish a comfortable, human-scale environment for pedestrians;
  - B. Encourage pedestrian travel throughout the Thornhill Town Centre through establishment of a network of streets, creation of a comfortable, safe and attractive walking environment, and connectivity to parks, public buildings and facilities and commercial uses;
  - C. Provide a consistent level of streetscape design, lighting, planting, signage, street furniture and other amenities;
  - D. Ensure that all public and private areas are designed in a manner which is safe, secure, and subject to informal surveillance, including walkways, building entrances and parking areas;
  - E. Establish a strong relationship between buildings and the street by minimizing setbacks and orienting main entrances to public sidewalks and on-street parking (except for the First Phase of Development, within the area designated High-Rise Mixed-Use); and,
  - F. Design service and parking facilities to complement the pedestrian system and enhance the attractiveness of the public realm;

### **Natural Environment**

- d. Development shall occur in a manner consistent with the City’s philosophy of ecosystem planning, which provides for the protection and conservation of the quality of environmental resources. The objectives associated with this principle are:
  - i. To provide for a system of stormwater management for both water quality and quantity, through the application of state-of-the-art Stormwater Management Practices, designed to protect lands and receiving waters within and outside the Thornhill Town Centre from detrimental effects such as erosion, flooding and sedimentation resulting from increased levels and rates of runoff;
  - ii. To ensure that environmental impacts, are addressed in an ecologically

- responsible manner, consistent with the City's environmental policies;
- iii. To encourage environmental remediation measures within new developments, where appropriate;
  - iv. To encourage the use of passive and active renewable energy sources, where appropriate;
  - v. To encourage the use of 'green' technologies in the design of new buildings and infrastructure, where appropriate; and,
  - vi. To reduce vehicular dependency by locating residential and work related uses in proximity to one another in order to encourage residents to both live and work in the vicinity, and by providing easy access to public transit;

### **Residential Development**

- e. Development of higher density housing forms and types will be encouraged, in order to provide households of varied ages, incomes and demographic composition with choices and lifestyle opportunities. The objectives associated with this principle are:
  - i. To create an adequate supply of housing, including a variety of mid-rise, high-rise and mixed-use housing options, unit types and sizes;
  - ii. To encourage opportunities for residents to live and work in the Thornhill Town Centre, through encouragement of appropriate mixed-use residential/commercial development, and enabling home-based employment; and,
  - iii. To encourage development of housing for seniors throughout the Thornhill Town Centre;

### **Commercial Development**

- f. Development of a broad range of retail and office commercial uses will be encouraged within the Thornhill Town Centre. This commercial development will provide increased opportunities for business competition, and a wide variety of commercial goods and services at competitive costs and prices to meet the needs of Thornhill residents. The objectives associated with this principle are:
  - i. To promote the development of a vibrant commercial shopping district focused on the "Main Street" identified on Map 12.12.B. It is required that development abutting "Main Street" will be street-related and pedestrian-friendly, in order to create an attractive shopping precinct at a human scale;
  - ii. To establish the commercial shopping district as an integral component of a

- pedestrian-friendly urban environment which is conveniently accessible by public transit, bicycle, and on foot from the surrounding community;
- iii. To direct auto-oriented retail uses, such as service stations and drive-through businesses to locations outside the Thornhill Town Centre;
  - iv. To discourage large format, single storey retail uses from locating within the Thornhill Town Centre (except for the First Phase of Development within the lands designated High Rise Mixed-Use) and to redirect these forms of retail development elsewhere within the City of Vaughan; and,
  - v. To ensure that the Thornhill Town Centre develops in a manner consistent with the vision for the Thornhill Town Centre and the phases of development contemplated by this Secondary Plan.

### **Parks and Open Space**

- g. Parks will be developed, where appropriate, within the Thornhill Town Centre south of Centre Street, and within the Thornhill Town Centre north of Centre Street (except for the First Phase of Development), to satisfy the passive recreational needs of Thornhill Town Centre residents, to enhance the aesthetic appeal of the Town Centre and to provide connections to the park and open space system in the surrounding community. The objectives associated with this principle are:
  - i. To establish urban parks where appropriate and passive recreation opportunities are available to address the needs of the anticipated resident and worker population within the Thornhill Town Centre;
  - ii. To plan and design open space linkages that facilitate continuous, uninterrupted pedestrian and cycling movement within the Thornhill Town Centre, and to the surrounding community;
  - iii. To plan and design parks that provide central public green spaces within the Thornhill Town Centre to address the recreational needs of residents, employees and visitors; and,
  - iv. To establish a hierarchy of parks to meet the varying social and recreational needs typical of an urban centre;

### **Institutional Uses and Facilities**

- h. The opportunity for a range of institutional services and facilities will be provided within the Thornhill Town Centre, to address the needs of Town Centre residents and the surrounding community. The objectives associated with this principle are:

- i. To encourage and assist institutional users and public agencies in securing suitable sites and space to enable them to serve the community's social, cultural, education, government, health care and religious needs;
- ii. To ensure that the institutional uses are compatible with and complementary to neighbouring uses and the surrounding community;
- iii. To encourage major institutional users to locate on sites conveniently accessible to pedestrians and via the public transit network;
- iv. To ensure convenient access for all residents to essential community services, parks, day care, recreation facilities, other institutional uses, and public transit services; and,
- v. To encourage establishment of social services, parks, public facilities and public transit services in locations which are conveniently accessible, particularly to individuals and households with more limited mobility, including seniors, families with young children, and the physically challenged.

### **Transportation and Transit-Supportiveness**

- i. An efficient transportation network is needed to serve the Thornhill Town Centre, and the surrounding community, which permits efficient vehicular movement while encouraging travel by non-auto modes, including on foot, bicycle and public transit. The objectives associated with this principle are:
  - i. To develop a network of continuous and interconnected public arterial, collector and local streets and laneways, complemented by a public transit network, facilitating efficient movement by all modes of transportation within the Thornhill Town Centre, and connecting with the surrounding community;
  - ii. To develop a walkable, pedestrian-friendly environment, which is well connected to public facilities, parks and commercial areas, To encourage travel via non-auto modes by establishing development which is safe, attractive, and human in scale, in keeping with the urban design and streetscaping objectives of this Secondary Plan;
  - iii. To encourage a significant increase in public transit ridership to reduce traffic congestion, energy costs and pollution, and foster greater efficiency in the operation of the transportation system;
  - iv. To support the viability of public transit by planned and coordinated land use, transportation infrastructure and urban design;
  - v. To encourage commercial and residential development at a density sufficient

- to reinforce the viability of local and regional transit;
- vi. To encourage development to become increasingly transit-supportive in terms of land use, density and urban design as the Thornhill Town Centre evolves over time;
  - vii. To provide on-street parking on roads throughout the Thornhill Town Centre to support street related building entrances, and retail uses that are accessible to both pedestrians and drivers;
  - viii. To promote shared and structured parking, and permit higher lot coverage in specific areas, in order to minimize areas devoted to surface parking;
  - ix. To encourage travel on foot and bicycle, by providing safe and convenient pedestrian and bike networks on public streets connected to parks, public facilities and commercial uses;
  - x. To design streets to enhance public safety and reduce traffic speeds;
  - xi. To reduce the need for commuter travel by encouraging a closer relationship between the workplace and the home;
  - xii. To acquire and protect road rights-of-way and transit rights-of-way that make up the network of collector and local roads and support the urban design and streetscaping policies of this Secondary Plan; and
  - xiii. To support the provision of transit priority measures for transit services on Centre Street and Bathurst Street.

### **12.12.3 General Land Use Policies**

The Secondary Plan for the Thornhill Town Centre provides land use policies that are in keeping with its vision as the pre-eminent focus of commercial and residential development in the Thornhill community. The Thornhill Town Centre is intended to become a highly desirable and attractive mixed-use urban area. The preferred form of development is intended to support public transit services and provide a comfortable pedestrian environment. The central location of the Thornhill Town Centre affords a unique opportunity to connect and unify the Thornhill Town Centre via a new “Main Street’ in keeping with the Town Centre vision.

- 12.12.3.1. That the policies contained in this Secondary Plan shall apply to the lands shown on Map 12.12.A and Map 12.12.B as the “Thornhill Town Centre”. Except as otherwise provided herein, the policies of this Secondary Plan shall supersede the policies of any

other area or specific Official Plan Amendment which is in force in the City on the date of the approval of this Secondary Plan.

### **Population and Employment Growth**

- 12.12.3.2. It is intended that the Thornhill Town Centre accommodate a mix of residential and commercial development. North of Centre Street within the Thornhill Town Centre, ultimately development will provide opportunities for people to choose to both live and work in the area, including the provision of live/work units.

### **Retail Commercial Development**

- 12.12.3.3. Within the Thornhill Town Centre north of Centre Street, a significant retail shopping component is expected to be established on the site, with “Main Street” being developed as a pedestrian-friendly shopping precinct and focus for the surrounding development. Retail development will primarily be established at grade, but on “Main Street”, second floor retail uses will also be permitted. The first phase of such development will be in the form of a shopping centre, with surface parking, to the east and west of the “Main Street” shopping precinct.

### **Mixed-Use Development**

- 12.12.3.4. Ultimately, development north of Centre Street may include a mix of retail, office and residential uses, either in single use buildings or mixed-use buildings. Mixed-use buildings will be encouraged to develop throughout most of the Thornhill Town Centre as the lands intensify over time.

### **Prohibited Development**

- 12.12.3.5. The following types of development are not permitted within the Amendment Area:
- a. Drive-through facilities associated with any permitted use;
  - b. Service Station uses and car washes; and,
  - c. Retail uses that require outdoor storage, with the exception of garden centres that are accessory to a primary use.

### **Development Density**

- 12.12.3.6. It is intended that the density of development within the Thornhill Town Centre will be controlled through the use of a Floor Space Index (FSI). A Floor Space Index is a ratio

of the amount of permitted development (measured as gross floor area) in relation to the size of the development lot.

- 12.12.3.7. The area included in the calculation of residential density shall include the land for the buildings, private roads and driveways, parking areas and landscaping, and amenity areas related to the specific development, but shall exclude all other lands.
- 12.12.3.8. Wherever used in this Secondary Plan, the words “First Phase of Development” shall mean, development within the Thornhill Town Centre north of Centre Street up to a maximum gross floor area of 34,373 square metres (370,000 square feet) within the lands designated Mid-Rise Mixed-Use and High-Rise Mixed-Use.

#### 12.12.4 Mid-Rise Mixed-Use

- 12.12.4.1. The **Mid-Rise Mixed-Use** designation provides a lively and active, pedestrian-related, retail shopping environment fronting on “Main Street”. To support visual attractiveness, a consistent image, and a high quality design and development related and scaled to the pedestrian are required for all lands subject to this designation.
- 12.12.4.2. The development potential in the “**Mid-Rise Mixed-Use**” designation provides the opportunity to support existing and planned public transit services within an easy walk of Centre Street. Ultimately, residents and employees located within the designation will also support the retail shopping precinct on “Main Street’.
- 12.12.4.3. That notwithstanding policy 9.2.1.1 the following uses shall be permitted within **Mid-Rise Mixed-Use** areas identified on Map 12.12.A (which includes all properties fronting “Main Street’):
  - a. Residential Units;
  - b. Home Occupations (provided the residential component of the live/work unit is located above-grade in a mixed-use building);
  - c. Retail uses, including department stores and supermarkets, personal service establishments, restaurants and entertainment facilities, doctor and dental offices, veterinary clinics, and banks and financial institutions;
  - d. Office Uses;
  - e. Places of worship; and,
  - f. Community and institutional uses, including public health facilities, all types

of special needs housing, cultural and social facilities, non-profit clubs and organizations, public utilities, and community facilities such as libraries and day nurseries, provided they are located above-grade in a mixed-use building.

- 12.12.4.4. That the following building types are permitted, pursuant to Section 9.2.3:
  - a. Mid-Rise buildings;
  - b. Low Rise buildings;
  - c. Stacked Townhouses; and,
  - d. Public and Private Institutional buildings
- 12.12.4.5. That notwithstanding Section 9.2.1.9 stormwater management facilities shall not be permitted in Mid-Rise Mixed-Use areas.
- 12.12.4.6. That offices (with the exception of doctor and dental offices and veterinary clinics), residential, community and institutional uses are not permitted in stand-alone, single use buildings and are permitted only if located above grade in a mixed-use building, with the exception of places of worship.
- 12.12.4.7. Map 12.12.A identifies a “Main Street”. It is the intent of the City that this “Main Street” be developed with predominantly small scale retail and service uses at-grade, with a maximum unit ground floor size of 350 square metres (3,767 square feet). There is no unit size restriction for uses located on storeys above the ground floor.
- 12.12.4.8. Notwithstanding the foregoing, two larger scale units are permitted as at grade units on “Main Street”, provided they do not need exceed a ground floor area of 1,162 square metres (12,500 square feet) and 557 square metres (6,000 square feet), one located on the west side of “Main Street” south of the planned east/west road, and one located on the east side of “Main Street” north of the planned east/west road, respectively.
- 12.12.4.9. To create the desired character and environment on lots abutting “Main Street”, buildings and their primary public pedestrian entrances shall face the “Main Street” and shall be located close to the front lot line. All buildings on lots abutting the “Main Street” shall provide retail and service uses at-grade.

- 12.12.4.10. That notwithstanding policy 9.2.1.5 the maximum height along the “Main Street” frontage shall be 4 storeys with a permitted step back from the street to 6 storeys (with the exception of corner buildings);
- 12.12.4.11. Notwithstanding policy 11.6.4.8 the minimum height of buildings on “Main Street” is 2 storeys.
- 12.12.4.12. Corner buildings shall be a minimum of 2 storeys and a maximum of 8 storeys or 29 metres.
- 12.12.4.13. That notwithstanding Section 9.2.1.6 the maximum Floor Space Index is 2.0 FSI.
- 12.12.4.14. Within the Thornhill Town Centre north of Centre Street, the first phase of development shall include the construction of “Main Street”, and the construction of buildings fronting onto both sides of “Main Street” concurrent with construction of the large format, retail building at the eastern limit of the property.
- 12.12.4.15. All buildings shall be sited close to the “Main Street” right-of-way and create an attractive and interesting public realm.
- 12.12.4.16. All individual retail uses in buildings on lots abutting the “Main Street’ shall be in buildings limited in size to a maximum of 2,500 square metres of ground floor area.
- 12.12.4.17. The **Mid-Rise Mixed-Use** designation along “Main Street” shall have a minimum depth of 45 metres.
- 12.12.4.18. Parking for uses within the **Mid-Rise Mixed-Use** designation may be provided within the area designated **High-Rise Mixed-Use**; for the purpose of providing parking to satisfy the City’s parking standards, the lands west of “Main Street’ are required to provide the parking required for the development located west of “Main Street’; and, similarly, the lands east of “Main Street’ will provide the parking required for the development located east of “Main Street’.

- 12.12.4.19. Parking for uses within the **High-Rise Mixed-Use** designation may be provided within the area designated **Mid-Rise Mixed-Use**, except for the area of the **Mid-Rise Mixed-Use** designation within 20 metres of the Main Street right-of-way.
- 12.12.4.20. Notwithstanding the permitted uses noted above, one park/urban square shall be permitted within the **Mid-Rise Mixed-Use** designation.

### 12.12.5 High-Rise Mixed-Use

The **High-Rise Mixed-Use** designation provides the opportunity, ultimately, for residential, retail and commercial development, in buildings of single use, or in mixed-use buildings. This designation provides considerable flexibility for development to evolve over time in response to market conditions. Flexibility in density and height will permit buildings as little as a single storey, up to a maximum of fourteen storeys for apartment or office buildings. High quality design is required to give the area attractiveness and a consistent image. In time, a structure of public roads and development blocks will be created that is expected to facilitate gradual intensification of development within this designation over time.

- 12.12.5.1. Notwithstanding policy 9.2.1.1 **High-Rise Mixed-Use** designation encourages mixed-use development, and permits the following:
- a. Residential units;
  - b. commercial uses, including retail, restaurant, service, doctor and dental offices, veterinary clinics, banks and financial institutions and office uses,
  - c. Parks and urban squares;
  - d. Public parking facilities;
  - e. Storm water management facilities;
  - f. Places of worship; and,
  - g. Community and institutional uses, including public health facilities, all types of special needs housing, cultural and social facilities, non-profit clubs and organizations, public utilities, and community facilities such as libraries and day nurseries.
- 12.12.5.2. That the following building types are permitted, pursuant to Section 9.2.3:
- a. High-Rise buildings;
  - b. Mid-Rise buildings;

- c. Low Rise buildings;
  - d. Stacked Townhouses; and,
  - e. Public and Private Institutional buildings.
- 12.12.5.3. Notwithstanding policy 9.2.1.5, the maximum height of all buildings shall be fourteen storeys or 44 metres, whichever is less.
- 12.12.5.4. Notwithstanding policy 9.2.1.6, the maximum FSI is 2.0.
- 12.12.5.5. No individual retail use shall be permitted with a gross floor area in excess of 12,635 square metres (136,002 square feet).
- 12.12.5.6. Parking for uses within the **High-Rise Mixed-Use** designation may be provided within the area designated **Mid-Rise Mixed-Use**, except for the area of the **Mid-Rise Mixed-Use** designation within 20 metres of the Main Street right-of-way; for the purpose of providing parking to satisfy the City's parking standards, the lands west of Main Street are required to provide the parking required for the development located west of Main Street; and, similarly, the lands east of Main Street will provide the parking required for the development located east of Main Street.

## 12.12.6 High-Rise Residential

- 12.12.6.1. A portion of the Thornhill Town Centre on the northeast corner of New Westminster Drive and Centre Street is designated for **High-Rise Residential** use. It is the intention of this Secondary Plan that this area shall be developed with high quality and a consistent level of urban design for the public and private realm.
- 12.12.6.2. Notwithstanding policy 9.2.1.1 following uses are permitted in the **High-Rise Residential** designation:
- a. Residential units, including live-work units; and,
  - b. Parks and public squares.
- 12.12.6.3. The following building types are permitted, pursuant to Section 9.2.3:
- a. High-Rise buildings; and,
  - b. Mid-Rise buildings.

- 12.12.6.4. Notwithstanding policy 9.2.1.5 the maximum height of apartment buildings shall be 18 to 22 storeys, depending upon resolution of matters respecting shadowing, massing and built form, to the City's satisfaction.
- 12.12.6.5. Notwithstanding policy 9.2.1.6 the maximum permitted residential density is 2.7 FSI for apartment buildings, calculated on the land area designated **High-Rise Residential**, excluding road allowances as identified on Map 12.12.B. Within the **High-Rise Residential** designation, variation in the allocation of density to individual land parcels is permitted, provided that the density on the **High-Rise Residential** lands collectively does not exceed 2.7 FSI, and subject to addressing built form, massing, shadowing and amenity space requirements, to the City's satisfaction. Further minor variations may be permitted where appropriate to provide for effective floor space arrangements, subject to the other applicable considerations for development within this designation.
- 12.12.6.6. Notwithstanding Section 9.2.2.5 local convenience commercial uses shall be permitted in **High-Rise Residential** buildings without amendment to this Plan, provided they are designed as an integral part of the building. Local convenience commercial uses include the following uses: bank or financial institution, retail store, business or professional office, personal service shop and video store. Such uses in total shall be limited in size to a maximum of 30% of the ground floor area of the building.

### 12.12.7 Special Policy Area

The "Special Policy Area" designation applies to a portion of the Thornhill Town Centre located south of Centre Street as shown on Map 12.12.A. It is the intention of Plan that, if new development or redevelopment of these lands is proposed which significantly alters the form and function of the existing development, it will facilitate the extension of "Main Street" south of Centre Street to provide an integrated connection between the Town Centre North and Town Centre South. It is also intended that the development of the Town Centre South will occur in a manner which is consistent with the "mixed-use" vision and urban design principles of this Plan.

- 12.12.7.1. Notwithstanding policy 9.2.1.1, the following policies apply within the "Special Policy Area" designation:
- a. Commercial development that is consistent with and permitted by the site-specific

zoning bylaw, may proceed within the “Special Policy Area” without completion of the comprehensive development plan outlined below;

- b. Any development requiring a zoning by-law amendment and which entails a significant change to the form or function of the existing mall (i.e. change in design of building, change to internal road structure, etc.), or any development requiring an Official Plan Amendment; shall be developed in accordance with a comprehensive development plan (or plans) including a transportation component and a comprehensive concept plan which is to be adopted by Council prior to any redevelopment proceeding; and,
- c. Any significant new development or redevelopment plan shall consider the underlying principles and objectives of this Secondary Plan and supporting design policies.

### **12.12.8 Built Form**

The intent of the built form policies is to reinforce the overall urban structure consistent with the ‘vision’ for the Thornhill Town Centre. The built form policies complement and are integrated with the land use policies of the Secondary Plan. Built form policies specify the preferred relationship of buildings to the street through policies governing building heights, massing, frontage, setbacks, and “build-to” zones that define the street edge.

The following built form policies are intended to achieve the ultimate overall vision for the Thornhill Town Centre, some of them being inconsistent with the shopping centre built form permitted as the first phase of development within the Thornhill Town Centre north of Centre Street:

- Buildings (with the exception of the First Phase of Development within the High-Rise Mixed-Use designation) shall be sited to promote positive building-to-street relationships;
- Buildings (with the exception of the First Phase of Development within the High-Rise Mixed-Use designation) shall be located close to the street and sidewalk;
- Buildings (with the exception of a large format store permitted at the Bathurst/ Centre intersection within the High-Rise designation) shall have active architecture. Convenient access onto the site shall be provided from Centre Street for pedestrians and users of public transit;

- When buildings are related to and located at the street, they shall not be separated from the street by either parking areas or drive aisles.
  - Buildings shall be designed to comply with the requirements of the City's Accessibility Guidelines, and the Ontario Building Code respecting access for the physically challenged.
- 12.12.8.1. Buildings shall be sited to minimize their impact on neighbouring properties, respecting and protecting the existing scale and character of adjacent residential neighbourhoods;
- 12.12.8.2. Buildings shall be sited and massed to ensure adequate sunlight, sky views, and to mitigate wind conditions on the street, sidewalk and in open spaces;
- 12.12.8.3. Except for the First Phase of Development within the High-Rise Mixed-Use designation, buildings are to be generally sited parallel to the public street and along the edges of parks and open spaces. The public faces of these buildings are to align with neighbouring buildings to define the street wall in a consistent manner.
- 12.12.8.4. Except for the First Phase of Development within the High-Rise Mixed-Use designation, buildings shall be sited and organized so that streets and parks are overlooked by active building faces. This Plan and the zoning bylaw will establish build-within zones and identify the minimum proportion of frontage within each block face which shall be occupied by buildings. In addition to providing a 'connecting link', a relatively consistent building edge is important to provide spatial definition and containment to the street. "Build-within" zones require buildings to locate their front and exterior side-walls within a defined zone on the lot. The "build-within" zones essentially set both a minimum and maximum setback.
- a. Except for development within the High-Rise Mixed-Use designation in the First Phase of Development, all commercial buildings within the Thornhill Town Centre north of Centre Street shall be developed with a substantial portion of their front and exterior side facades between 1.0 and 3.0 metres of the front lot line and the exterior side lot line; and,
  - b. Except for development within the High Rise Mixed-use designation in the First Phase of Development, to reflect the importance of corner properties and reduce the distance between buildings along Centre Street and Bathurst Street, the "build-within" zone abutting the Regional road sight triangle will be between 0.6 and 2.0 metres.

### **Minimum Built Frontage**

- 12.12.8.5. A street wall or part of the building that is closest to the public street provides important spatial definition and a sense of enclosure for that street. This is critically Important along the pedestrian-oriented “Main Street” identified on Map 12.12.A. A minimum amount of active, transparent and articulated building wall located within the build-within zone shall be required, as follows:
- a. On all lands adjacent to the “Main Street” identified on Map 12.12.A, the minimum built frontage shall be 75 percent of the developable block face of each block;
  - b. On all lands adjacent to local roads that generally run north-south, the minimum built frontage shall be 75 percent of the developable block face of each block;
  - c. On all lands adjacent to Local Roads that generally run east-west, the minimum built frontage shall be 50 percent of the developable block face of each block; and,
  - d. On all lands adjacent to Centre Street and Bathurst Street, the minimum built frontage shall be 50 percent of the developable frontage.

### **Corner Buildings**

- 12.12.8.6. The advantages of better visibility, light and view, make corner sites good locations for landmark buildings. Corner sites are consequently often occupied by prestigious buildings, or by buildings of community status. In the Thornhill Town Centre, corner sites will play particularly significant roles in defining landmarks. It is therefore important that the treatment of the corner sites be consistent throughout the Plan. The following policies apply to corner sites:
- a. Corner designs shall enhance the presence of each corner building;
  - b. With the exception of the large format building at the northwest corner of Bathurst Street and Centre Street, buildings should ‘turn the corner’ i.e. they should have articulated, active and transparent facades towards both streets and should be visually distinguishable from adjacent development; and,
  - c. Corner buildings should have a high level of architectural detailing and a distinct architectural appearance.
- 12.12.8.7. Service areas and access to underground parking shall be appropriately integrated into the buildings they serve, and located so as to minimize physical and visual impact on public sidewalks and open spaces. Service areas shall not be located between any building and any public street.

- 12.12.8.8. Except for the First Phase of Development, reverse lot development (residential and/or commercial) will not be permitted along Centre Street. Where parking access is located off the street by way of internal driveway circulation system, the units should still have a functional primary pedestrian entrance that relates to the street.
- 12.12.8.9. The area between the building wall and the street should contribute to the positive social and visual environment of the street.
- 12.12.8.10. Buildings should be designed with a high level of architectural resolution and materials.
- 12.12.8.11. Blank walls and monotonous facades should be avoided, particularly along the “Main Street”, Centre Street and Bathurst Street, owing to their high degree of visibility in presenting the “face” of the Town Centre to the passing public. Variation in vertical and horizontal articulation shall provide façade treatment and detailing, including lighting, awnings, and except for the large format retail buildings within the High-Rise Mixed-use designation, windows and primary entrances.
- 12.12.8.12. Buildings should be vertically articulated into base, middle and top section, and horizontally articulated to break up building mass along a street face.
- 12.12.8.13. Architectural variation and animation should be encouraged and include building elements such as balconies, windows, porches, railings, entrances, awnings and attractive signage where appropriate.
- 12.12.8.14. Where the grade changes over the length or width of the site, accommodation for the grade changes, if required, shall be accomplished on the site in such a manner as to avoid substantial differences in the level of primary entrances and the public sidewalk in regard to buildings constructed along the site edges.
- 12.12.8.15. Surface parking should have the least possible impact on the public realm; except for the First Phase of Development, large surface parking areas are discouraged; and, where large surface parking areas are provided, they should be setback, screened and buffered from the street edge, with comfortable pedestrian routes provided through and around parking areas.

- 12.12.8.16. Ramps to underground parking should be located within the building
- 12.12.8.17. Service and loading areas are prohibited adjacent to public streets and must, at all times, be effectively screened and, if possible, fully enclosed.
- 12.12.8.18. Garbage shall be stored within buildings.
- 12.12.8.19. Urban Design Guidelines and Architectural Control Guidelines shall be prepared by development applicants, and shall address both built form and public realm considerations to the satisfaction of the City. Such guidelines shall be prepared in regard to the shopping centre form of development permitted by this Secondary Plan within the First Phase of Development, with a further set of guidelines to be prepared for development in excess of the First Phase of Development.

### **12.12.9 Streetscape and Public Realm**

The following policy framework is intended to create a unified public realm and streetscape along Centre Street and within the Town Centre. Although some elements are addressed separately, the success of the overall vision will be through the careful implementation of these elements into an interconnected whole over time.

The public realm includes many separate elements that, when brought together in a comprehensive and consistent approach can create an attractive, high quality urban space and enhance the character of the neighbourhood. The connectivity of the Thornhill Town Centre will be addressed over time through an integrated network of streets, pedestrian systems, bicycle routes, transit facilities, and weather protection which collectively create a pedestrian-friendly environment. Centre Street and Bathurst Street play key roles in making connections to the community and in presenting the Town Centre to the public, owing to the high level of visibility and accessibility they afford to the Town Centre.

The community surrounding the Town Centre is well served by an active park system. Over time, the City of Vaughan shall encourage the further expansion and improvement of the public realm with street plantings, promenades, small, passive urban-scale parks and/or squares, private courtyards and gardens: The City shall also seek opportunities to improve linkages between existing parks and open spaces nearby, and the Thornhill Town Centre.

- 12.12.9.1. Public realm conditions should support year-round and day and night activities.
- 12.12.9.2. Streetscape improvements, linked by a common theme and selection of materials, shall be made to both sides of each street to enhance the identity of place, pedestrian comfort and safety. In this regard, Bathurst Street and Centre Street are priority areas. Streetscape improvements shall include:
  - a. Paving and soft landscaping;
  - b. Street furniture;
  - c. Pedestrian-scale and street lighting; and,
  - d. Signage.
- 12.12.9.3. Pedestrian weather protection shall be provided along the facades of buildings where entrances are located.
- 12.12.9.4. For development within the High Rise Residential designation, a landscaped strip, 6.0 metres in width, shall be provided, within and along the northern edge of the Thornhill Town Centre north of Centre Street, from New Westminster Drive to the eastern limit of the High Rise Residential designation.
- 12.12.9.5. For the First Phase of Development, a landscaped strip, 3.5 metres in width, shall be provided, within and along the northern edge of the Thornhill Town Centre north of Centre Street, from the western limit of the High Rise Mixed-use designation to the western boundary of the synagogue property. Within the 3.5 metre landscape strip, the landscaping shall be significantly enhanced. For development in excess of the First Phase of Development, this landscape strip shall be 4.5 metres in width.
- 12.12.9.6. Mature trees within the public and private realm shall be preserved wherever possible. A tree preservation plan must be submitted with all development or redevelopment applications. Soft landscaping shall be maximized and hard surfacing minimized, with the exception of well-designed and planted plazas, forecourts, patios, and streetscapes associated with “Main Street” and Centre Street.
- 12.12.9.7. Public spaces shall be designed to be inviting to the public, well lit, of a comfortable pedestrian scale, and complementary to the architecture of adjacent buildings.

- 12.12.9.8. High-quality architectural elements such as railings, pedestrian lighting and tree gates shall be provided.
- 12.12.9.9. The pedestrian environment and connections both in the public realm (e.g. streets and sidewalks) and within the private realm shall be improved in the following ways:
- a. Design blocks at a pedestrian scale rather than an automobile scale except for development on the High-Rise Mixed-use lands in the First Phase of Development;
  - b. Provide mid-block connections to enhance pedestrian circulation;
  - c. Provide for public safety and comfort through a coordinated sidewalk and street furniture program, including adequate lighting (With reference to the City of Vaughan Draft Outdoor Lighting Design Guidelines), seating, wind and rain protection, bicycle parking, and enhanced and identifiable pedestrian crossing points;
  - d. Provide additional planting of street trees and plantings on private lands;
  - e. Provide buildings at the street with the primary access from the street. Except for the First Phase of Development, other than as specifically provided elsewhere in this Secondary Plan for “Main Street”, the primary access from the street to a commercial building shall be directly connected to the street in such a manner as to avoid substantial differences in the level of the primary entrance and the public sidewalk;
  - f. Ensure proper maintenance of public and private walkways;
  - g. Provide bicycle parking facilities in commercial, public open space and public transit areas to which bicycles may be locked;
  - h. Ensure that development complies with the requirements of the City’s Accessibility Guidelines to ensure acceptable access for the physically challenged;
  - i. Open spaces, streets and walkways should be interconnected and linked to important destinations within the community and outside;
  - j. Pedestrian circulation spaces should be well buffered from moving cars and noise, and have sufficient sunlight penetration for enjoyment year-round;
  - k. Visual screening and buffering should be provided to adjoining parking, loading, underground parking and servicing areas; and,
  - l. Except for the First Phase of Development, other than as specifically provided elsewhere in this Plan for “Main Street”, the street-related storeys of buildings should further enhance the quality of pedestrian open space and circulation within

the public realm through building transparency, windows, primary entrances, engaging architectural articulation and pedestrian-scaled detail.

### **12.12.10 Main Street**

- 12.12.10.1. Development on “Main Street” shall be characterized by the following five elements:
- a. buildings that maintain a consistent build-within zone and frontage to define a street wall; buildings on both sides of the street that are oriented to the “Main Street”; buildings that face the “Main Street” at the first floor with the primary facade; and buildings with primary entrances that front onto the “Main Street”, and that are directly connected to, the street in such a manner as to avoid substantial differences in the level of the primary entrance and the public sidewalk;
  - b. A complementary relationship between pedestrian, vehicular and transit circulation systems where conflicts are minimized;
  - c. Highly articulated buildings with distinctive architectural expression and massing designed to provide an attractive pedestrian-scaled public face despite large parcel land ownership;
  - d. Buildings designed with high ground floor-to-ceiling heights (3.5 metres minimum) in order to accommodate a range of programs (retail, restaurant, institution, etc.) and the hierarchical distinction between ground, middle and upper storeys architecturally expressed; and,
  - e. A break in the buildings along “Main Street” at the first floor shall be provided every 35 metres for the purpose of a pedestrian connection (either enclosed or unenclosed), in the form of sidewalks and stairs, open to the public at all times, connecting “Main Street” to the parking areas behind the “Main Street” buildings.
- 12.12.10.2. The greatest building heights shall be permitted at the key corner locations, with corner sites defined by more intense development and high quality architecture, to serve as landmarks or gateway entry points.
- 12.12.10.3. Development of mixed-use buildings is encouraged in order to help facilitate live-work opportunities and a lively and active district.
- 12.12.10.4. Extensive landscaping treatment and outdoor features should be provided to encourage and facilitate pedestrian activity.

- 12.12.10.5. Except for the First Phase of Development, parking shall be located below-grade wherever possible. Any above-grade parking garages should be faced with active uses.
- 12.12.10.6. Sharing of parking in mixed-use developments is encouraged.
- 12.12.10.7. On-street parking shall be provided on “Main Street”.
- 12.12.10.8. Publicly accessible, privately owned open space should be provided in the Town Centre (except for the First Phase of Development) in order to complement the public realm, support development and provide connectivity throughout the area.
- 12.12.10.9. The street-related storeys of buildings on “Main Street” should further enhance the quality of pedestrian open space and circulation within the public realm through building transparency, windows, primary entrances, engaging architectural articulation and pedestrian-scaled detail.

### **12.12.11 Local Streets**

- 12.12.11.1. Except for the First Phase of Development, (or as otherwise provided for “Main Street”), a network of local streets north of Centre Street shall be provided, as illustrated on Map 12.12.B of this Plan. An inter-connected network of streets minimizes distances for walking, cycling and vehicular movements, and facilitates more efficient traffic circulation. Streets with sidewalks and pedestrian paths offer direct, convenient access to transit services and throughout the Thornhill Town Centre north of Centre Street.
- 12.12.11.2. Local streets are relatively narrow to slow traffic and reduce speeds to levels more compatible with children, pedestrians and cyclists. On-street parking is usually provided on local streets, reducing speeds and buffering pedestrians from moving traffic.
- 12.12.11.3. The street network also establishes blocks of sufficient depth to accommodate buildings fronting onto the local streets, with parking to the rear or side. This reduces the visual impact of surface parking areas, and allows for intensification of buildings and parcel development over time.

### **12.12.12 Parks and Open Spaces**

- 12.12.12.1. The following Parks policies apply to the Thornhill Town Centre:

- a. The City may acquire lands anywhere within the Thornhill Town Centre for the purposes of establishing parkland. The parkland designated by this Secondary Plan shall be shown on draft plans of subdivision. Their actual size, configuration and design will be subject to confirmation by the City during the development approval process;
- b. Lands utilized for stormwater management facilities shall not be considered as any part of the parkland dedication requirement provided via the Planning Act.;
- c. All development applications may be required to identify a potential location for a public square, to the satisfaction of the City; and,
- d. Public squares are intended as formal spaces for passive recreation, in support of the adjacent development. Public squares should address the following design guidelines:
  - i. Public squares are to be planned as focal points;
  - ii. Relationships with adjacent buildings should provide optimal sunlight penetration, skyview and wind conditions;
  - iii. Streetscapes and buildings that abut a public square should be designed to reinforce an interactive relationship between the open space and its adjacent land use;
  - iv. Buildings should front onto a public square to create built form edges to the public space;
  - v. The landscape along the street frontage, including high canopy street trees, should be complementary on both sides of the street;
  - vi. Entry and access points should be located conveniently and incorporate civic design themes; and,
  - vii. hard and soft landscape elements and features shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.

### 12.12.13 Municipal Services, Utilities and Infrastructure

- 12.12.13.1. That urban water and sewage capacity shall be allocated by the City of Vaughan sufficient for the First Phase of Development of the Thornhill Town Centre north of Centre Street, inclusive of the High-Rise Residential Area designation and the First Phase of Development within the **Mid-Rise Mixed-Use** and the **High-Rise Mixed-Use** designations.

- 12.12.13.2. A Master Servicing Strategy outlining preliminary functional water distribution, wastewater collection and stormwater management facilities, including designs and costs, will be prepared by proponents to the satisfaction of the City and the Region prior to approval of development applications; the associated details of the Strategy and costs shall be provided as a condition of approval of development applications.
- 12.12.13.3. The precise operation, dimensions and design of all required servicing facilities will be determined and addressed as part of the Master Servicing Strategy.
- 12.12.13.4. New infrastructure, including roads, site grading, sanitary and stormwater management facilities, municipal water and electricity supplies, required to serve proposed development, shall be provided concurrent with the rate of development, and shall not place a financial burden on the City.

#### 12.12.14 Transportation

The general intent is to provide the resident and business community with a transportation network which is safe, convenient, affordable, efficient and energy-conserving while minimizing environmental impacts. To accomplish this intent, transportation infrastructure and services within the Amendment Area must be integrated with the broader existing and planned road and public transit network serving the surrounding development and community.

##### **Collector Road and Laneway Network**

- 12.12.14.1. The road network includes a collector road ('Main Street') and local roads and laneways. "Main Street" is required as part of the first stage of the first phase of development of the lands designated **Mid-Rise Mixed-Use** and **High-Rise Mixed-Use**. Map 12.12.B identifies the road network required for development on the lands designated **Mid-Rise Mixed-Use** and **High-Rise Mixed-Use** in excess of a gross floor area of 0.5 times the Site area (0.5 FSI), excluding the GFA of the second storeys of "Main Street" buildings up to a maximum exemption of 5,600 square metres GFA(60,000 square feet) in the calculation of the 0.5 FSI. The right-of-way widths are 26.0 metres for "Main Street", and generally 20.0 metres for local roads and 7.5 metres for laneways. The following policies apply to the road network:

- 12.12.14.2. The road allowance widths identified in this Plan are approximate, and the final right-of-way requirements and the provision of daylight triangles, and left turn lanes will be determined during the review of development applications.
- 12.12.14.3. Prior to any development or redevelopment within the Thornhill Town Centre, applicants will prepare a detailed traffic impact study to the satisfaction of the City and the Region, which will address in part, but not limited to capacity, access, function, neighborhood infiltration, transportation and transit objectives. With the exception of the 26-metre right-of-way for “Main Street”, the road allowance widths identified in this Plan are approximate, and the final right-of-way requirements and the provision of daylight triangles, and left turn lanes will be determined during the review of development applications.
- 12.12.14.4. Notwithstanding the foregoing, for the Thornhill Town Centre north of Centre Street, no detailed traffic impact study will be required by the City in regard to the First Phase of Development in accordance with the other policies of this Secondary Plan. Future development which proposes to increase total retail commercial GFA to more than this threshold, will be required to complete the traffic impact study described in the paragraph above.
- 12.12.14.5. One collector road -“Main Street’ -is identified in the Secondary Plan with a right-of-way of 26 metres. Its location is established by the location of the existing intersection of Centre Street and North Promenade to the south, and the planned north/south road in the development to the north, to which it will connect. “Main Street’ is the key north/south road connecting the Town Centre North and Town Centre South via a signalized intersection with Centre Street. As the focus of a pedestrian-oriented shopping precinct, “Main Street” shall include on-street parking on both sides and be sized within the road allowance to accommodate public transit vehicles. It is expected to receive the highest level of landscaping, streetscaping and design treatment in keeping with its key role in the success of the “Main Street” development.
- 12.12.14.6. With the exception of the “Main Street” connection identified on Map 12.12.B, the remaining road network is conceptual in nature, with the final locations of roads and intersections to be determined through the development approval process for

development in excess of the gross floor area limit specified in policy 12.12.14.1, to the satisfaction of Vaughan Engineering and Public Works.

- 12.12.14.7. It is a requirement of this Secondary Plan that all roads, as identified on Map 12.12.B, shall be built and maintained to an operational standard satisfactory to the City, and shall provide for permanent public access for traffic through the Thornhill Town Centre.
- 12.12.14.8. The road network illustrated on Map 12.12.B is fundamental to the efficient functioning of the Thornhill Town Centre transportation network for development in excess of a gross floor area limit specified in policy 12.12.14.1; laneways and road rights-of-way shall be identified in all approved plans within the Thornhill Town Centre for development in excess of a gross floor area limit specified in policy 12.12.14.1. For the Thornhill Town Centre north of Centre Street, with the exception of “Main Street”, the road allowances for the roads shown on Map 12.12.B will be conveyed to the City and will be dedicated and built by the owner to the City’s satisfaction when the gross floor area of development on the lands north of Centre Street designated **Mid-Rise Mixed-Use** and **High-Rise Mixed-Use** equals a gross floor area limit specified in policy 12.12.14.1.
- 12.12.14.9. No plan shall be approved which permits the establishment of any structure, except private or public infrastructure which is permitted, on any road allowance or laneway identified on Map 12.12.B. The land located within such areas may be used for any purpose that does not involve the establishment of a prohibited structure, including parking and circulation, until such time as the land is conveyed to the City. At such time, any parking spaces which are provided within this area and which are required in order to continue to use gross floor area on the site, must be replaced.
- 12.12.14.10. Notwithstanding the foregoing, the parking area west of “Main Street” shall be designed with an aisle and sidewalk on the future east/west road allowance, providing a direct continuous connection for vehicles and pedestrians, from “Main Street” to the **High-Rise Residential** lands to the west.
- 12.12.14.11. In time, in regard to the development of the lands designated **Mid-Rise Mixed-Use** and **High-Rise Mixed-Use** in excess of the gross floor area limit specified in policy 12.12.14.1, the road network within the Thornhill Town Centre shall form a continuous

and interconnected pattern, integrated with the road network planned or in place on surrounding lands bounding the Thornhill Town Centre.

- 12.12.14.12. In time, in regard to the development of the lands designated **Mid-Rise Mixed-Use** and in excess of a gross floor area limit specified in policy 12.12.14.1, roundabouts, cul-de-sacs, hammerheads or other similar traffic calming and traffic control measures may be implemented subject to evaluation by the City and/or the Region of York, and subject to the resolution of the functional, operational and financial issues associated with their use.
- 12.12.14.13. Streetscape design for arterial roads, collector roads, and local roads shall be subject to the urban design requirements and policies articulated in this Secondary Plan.
- 12.12.14.14. Individual direct access to any arterial road shall be discouraged.
- 12.12.14.15. Except as otherwise provided in this Secondary Plan, building setbacks from arterial roads will be minimized and parking areas shall be directed to areas less visible from the street.
- 12.12.14.16. The Regional road structure (the arterial roads) of Centre Street and Bathurst Street will play an important role in the success of the Thornhill Town Centre as it continues to develop and mature. The Regional roads are the location for public transit serving the Town Centre, including the existing transit station at Centre/North Promenade. The City will work with York Region to identify and implement streetscape improvements on the arterial roads to enhance their appeal to pedestrians and bicyclists, and other potential transit riders.

### **Public Transit**

- 12.12.14.17. It is a major objective of the City to increase public transit ridership. To achieve this objective, the transit network within the Thornhill Town Centre is encouraged to be integrated with local, regional and inter-regional transit routes and services, including:
  - a. Connections to the local transit system along arterial and collector roads.
  - b. Connections to the TTC Subway system via the Highway 7 corridor, Vaughan Metropolitan Centre (Spadina line) and Finch station (Yonge line).
  - c. Connections to the GO rail stations at Major Mackenzie Drive and Rutherford

Road, and any future station in the Vicinity of Highway 7/Concord, and connections to commuter parking lots and GO bus operations along the Highway 400 corridor.

- 12.12.14.18. The City will take into account the requirements for a comprehensive and interconnected public transit network by ensuring that all development applications fully consider short and long-term transit opportunities.
- 12.12.14.19. The internal road network shall be continuous such that it is conducive to the efficient routing of public transit and provide the maximum possible service coverage.
- 12.12.14.20. The City shall encourage the increased use of public transit by requiring transit-supportive urban design. Improvements to the public transit network, based on the existing and proposed system of roads, should be initiated immediately and continue as necessary to meet the needs of development as it proceeds.
- 12.12.14.21. Local transit routes serving the surrounding community should converge within the Thornhill Town Centre, and additional transit stops/stations shall be established at appropriate locations as development proceeds.
- 12.12.14.22. The City will continue to work with York Region Transit (YRT) and VIVA to foster transit ridership and ensure that the Thornhill Town Centre is well provided with transit services to meet the needs of its existing and future resident and business community.

### **Pedestrian and Bicycle System**

- 12.12.14.23. The development of all roads shall include safe, convenient and attractive facilities for pedestrians and cyclists.
- 12.12.14.24. A pedestrian walkway and bikeway system shall be required in the Thornhill Town Centre. Pedestrian and bicycle facilities will be designed to provide connections to bus stops, and linkages between buildings, adjacent sites, surrounding areas and public streets, particularly those with transit facilities.
- 12.12.14.25. An adequate supply of secure bicycle parking facilities shall be provided near bus stops, in high activity areas and park areas to which bicycles may be locked.

## **Parking**

- 12.12.14.26. Sharing of parking in mixed-use developments will be encouraged within the Thornhill Town Centre, subject to evaluation by the City.
- 12.12.14.27. On-street parking shall be permitted on all streets fronting commercial and mixed-use development within the Thornhill Town Centre. Parking underground and in structures is encouraged.
- 12.12.14.28. To ensure that an adequate supply of parking is available, minimum parking standards will be established in the zoning by-law.
- 12.12.14.29. It is recognized that suburban parking standards are inappropriate within the Thornhill Town Centre, given the urban character of development to be established in the Town Centre, and its growing role as a transit node. Therefore the City will consider reduced parking standards within the Thornhill Town Centre, provided they are justified by a parking study completed to the City's satisfaction. To encourage the desired form of development on "Main Street", the City, in calculating required parking, will also exempt the GFA on the second floor of "Main Street" buildings equal to the GFA located in the same building on the first floor up to a maximum of 5,600 square metres GFA.
- 12.12.14.30. For the purpose of providing parking to satisfy the City's parking standards, the lands west of "Main Street" are required to provide the parking required for the development located west of "Main Street"; and, similarly, the lands east of "Main Street" will provide the parking required for the development located east of "Main Street".

## **Transportation Improvements**

- 12.12.14.31. The full achievement of the development proposed by this Secondary Plan, requires the following improvements to the transportation system:
- a. The following Region of York road and transit improvements have been identified as necessary to support the full achievement of the development of the Thornhill Town Centre:
    - i. Bathurst Street to be widened from four lanes to six lanes from north of Steeles Avenue to Highway 407 in the 2011 to 2021 time period, or sooner if warranted based on monitoring of local traffic conditions.
    - ii. Implementation of the York Rapid Transit Plan with both phases of the

- planned, upgraded transit in place to serve the Thornhill community by 2015;
  - iii. Traffic signal controls installed at the Bathurst/Beverley Glen intersection;
  - iv. Streetscaping improvements to Bathurst Street and Centre Street;
  - v. A local transit network and associated transit infrastructure focused on the transit station at Centre Street and North Promenade; and,
  - vi. Transit links to the Highway 407 Transitway, Yonge Subway, Spadina Subway, Vaughan Corporate Centre and York University, and the GO rail commuter system;
- b. The following City of Vaughan road and transit improvements have been identified as necessary to support the full achievement of the development of the Thornhill Town Centre:
- i. The collector and local road and laneway network;
  - ii. The provision of a major collector road or minor arterial roadway (the “Main Street” identified on Map 12.12.B), built to public road standards, from Centre Street to Beverley Glen Boulevard. This roadway should provide suitable pedestrian amenities to promote pedestrian travel between adjacent residential and commercial areas;
  - iii. On-street and lay-by parking on all public roads within Thornhill Town Centre, in particular on both sides of the “Main Street”; and,
  - iv. A pedestrian and bicycle system linking to areas in the rest of the Thornhill Community.

12.12.14.32. It is the policy of Council to assist the Region of York in protecting and obtaining lands required for the rights-of-way for the Widening of arterial roads and the provision of public transit services through the development approval process.

12.12.14.33. For the Amendment Area north of Centre Street, with the exception of “Main Street”, the dedication of the road network identified on Map 12.12.B and the construction of such roads to the City’s satisfaction by the owner will be required when the gross floor area of development on the site equals 0.5 times the Site area (0.5 FSI), excluding the GFA of the second storeys of “Main Street” buildings up to a maximum exemption of 5,600 square metres GFA (60,000 sq ft) in the calculation of the 0.5 FSI.

12.12.14.34. As development within and adjacent to the Amendment Area proceeds, operations along Bathurst Street and the critical intersections with Bathurst Street and Centre

Street should be monitored to identify the need to widen Bathurst Street in advance of what is currently planned.

- 12.12.14.35. Until such time as the Regional road improvements identified in this Secondary Plan have been undertaken, Council may phase development in the Secondary Plan Area and in regard to the lands in the First Phase of Development, until the required road capacity is available. Where the arterial roads have not been widened to the width set out in this Secondary Plan, development applications will be required to submit a traffic study to demonstrate that capacity is available.

### **12.12.15 Implementation**

- 12.12.15.1. To provide a context for coordinated development, and to demonstrate conformity with the policies of this Secondary Plan, each development application, except for the First Phase of Development, shall include a Development Concept Report per policy 10.1.1.5, providing a detailed description of the proposed development, and the manner in which it addresses the policies of this Secondary Plan.
- 12.12.15.2. Within the Thornhill Town Centre, development applications shall co-ordinate with existing and proposed neighbouring development in a mutually complementary fashion.
- 12.12.15.3. Except for the First Phase of Development (subject to the exception in Item 'a' below), development shall be phased to provide for the orderly development of the Thornhill Town Centre and to ensure the most efficient and economical use of existing and proposed infrastructure. The following phasing criteria shall be considered in the review of all development applications:
- a. Within the Thornhill Town Centre north of Centre Street, the first phase of development shall include, at least, the construction of the "Main Street", and construction of buildings fronting onto the "Main Street". The foregoing construction shall coincide with the construction of the large format retail store at the east end of the site during this phase of development;
  - b. The development contributes to, or can be appropriately integrated within the logical sequence of construction of all required sewer, water, stormwater and transportation facilities;
  - c. The development satisfies all requirements regarding the provision of parkland and other facilities;

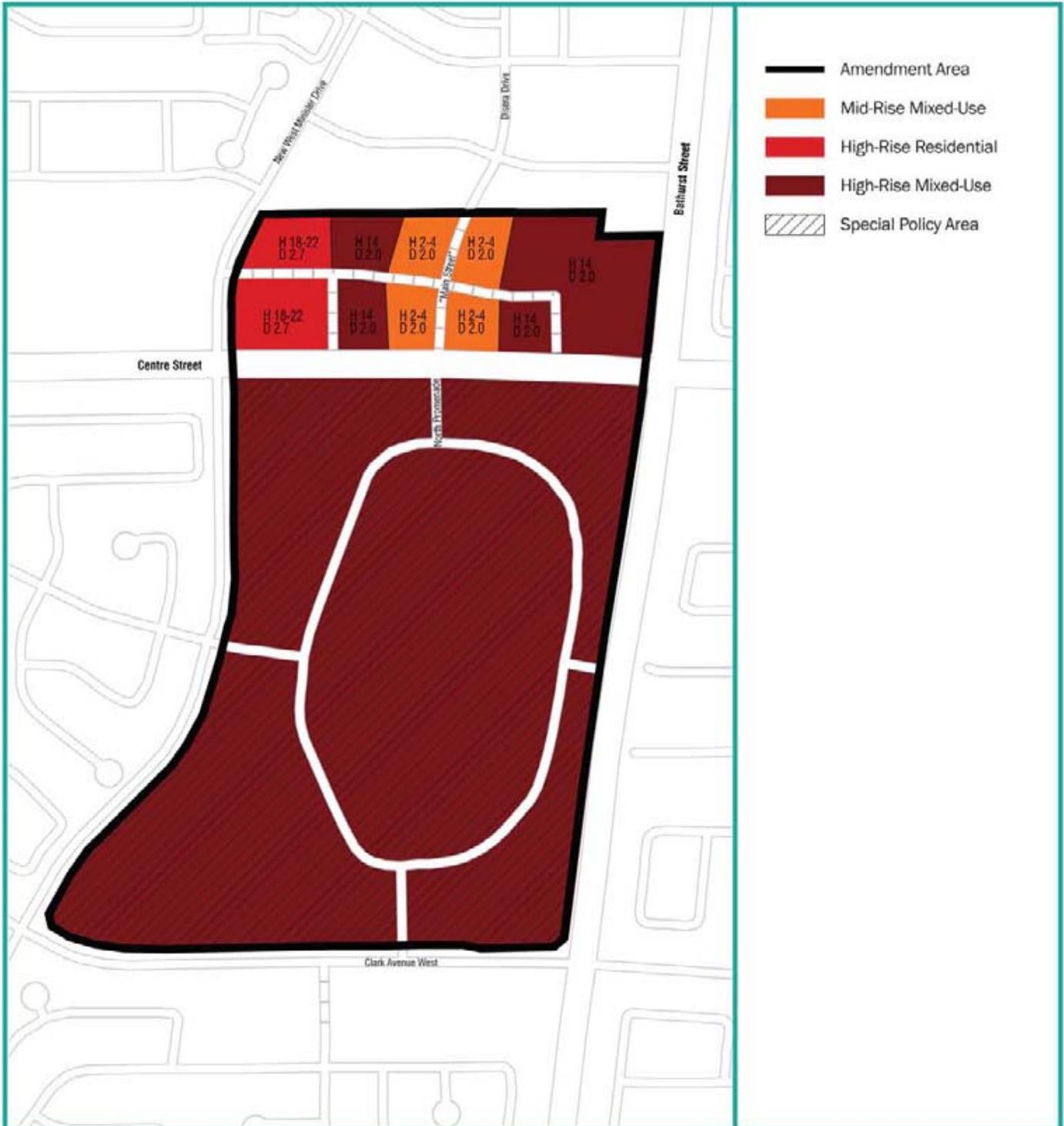
- d. Traffic from the proposed development can be accommodated on the existing arterial road network; and,
- e. Phasing may be addressed through the appropriate use of the holding (H) provisions of this Secondary Plan.

- 12.12.15.4. Except for the First Phase of Development, as a component of the Development Concept Report, development applications shall provide a Phasing Plan, which:
- a. Describes and illustrates how existing and proposed development can be incorporated into the site to achieve the full development potential of the site;
  - b. Identifies the public infrastructure and facilities required to serve the development, including water, sewer, stormwater, roads, transit, parks and open space facilities, and their proposed phased construction; and
  - c. Describes the expected financial requirements for such public infrastructure, and the appropriate financial contributions from benefiting landowners.

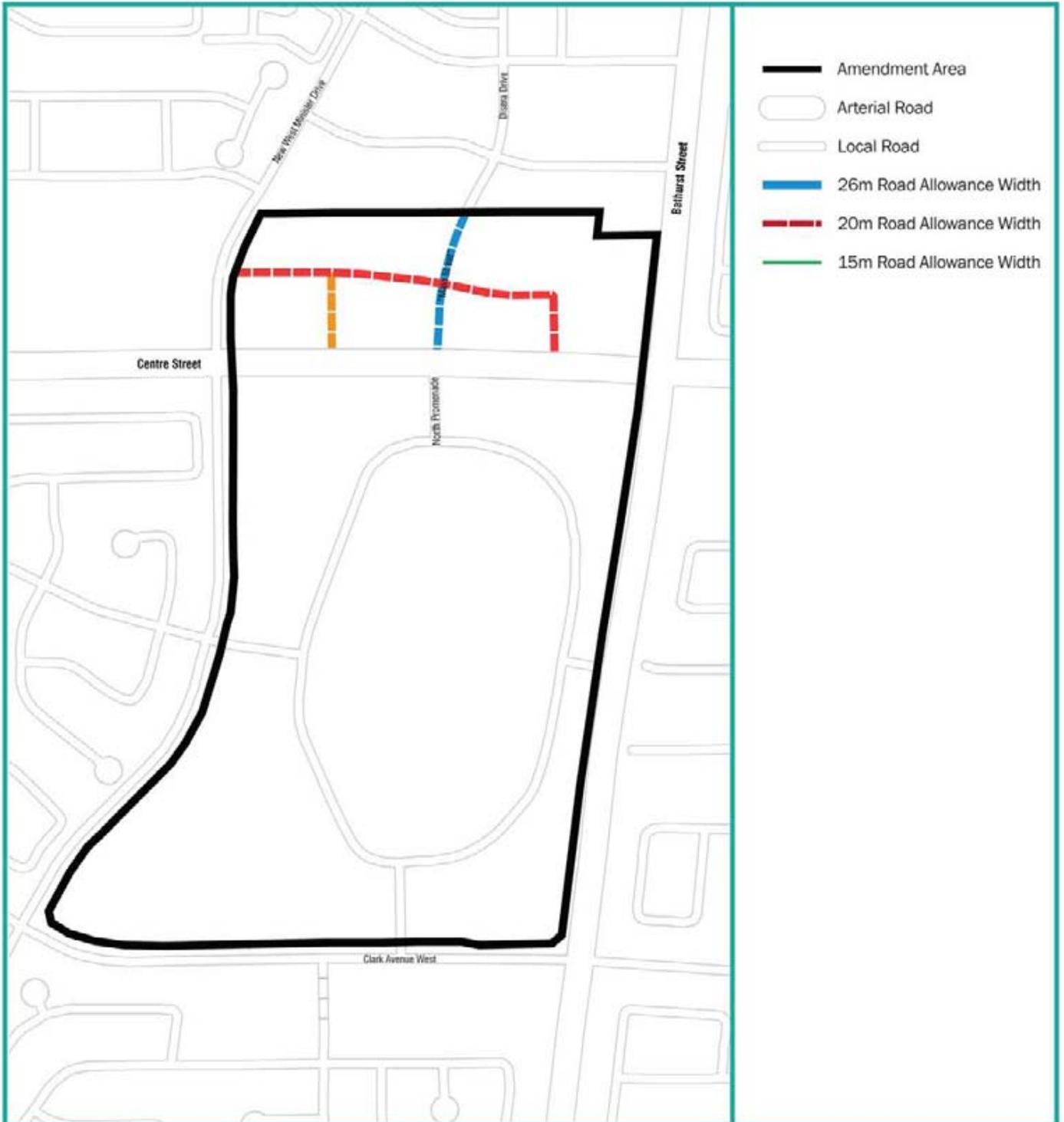
### **12.12.16 Interpretation**

- 12.12.16.1. The designations identified on Map 12.12.A are intended to show general areas. Minor adjustments to the boundaries of the designations may occur through the City's approval process without amendment to the Secondary Plan, except where the designations are established by fixed boundaries, such as road rights-of-way, or where specifically stated to be fixed in the policies of this Secondary Plan.
- 12.12.16.2. Where lists or examples of permitted uses are provided, they are intended to indicate the possible range and type of uses that are to be considered. Specific uses not listed, but considered by the City to be similar to the listed uses and to conform to the general intent of the applicable land use designation may be recognized as a permitted use and recognized in the implementing zoning by-law.
- 12.12.16.3. Minor variations from numerical requirements in the Secondary Plan may be permitted without an Official Plan Amendment provided that the general intent of the Plan is maintained. Minor variations, in this Secondary Plan, shall mean a maximum of 5 percent variation from the identified numerical requirement.

▼ Map 12.12.A: Bathurst and Centre Streets- Land Use



▼ Map 12.12.B: Bathurst and Centre Streets- Transportation Network

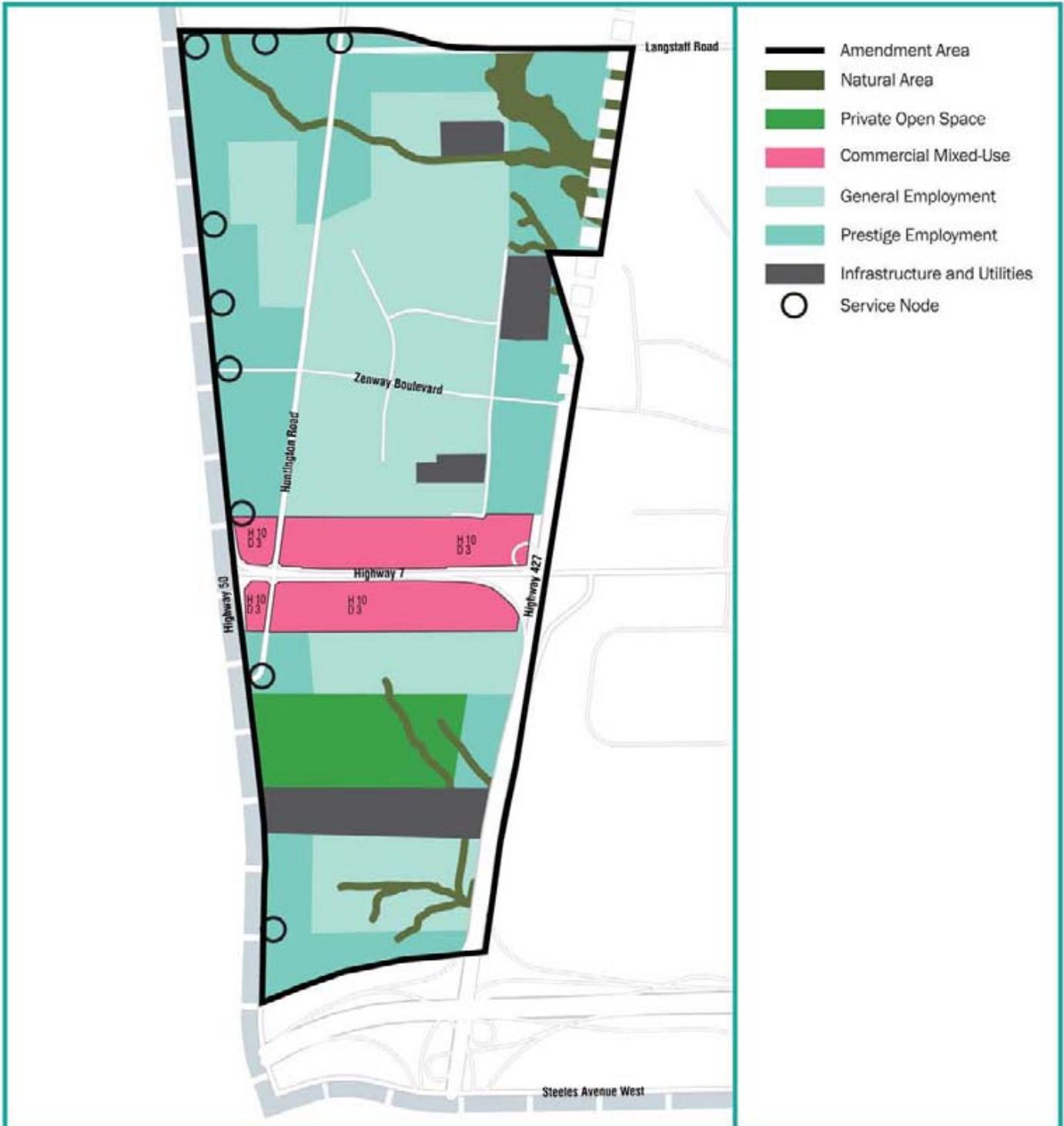


## 12.13 Huntington Business Park

### 12.13.1 General

- 12.13.1.1. Notwithstanding policy 9.2.1.1, within the Huntington Business Park identified on Map 12.13.A, service nodes are permitted at the intersection of arterial and collector streets to:
- a. To provide for the day to day convenience and service needs of businesses, industries and their employees;
  - b. To ensure that service opportunities are provided at convenient, easily accessible locations throughout the Huntington Business Park area; and,
  - c. To ensure that these facilities are comprehensively designed in order maximize functional efficiency and visual amenity.
- 12.13.1.2. Acceptable sites for service nodes are identified on map 12.13.A and will be and zoned either through site specific zoning amendment applications or through the Block Plan process.
- 12.13.1.3. Prior to approving an application to permit a service node, the approval of an urban design plan and traffic impact study may be required.
- 12.13.1.4. The maximum area of a service node shall be approximately 1.2 hectares. A service node may exceed 1.2 hectares if the site is to be developed in conjunction with a predominant use such as an office complex, hotel or retail warehousing.
- 12.13.1.5. Uses permitted shall provide for the day to day convenience and service needs of businesses, industries and their employees. Retail uses which service the convenience needs of employees (i.e. convenience retail store or pharmacy) and the needs of business (i.e. business supplies) are considered to conform. The detailed range of uses shall be established in the zoning by-law.

▼ Map 12.13.A: Huntington Business Park



# 13

## SITE SPECIFIC POLICIES



## 13.1 Lands Subject to Site Specific Policies

### 13.1.1 Lands subject to Site Specific Policies

It is the policy of Council that:

- 13.1.1.1. The lands known as the South-West corner of Bathurst Street and Teston Road are identified on Schedule 14-C and are subject to policies set out in Section 13.2 of this Plan.
- 13.1.1.2. The lands known as 7242 Highway 27 are identified on Schedule 14-C and are subject to policies set out in Section 13.3 of this Plan.
- 13.1.1.3. The lands known as 1125 Highway 50 are identified on Schedule 14-C and are subject to policies set out in Section 13.4 of this Plan
- 13.1.1.4. The lands know as Thornhill Liberty Lands are identified on Schedule 14-C and are subject to the policies set out in Section 13.5 of this Plan.
- 13.1.1.5. The lands known as the Vaughan Healthcare Campus Centre are identified on Schedule 14-C and are subject to policies set out in Section 13.6 of this Plan.
- 13.1.1.6. The lands at the north west corner of Rutherford Road and Bathurst Street are identified on Schedule 14-C and are subject to policies set out in Section 13.7 of this Plan.

## 13.2 South West Corner of Bathurst Street and Teston Road

### 13.2.1 General

- 13.2.1.1. Notwithstanding Policy 9.2.2.4(f) of the Official Plan, for the area within the heavy lines on Map 13.2.A, Low-Rise Buildings are permitted pursuant to policies in Section 9.2.3 of this Plan.
- 13.2.1.2. Parking between a building and a local street is permitted on the subject lands, shown on Map 13.2.A, provided it is appropriately screened with significant landscaping.

▼ Map 13.2.A:  
South West Corner of Bathurst St. and Teston Rd.

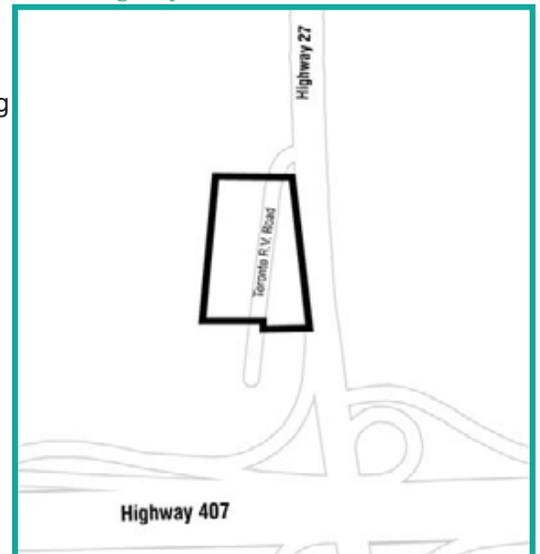


## 13.3 7242 Highway 27

### 13.3.1 Land Uses and Development Criteria

- 13.3.1.1. Notwithstanding policy 9.2.1.1, the following policies apply to the area identified as Prestige Employment on Map 13.3.A.
- 13.3.1.2. The priority uses for the subject lands shall be offices, hotels with related hospitality, eating establishment and conference/banquet hall facilities, and other prestige employment uses. Ancillary retail commercial uses shall be permitted where their orientation and location are

▼ Map 13.3.A:  
7242 Highway 27



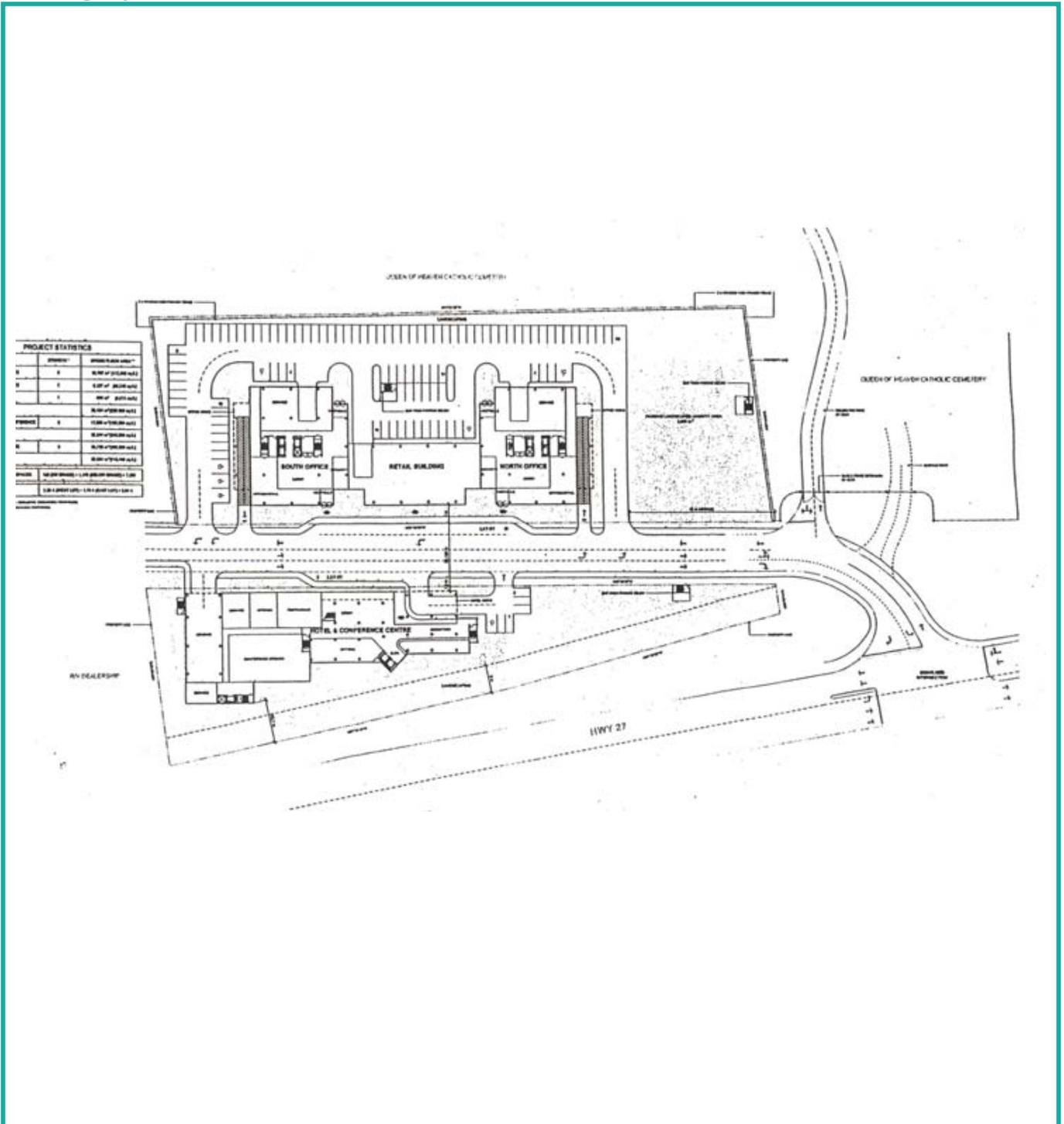
appropriately integrated into a development that is designed and devoted to the priority uses.

- 13.3.1.3. The implementing Zoning By-law shall establish the permitted uses and development standards as follows:
- a. The provision of a passive landscaped amenity area containing a minimum area of 3000 square metres, with a minimum frontage along Toronto RV Road of 45 metres immediately south of the adjacent cemetery lands. The use of such area will be restricted in the zoning by-law to landscaping;
  - b. The northerly office building will consist of a maximum of 7 storeys, comprised of 6 typical storeys, in addition to the ground floor. The maximum height of the building shall be as shown on Map 13.3.B. The southerly office building will consist of a maximum of 8 storeys, comprised of 7 typical storeys, in addition to the ground floor; the maximum height of the building shall be as shown on Map 13.3.B. Any retail facility located between the office buildings shall be a maximum of 1 storey and not exceed 12 metres in height. The hotel building on the Site will consist of a maximum of 9 storeys, comprised of 6 typical storeys, in addition to the ground floor, a second storey and a mezzanine; the maximum height of the building shall be as shown on Map 13.3.B;
  - c. All parking shall be located underground or at grade. No parking, shall be located in above ground structures; and,
  - d. Landscaping and fencing requirements and window treatment shall be established by the City and to the satisfaction of the City in the Site Plan Agreement or Letter of Undertaking, whichever is in effect, in consultation with the land owner and the owner of the adjacent cemetery lands.
- 13.3.1.4. Given the scale and phasing of the overall development, a detailed urban design guidelines report, master landscape plan, and comprehensive development phasing plan shall be submitted with the site plan application.
- 13.3.1.5. Enhanced architectural treatment and appropriate mass and scale of the buildings, shall be provided for the overall development, reflecting the prominence of the Highway 407 and Regional Road 27 location. Buildings shall be designed so that elevations facing a street and the entrance to the site present a “front-like” elevation. Similarly. Sensitive

architectural treatment shall be required for the facades that face the cemetery and Toronto RV Centre.

- 13.3.1.6. Surface parking and service areas shall be appropriately landscaped and screened from view, and the outside storage of goods and materials and garbage shall not be permitted. Perimeter landscaping shall be required to enhance the built environment, including sufficient-sized landscaped buffers adjacent to Regional Road 27 and the cemetery, and a significant urban landscaped treatment within the right-of-way of Toronto RV Road.
- 13.3.1.7. The comprehensive site development shall address the above-noted development and policies in Section 9.1 of the Official Plan.
- 13.3.1.8. Road improvements satisfactory to the Region and the City and of the Ministry of Transportation shall be undertaken. Such road improvements are intended to include:
  - a. Traffic signal installation at the Highway 27/Toronto RV Road intersection;
  - b. The construction of a southbound right turn lane at the Highway 27/Toronto RV Road intersection; and,
  - c. The widening of the eastbound approach of the Highway 27 / Toronto RV Road intersection. The widening should accommodate two eastbound and one westbound lane.
- 13.3.1.9. Development shall only occur on full water and sewer services. The extension of water and sewer services to the subject lands shall be sized to accommodate the present and future requirements of the adjacent cemetery lands. The costs thereof have been dealt with by private agreement between the land owners.

▼ Map 13.3.A:  
7242 Highway 27. - Floor Plan



## 13.4 1125 Highway 50

### 13.4.1 General

13.4.1.1. Notwithstanding policy 9.2.1.1 of the Official Plan, the following uses shall be permitted on the lands identified as **Area D** on Map 13.4.A:

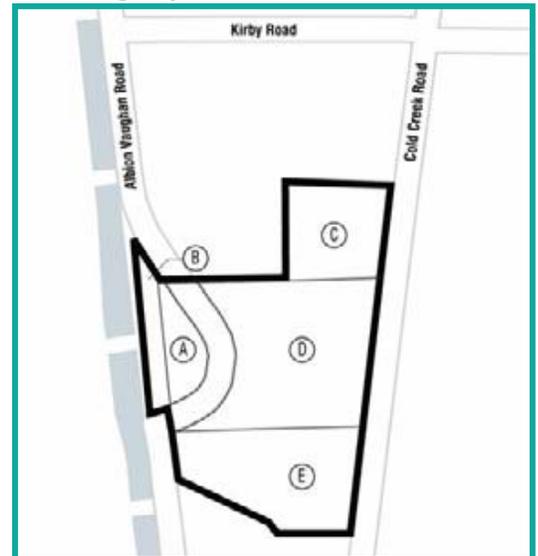
- a. truck transport facility (truck trailers, tip trailers and gravel trucks) and the uses normally incidental to the main trucking uses such as offices, employee parking, truck repair and maintenance facilities, fuel pumping, public truck maintenance garage, public truck body shop, public truck fuel sales, truck sales, and limited retail and/or office accessory uses;
- b. for the purposes of policy 13.4.1.1.a, a truck transport facility shall be defined in the implementing zoning by-law as a building or place where trucks or transports are stored, parked and maintained or from which trucks or transports are dispatched for hire as common carriers.

13.4.1.2. Notwithstanding policy 9.2.1.1 of the Official Plan, for lands identified as **Areas A** and **B** on Map 13.4.A, the following uses shall be permitted, but only if municipal water and sanitary sewer services are available to the site:

- a. automobile service station, car wash, motor vehicle sales establishment, car rental agency accessory to a main use, restaurant, hotel, motel, tourist information centre accessory to a main use and limited retail uses accessory to a main use.

13.4.1.3. Notwithstanding policy 9.2.1.1 of the Official Plan, the lands identified as **Area C** on Map 13.4.A shall only be used for the parking and storage of transport trucks that are accessory to the main uses permitted on **Areas D** and **E**.

▼ Map 13.4.A:  
1125 Highway 50



- 13.4.1.4. Notwithstanding the policy 9.2.1.1 of the Official Plan, for lands identified as **Area E** on Map 13.4.A, the following additional uses shall be permitted:
- a. public truck maintenance garage, public truck body shop, public truck fuel sales, truck sales, storage and limited retail and offices accessory to the main permitted uses.
- 13.4.1.5. The lands identified on Map 13.4.A shall be developed in accordance with the following policies:
- a. The lands may be serviced by a private well and a private waste disposal system subject to the approval of the Ministry of the Environment and the York Region Environmental Services Division. Prior to the approval of any site development applications for the subject lands or any part thereof the Owner shall prepare and have approved a servicing study which demonstrates the ability of the on-site servicing system to support any increase in the intensity of development. If required, additional development may not occur until such time as full municipal services are available. A site servicing study for **Areas A** and **B** shall be required prior to site plan approval for those areas;
  - b. The implementing zoning by-law shall establish the following restrictive provisions:
    - i. restricting the use of **Areas A** and **B** on the land, to the west of the proposed Albion-Vaughan Road realignment, to an automobile gas bar, automobile service stations, car wash, motor vehicle sales establishment, car rental agency accessory to a main use, restaurant, hotel, motel, tourist information centre accessory to a main use, and limited retail use accessory to the main use;
    - ii. restricting the use of **Area C** to truck parking and storage;
    - iii. restricting the use of **Area D** to a truck transport facility (truck trailers, tip trailers and gravel trucks) and the uses normally incidental to the main trucking uses such as offices, employee parking, truck repair and maintenance facilities, fuel pumping, public truck maintenance garage, public truck body shop, public truck fuel sales, truck sales, and limited retail and/or office accessory uses;
    - iv. restricting the use of **Area E** for public truck maintenance garage, public truck body shop, public truck fuel sales, truck sales, storage and limited retail and offices accessory to the main permitted uses, in addition to those uses permitted on the central parcel, west of the proposed Albion-Vaughan Road realignment; and,

- v. the truck transport facility and ancillary uses shall only be permitted provided that the required berming and landscaping on the subject lands is in place.
- c. The perimeter of the portion of the lands comprising **Areas C, D and E** shall be bermed and landscaped so as to ensure that the truck storage on site shall be screened. In addition the design of any buildings on **Areas E or D** shall ensure that there will be no negative visual impact on the adjacent uses. No truck parking or storage shall be permitted until such berming and landscaping is in place to the satisfaction of the City of Vaughan;
- d. At the time of site development for **Area A and B**, the Owner shall submit a full landscape plan which will determine, among other things normally required as part of the site development process, the extent of landscaping required including the width of landscape strips which may be above the minimum by-law requirements;
- e. Access to the subject lands shall only be permitted via the re-aligned Albion-Vaughan Road subject to the approval of the appropriate road authorities, which may include the City of Vaughan, Regional Municipality of York and the Regional Municipality of Peel. The dedication of a one-foot reserve along the entire Cold Creek Road frontage and along the Albion-Vaughan Road frontage save and except for the required driveway locations shall be required. No direct truck access shall be permitted to Cold Creek Road or to Highway No.50. Any passenger access to Cold Creek Road shall only be permitted subject to the approval of the City of Vaughan;
- f. The Owner shall dedicate the road allowance and any road widening for Albion-Vaughan Road, as determined by the City, to the appropriate authority free of all cost and encumbrance. Buildings permits for a permanent building or structure on the lands will not be issued until the alignment is established;
- g. In consultation with the Regional Municipalities of York and Peel, the Town of Caledon and the Ministry of Transportation Ontario, the City of Vaughan shall ensure that all issues concerning road improvement and driveway access have been resolved and the matters dealt with through the implementing zoning by-law, the site development agreement or other means as may be appropriate; and,
- h. Notwithstanding the planned road allowance standards of the Albion-Vaughan Road, the required right-of-way shall be established upon the preferred road alignment for the Albion-Vaughan Road.

- 13.4.1.6. In reviewing site development applications, the City of Vaughan shall ensure that matters such as landscaping and screening, lighting, noise, dust and other site development matters are addressed to ensure nearby residential uses will not be detrimentally impacted. In order to establish necessary noise attenuation measures, a noise study may be required prior to the approval of a site development application
- 13.4.1.7. Prior to the execution of site development agreements, or site plan letters of undertaking, whichever is in effect:
- a. The Owner may be required to submit traffic studies, approved by the appropriate road authorities, to determine the level of development and the location and configuration of access points; and,
  - b. The Owner may be required to submit and have approved additional servicing studies to determine the level of development which can be supported by the current on-site servicing facilities. Any development on the subject lands shall require the approval of the Region of York Health Protection Division. If it is determined that the current servicing facilities cannot support any additional development then subsequent development proposals shall not be approved until such time as full municipal services are available.

## 13.5 Thornhill Liberty Lands

### 13.5.1 General

13.5.1.1. Notwithstanding policy 9.2.1.1 the following policies and development criteria shall apply to the lands identified on Map 13.5.A:

- a. a maximum number of 1,598 units, comprising 93 townhouse units and 1,505 apartment units, or a combination thereof not to exceed 1,598 units, shall be permitted;
- b. the number of apartment buildings shall not exceed seven;
- c. a maximum building height of 3 storeys for the townhouse units, and 22 storeys for the apartment buildings shall be permitted;
- d. a convenience retail store and coffee shop/outdoor patio use shall be permitted on the ground floor of one condominium apartment building, to be located at the northwest corner of Disera Drive and the planned east/west local road between Bathurst Street and New Westminster Drive; and,
- e. the overall development of the lands shall be in accordance with a master plan approved by Council, and intended to guide future development within the subject lands, together with the submission of the following reports to be approved through consideration of a draft plan of subdivision application:
  - i. urban design guidelines;
  - ii. landscape/streetscape and open space master plans;
  - iii. shadow study;
  - iv. traffic impact/phasing report; and,
  - v. any other reports considered appropriate by the municipality.

▼ Map 13.5.A:  
Thornhill Liberty Lands



## 13.6 Vaughan Healthcare Campus

### 13.6.1 General Intent

The purpose of the Official Plan Amendment is to provide policy to guide development of a specialized urban Centre focused on a health campus of care to serve the citizens of Vaughan as well as residents of the broader region. Its exceptional accessibility and visibility from Highway 400, Major Mackenzie Drive and Jane Street, as well as its central location within the City of Vaughn, support the development of a Centre in this location. Major Mackenzie Drive is planned to be a Regional Rapid Transit Corridor in this location, providing a ready link to the nearby GO station, municipal civic centre and York Central Hospital in Richmond Hill, as well as to many neighbourhoods within Vaughan. Jane Street south of Major Mackenzie Drive is also planned as a Regional Rapid Transit Corridor, which will provide service to this area. The development of a Healthcare Campus Centre consisting of a mixture of uses at higher densities in a compact, pedestrian friendly form will support increased transit ridership, as planned.

▼ Map 13.6.A:  
Vaughan Healthcare Campus



This Plan covers approximately 33 hectares (82 acres), of which approximately 24 hectares (60 acres) will develop as a health campus of care to be anchored by a hospital, as generally shown on Map 13.6.A. The Plan also provides for other uses that are related to the primary healthcare function and further provides, through a further Special Study, the potential for more intensive forms of residential and employment uses, consistent with a Centre, on the remainder of the site. Further planning exercises will establish a greater level of detail for the site and the exact boundary between the two component areas.

A Hospital Precinct Plan exercise will be undertaken for the health campus of care to determine in greater detail future land uses and other matters pertaining to the Vaughan Campus of Care site. It must be approved prior to development. The Hospital Precinct Plan will also be required to address higher order transportation and servicing of the entire quadrant, including stormwater management and the treatment of the tributary of the West Don River that flows through the site. As a second phase, a further Special Study planning analysis of the remainder lands will be undertaken to determine the highest and best use after the Hospital Precinct Plan has been completed. Both of these further planning exercises will include public and agency input.

Access to the site will be primarily from Major Mackenzie Drive and from Jane Street. Both of these Regional Arterial Roads are planned for increasing levels of transit and have been designated as Local Corridors by the existing Region of York Official Plan, where intensification is intended to support planned levels of transit and where mixed use buildings, pedestrian supportive streetscapes and higher density employment and residential land uses will support an urban realm.

The Healthcare Campus Centre Plan provides that land uses along the northern boundary will provide appropriate compatibility with the established, low-density neighbourhood to the north in terms of general height and massing. Development will be required to provide a suitable transition in height and massing from north to south, so that the neighbourhood scale is respected along the northern boundary and more intensive land uses and higher heights will be located close to higher order transit and farther from the neighbourhood to the north.

Within the quadrant, the Plan provides that the vehicular circulation pattern will be organized in a clear and coherent manner to direct traffic to and from boundary roads and to facilitate visitors, transit service, emergency vehicles and service functions within the site. Further planning of the Centre must make provision for pedestrian and cycling opportunities within the site and appropriate linkages to the wider area.

Development throughout the Healthcare Campus Centre will be characterized by a high quality of urban design, including the treatment of streetscapes, public and private open space areas, and pedestrian linkages that are focused on creating a harmonious and attractive image and promoting pedestrian activity and comfort. Urban design will be more specifically addressed in the Hospital Master Plan and through the further planning of the Centre. Implementation of urban design requirements will also be achieved through zoning and site plan requirements.

Development of the Healthcare Campus Centre will be developed incrementally through further studies and analyses, beginning with the Hospital Precinct Plan. The Hospital Precinct Plan will include public input and consultation with approval agencies of the Region, Province, and the Toronto Region Conservation Authority prior to approval by the City. When the Hospital Precinct Plan is completed a Special Study of the remainder lands will be undertaken to determine their highest and best use in the context of the Hospital Precinct Plan; the role of a Centre; the capacity of planned infrastructure; and the appropriate transition to the abutting neighbourhood. The Special Study will also involve public input and consultation with agencies prior to approval by the City. The Hospital Precinct Plan and the subsequent Special Study of the remaining lands, will provide the basis of more specific Zoning and approval of site plans for each portion of the Healthcare Campus Centre prior to development.

Development approvals will be phased to ensure that infrastructure, including road capacity, is adequate to support planned levels of development and that necessary facilities or agreements are in place. The use of Holding Zoning Bylaws will be used to control development until such matters are in place to the satisfaction of the City. Transportation, services and utilities within the Healthcare Campus Centre will be integrated and co-ordinated to function as efficiently as possible. All development within the Healthcare Campus Centre will be subject to Site Plan Control.

## **13.6.2 Urban Structure**

- 13.6.2.1. The Healthcare Campus Centre is intended to develop with a primary focus on the delivery of healthcare services, but the site also has the potential to include a mixture of other uses that may be less directly related to its primary healthcare function. Development will be compact and transit supportive, at heights and densities appropriate for a Centre.
- 13.6.2.2. To take maximum advantage of the planned rapid transit corridor on Major Mackenzie Drive and enhanced transit on Jane Street, the arrangement of land uses and the design of internal transit routes, road and pedestrian connections, will be planned to optimize service to the hospital and other major employment uses.
- 13.6.2.3. A transition of land use, height and density will focus less-intensive uses to the north, adjacent to the low-rise neighbourhood and more intensive uses will be focused in close proximity to transit.

## **13.6.3 Population And Employment**

- 13.6.3.1. Based on a Central LHIN study in 2009, the Healthcare Campus Centre is expected to accommodate a hospital of approximately 600 beds as well as a significant component of long-term and complex care beds, within the continuum of care to serve the aged. Additional health services, medical offices, laboratories, research and development facilities and health-related educational activities and specialized housing may be part of the Hospital Precinct Plan area.
- 13.6.3.2. A district power system may be developed for the Hospital Precinct or for a wider area.
- 13.6.3.3. Additional opportunities exist in the development of this Healthcare Campus Centre for offices, research facilities, hospitality uses, commercial and institutional uses, specialized care facilities, and higher order forms of residential development. Such potential uses will be supported by open space, and will incorporate a high standard of urban design that creates an attractive pedestrian realm, vibrant streetscapes, mixed use buildings, and a distinctive 'sense of place', appropriate for a Centre.

### 13.6.4 Development Principles And Objectives

The following policies include principles and development objectives that will be considered in the evaluation of all future planning exercises and in the approval of capital works and development applications in the Healthcare Campus Centre.

#### **Sustainability**

- 13.6.4.1. Development within the Healthcare Campus Centre will exhibit best practices as established by the City to ensure sustainability by creating a healthy environment, vibrant communities and economic vitality in accordance with approved policies of the City such as those contained in “Green Directions” Vaughan Community Sustainability and Environmental Master Plan.
- 13.6.4.2. Development within the Healthcare Campus Centre should be compact and arranged in a manner that encourages pedestrian activity including accessibility for the elderly or disabled, cycling, and the use of transit in order to reduce energy consumption, reliance on the automobile, and the production of greenhouse gases or other harmful emissions.
- 13.6.4.3. Stormwater management practices should foster means of stormwater infiltration to reduce runoff, enhance water quality and support ecological functions, through the inclusion of such techniques as Low Impact Development Standards.
- 13.6.4.4. Green building and community design that promotes energy efficiency, use of renewable energy sources and reduction of waste will be encouraged in the infrastructure planning of the site, and through such programs as LEED.
- 13.6.4.5. The viability and benefits of a renewable source district energy system will be considered for the Hospital Precinct Plan and within the wider Centre, as well as “green” means of ensuring sustained energy production to serve the hospital and wider area.
- 13.6.4.6. The microclimate created by wind, sun and shadow will be considered in the arrangement of land use, the design of buildings, and the use of building materials and landscape treatment so that pedestrian comfort is enhanced and energy consumption is reduced.

- 13.6.4.7. A mixture of land uses and convenience facilities that are planned within the Centre will be arranged in a manner that encourages workers and residents to walk and support a safe, vibrant streetscape while reducing reliance on the automobile. Public safety will be fostered through such measures as the application of Community Policing Through Environmental Design (CPTED) principles.
- 13.6.4.8. Installation of state of the art telecommunication infrastructure and smart grid technology will be encouraged throughout the Centre to support advanced healthcare and business uses.

### **Land Use And Transition**

- 13.6.4.9. Higher densities and heights will generally be focused in locations that are best served by transit.
- 13.6.4.10. A sensitive transition in land use, height and massing should be created along the boundary to the low-density neighbourhood to the north, through the arrangements of land uses, as well as setbacks, angular planes and landscaped buffers as appropriate.
- 13.6.4.11. Land uses that may create noise, odour or reflected light should be separated, from sensitive residential/and uses or effectively screened and buffered, so that no adverse effect is created on a residential or other, sensitive use.
- 13.6.4.12. Sensitive residential land uses will be buffered or screened from road noise and from unacceptable noise levels from Canada's Wonderland.

### **Urban Design**

- 13.6.4.13. A high quality of urban design will be promoted in public spaces such as streetscapes, parks and open space and in the design of public buildings or infrastructure in order to create an attractive, coherent and comfortable public realm with signature elements that create a distinctive sense of place. These elements will also be promoted within privately owned spaces that are publicly accessible such as private roadways, walkways, squares or courtyards and in the streetscape facade of buildings.

- 13.6.4.14. Transit-supportive development will be strongly encouraged in the arrangement of land uses and through the design of streets and private roadways so that comfortable and coherent pedestrian connections are created that foster safe, convenient and attractive links to existing and planned transit routes and stops.
- 13.6.4.15. Public safety, accessibility and aesthetics will be considered in the arrangement of land uses and design of building elements and lighting, including walkways, parking areas and open areas, so that the design is conducive to public activity and to surveillance.
- 13.6.4.16. Focal points of a high standard of design will be encouraged at the terminus of a street or private roadway, at a significant intersection or at a key transit stop or facility. Particular visual prominence should be given to the hospital so that visitors to the site can easily identify it. This may be achieved through the building design as well as by creating view corridors from the arterial roads and highway, along prime access roads or driveways or from key pedestrian and open space corridors.
- 13.6.4.17. Street-related design shall be promoted through the placement of buildings and building entrances close to the street with consistent setbacks and by encouraging the location of retail and service commercial uses at grade within mixed-use buildings.
- 13.6.4.18. All development will be subject to site plan control and reviewed by the City's Design Review Panel if in effect.

### **Transportation**

- 13.6.4.19. A full range of mobility options will be planned within the Healthcare Campus Centre in order to provide convenient routes for walking, cycling and transit as well as automobiles and service vehicles, and will focus special attention on mobility issues of the elderly, injured or disabled.
- 13.6.4.20. Transit-supportive design of development will be required, in order to improve the modal split for transit and enhance the development potential of the Centre. This may include enhanced transit accommodation or facilities within the Centre.

- 13.6.4.21. Circulation within the Centre will be designed to facilitate emergency vehicles and the clear orientation of visitors within the site through the use of coherent and direct circulation routes and signage that can be readily understood. Delivery and service access roads or driveways will be separated from visitor and employee access routes, wherever feasible.
- 13.6.4.22. Opportunities to provide additional transportation improvements in the future shall be protected, as identified in this Plan, or as identified by higher levels of government through future planning exercises.
- 13.6.4.23. Development may be phased as necessary, to ensure that sufficient transportation capacity exists or will be in place through committed improvements to serve proposed levels of development.

### **Municipal Services & Stormwater Management**

- 13.6.4.24. Municipal services and utilities will be provided efficiently within the Centre with an emphasis on increased sustainability by reducing energy and water consumption and through increased use of renewable resources.
- 13.6.4.25. Development may be phased to ensure that municipal services are in place or will be available in time to serve proposed levels of development.
- 13.6.4.26. Stormwater management will be designed to minimize runoff, enhance water quality and to provide infiltration in a manner that is sensitive to the environment and supports natural heritage features and functions. Best management practices including Low Impact Development Standards and source controls will be investigated to best achieve this objective.
- 13.6.4.27. Watercourses and stormwater ponds will enhance the visual amenity of the Centre. They should be designed and located to best support its higher order function while not impeding the intensification potential of key sites, potential access locations, or prime connections to transit.

### 13.6.5 Land Use Designations

The Healthcare Campus Centre is intended to develop a range of land uses that are supportive of a hospital and healthcare focus, and are compatible with the higher order function of a Centre. More detailed Land Use designations shall be determined through a further Special Study of the eastern lands, comprising approximately 9 hectares), which are expected to be surplus to the prime healthcare campus needs. The following land use categories are expected to apply, with some variation as determined by further studies, to major portions of the Hospital Precinct site.

- 13.6.5.1. Notwithstanding policy 9.2.1.1 the following land use policies shall apply to the subject lands.

#### **Major Institutional**

- 13.6.5.2. The designation of Major Institutional within the Hospital Precinct Plan shall include:
- a. a hospital with a full range of care;
  - b. rehabilitation facilities, long term care and other forms of residential use related to healthcare;
  - c. research and development facilities;
  - d. medical and dental offices of all types;
  - e. laboratories;
  - f. facilities that construct or repair medical devices;
  - g. education, training, meeting or conference facilities related to healthcare; and,
  - h. businesses or health facilities that promote wellness.
- 13.6.5.3. Ancillary uses may include child or adult daycare, retail facilities, a chapel or small place of worship, accommodation facilities, parking areas or structures, utilities and maintenance operations, a district energy plant, and recreational facilities associated with a healthcare use.
- 13.6.5.4. The appropriate height and massing of buildings and structures within this designation will be further determined through a Hospital Precinct Plan that involves public input, and agency review prior to approval of a Zoning By-law or any further development approval by the City.

- 13.6.5.5. The uses permitted in policies 13.6.5.1 and 13.6.5.2 may be permitted within the Hospital Precinct through a Holding Zoning By-law until the Hospital Precinct Plan has been prepared in accordance with this Amendment and approved by Council, whereupon the “H” holding provision will be removed.

### **Natural Area**

- 13.6.5.6. The designation of Natural Area within the Healthcare Campus Centre will include the tributary corridor of the West Don River and natural or constructed wetlands related to stormwater management. Landscaped berm areas may also be designated as Natural Areas if they are intended as permanent features and support natural vegetation or contribute to an ecological function or linkage.
- 13.6.5.7. Uses within Natural Areas shall be limited to pedestrian or cycling pathways, passive recreation, and environmental management activities.

### **Special Study Area**

- 13.6.5.8. Areas designated as Special Study Area will be used only for passive uses or for infrastructure including roads and driveways, utility rights of way, and stormwater management facilities, until such time as a Special Study determines the appropriate range of land uses in the context of:
- a. the Hospital Precinct Plan for the Campus of Care;
  - b. an appropriate transition to the neighbourhood to the north; and,
  - c. the intended function of the site as an Urban Centre.
- 13.6.5.9. Public input and agency review will be a necessary and important component of the Special Study that must be undertaken prior to enabling future development.

## **13.6.6 Transportation Policy**

- 13.6.6.1. The intent of the transportation system is to provide a coherent, integrated, multimodal transportation network that is safe, convenient, and encourages transit, cycling and pedestrian alternatives in order to conserve energy and minimize impacts on the environment.

- 13.6.6.2. A Functional Master Plan of the transportation system shall be prepared and approved by the City and Region of York prior to development of any land use within the Healthcare Campus Centre. The intent of the Functional Master Plan is to ensure that proposed levels of development within the Centre can be readily accommodated both within the site and on the external transportation system of roads, highways and transit facilities with expected levels of background traffic, including traffic from Canada's Wonderland. Specific terms of reference must be approved by the City, in consultation with the Region and Ministry of Transportation, prior to initiation of the Functional Master Plan study.
- 13.6.6.3. The transportation system will consist of public streets and may, under limited circumstances, consist of private roadways that provide connection to the arterial road system, provided that full public access is permitted to any private road or driveway that is identified as a necessary component of the transportation system. Major connection points to the arterial road network shall be consistent with the approximate locations established on Map 13.6.A.
- 13.6.6.4. The Functional Master Plan shall address the accommodation of transit, cycling and pedestrian routes, service vehicle access, and parking for all types of vehicles. The internal transportation network shall be continuous so that it is conducive to the efficient routing of transit. The design of the proposed system shall consider the development opportunity of adjacent land and shall facilitate the development of parcels of useable size and maximum accessibility.
- 13.6.6.5. The Functional Master Plan shall be prepared in consultation with the Region of York and will establish necessary improvements to the Regional Road system including the potential expansion of Major Mackenzie Drive to 6 lanes of traffic.
- 13.6.6.6. Access through the Healthcare Campus Centre shall continue to provide secondary entrances to Canada's Wonderland, as long as it is in operation.
- 13.6.6.7. No new road links shall be created on the northern boundary of the Healthcare Campus Centre that will introduce increased levels of traffic into the residential community to the north.

- 13.6.6.8. Access from Major Mackenzie Drive to the Healthcare Campus Centre shall include ramps that lead to the current underpass structures and may pass through Canada's Wonderland in accordance with established easements and agreements in favour of Canada's Wonderland. The City will protect for the future normalization of streets intersecting with Major Mackenzie Drive at grade.
- 13.6.6.9. Provision for a future road or driveway connection shall also be protected at a point approximately midway between these underpass structures to connect to an east-west link within the quadrant, as shown on Map 13.6.A, until further determination of the feasibility of such link. This location on Major Mackenzie Drive shall also be protected for a signal that will enable pedestrians to cross in order to reach future transit planned for the centre of Major Mackenzie Drive, unless such signal is not warranted based on future transit plans or is not feasible.
- 13.6.6.10. The primary access from Jane Street shall be at a signalized intersection approximately midway between Major Mackenzie Drive and the signal at Grand Valley Blvd. and shall be co-ordinated with the access to the plaza east of Jane Street. Additional secondary access points to the north and south of this location, as shown on Map 13.6.A, may be limited by the Region, to right-in and right-out turns.
- 13.6.6.11. The potential for an extension of the Highway 400 (northbound) off-ramp to Major Mackenzie Drive into the Healthcare Campus Centre depicted on Map 13.6.A shall be protected in a manner that can accommodate two lanes of inbound traffic and at least one lane of outbound traffic through the existing traffic signal at Major Mackenzie Drive at the Hwy 400 off-ramp, until such time as the matter has been further investigated to determine the desirability and feasibility of constructing this connection. The design and construction of such access shall be subject to the Ministry of Transportation approval.
- 13.6.6.12. The City shall support the increased use of public transit by requiring transit-supportive urban design that will include such things as continuous and connected sidewalks, weather-protected connections, retaining rights of way for off-street transit loops, and on-street bus bays as well as providing for bus shelters, stop locations and other facilities that will enhance the transit system.

### **13.6.7 Municipal Services & Stormwater Management Policy**

- 13.6.7.1. Development within the Healthcare Campus Centre shall be on the basis of full urban water, wastewater and stormwater management facilities.
- 13.6.7.2. A Master Servicing Strategy that outlines the functional water distribution, waste water collection and stormwater management facilities that will service the entire Centre will be prepared to the satisfaction of the City, the Region of York, Ministry of Transportation and the Toronto and Region Conservation Authority, as a condition of approval of the Precinct Plan.
- 13.6.7.3. As a component of the stormwater analysis, studies shall be undertaken that address the floodplain associated with the tributary of the West Don River, to the satisfaction of the Toronto and Region Conservation Authority and the City of Vaughan. Such studies shall include: determination of appropriate buffers; determination of the floodplain in a Regional storm event through hydraulic and hydrologic modeling; assessment of the potential relocation of the watercourse that includes a fluvial geomorphic analysis; a full ecological assessment of all flora and fauna through an Environmental Impact Assessment; and remedial measures and opportunities for enhancement and restoration of the watercourse and stream corridor to improve water quality and encourage fish habitat. The relocation of the watercourse and floodplain will be assessed and may be permitted subject to satisfactorily addressing these matters.
- 13.6.7.4. Water conservation and means of reducing stormwater runoff and improving water quality will be encouraged.
- 13.6.7.5. New infrastructure or improvements to infrastructure, including roads, municipal water supply, wastewater systems, energy supply and stormwater management systems shall be provided concurrent with the rate of development. Development may be phased and holding provision imposed until adequate improvements have been made or committed.
- 13.6.7.6. Stormwater ponds shall be located in a manner that does not preclude intensification along transit corridors or future improvements to the transportation system.

### 13.6.8 Implementation

The more detailed planning of the Healthcare Campus Centre will occur through more intensive studies that will involve transportation and servicing analyses, consultation with the higher levels of government and public consultation as well as approval by Council. Through this process the boundaries of the healthcare campus will be established, and the appropriate land uses and massing will be determined in the context of available and planned infrastructure, sensitivity to adjoining land uses, the planning intent for the Healthcare Campus Centre and the input of residents and other stakeholders. The use of Holding By-laws may be employed to phase development until required studies have been undertaken or necessary infrastructure or required legal agreements are in place.

#### **Hospital Precinct Plan**

- 13.6.8.1. The Hospital Precinct Plan shall be a comprehensive document that will require approval by the City after consultation with the Region, Provincial agencies, TRCA and the public in its preparation. The Hospital Precinct Plan will require approval by the City but will not require further amendment to the Official Plan.
- 13.6.8.2. The Hospital Master Plan shall include:
- a. A Master Servicing Strategy that outlines the functional water distribution, waste water collection and stormwater management facilities that will service the entire Healthcare Campus Centre;
  - b. The establishment of appropriate boundaries of the Natural Areas within the Hospital Precinct, including plans to realign and improve the watercourse, and assess the extent of the current floodplain;
  - c. A Functional Transportation Plan that establishes the access points, internal street and driveway layout, expected traffic volumes, and the ability of both the internal rights of way and the surrounding road network to accommodate growth planned within the Hospital Precinct Plan area in relationship to the capacity of the entire Centre, including the identification of improvements that will be necessary; and identification of public transit, pedestrian and cycling routes and facilities;
  - d. A Community Energy Plan that sets out a strategy to enable the Hospital Precinct Plan area to reach goals of energy conservation, efficiency and the reduction of greenhouse gas emissions through consideration of such things as passive solar gains design, on-site energy generation, the use of “green” and white roofs, and

- the use of building materials and landscaping elements to provide shade or reflect or absorb heat to minimize energy consumption;
  - e. An Urban Design Framework that addresses urban structure, built form, massing, public realm including detailed open space and streetscape master plan and urban design including transit supportive design, sustainability and public art;
  - f. Investigation of potential archaeological resources;
  - g. A transition of height, massing, and intensity of land use, as well as means of buffering or screening to protect the low density neighbourhood to the north from adverse impacts of noise, shading, or inappropriate overview;
  - h. More specific location of differing land uses, including location of service facilities and major utilities;
  - i. Delineation of the boundary of the Hospital Precinct Plan area that will be planned as a health campus of care and lands that are surplus to that use that will be subject to the Special Study; and,
  - j. Proposed phasing of development.
- 13.6.8.3. Phasing will address the co-ordination of proposed development with the provision of infrastructure necessary to service that phase.
- 13.6.8.4. The approved Hospital Precinct Plan shall be used as the basis for Zoning.
- 13.6.8.5. Evaluation of the Hospital Precinct Plan shall be based on the Development Principles and Objectives of this Plan and its more specific policies.

### **13.6.9 Special Study**

- 13.6.9.1. The Special Study shall be a comprehensive document that will require approval by the City after consultation with the Region, Provincial agencies and the public in its preparation and will require a further amendment to the Official Plan. The Special Study will not be undertaken prior to Council approval of the Hospital Precinct Plan.
- 13.6.9.2. The Special Study shall include:
- a. The range and types of land use that will be permitted and specific formats that may be required or prohibited within specific areas;
  - b. A Servicing Strategy that outlines conformity with the Master Servicing Study with respect to the functional water distribution, waste water collection and stormwater management facilities that will service the Special Study Area;

- c. A Transportation analysis that includes an internal road or private roadway layout, with expected traffic volumes, that is able to demonstrate the ability of both the internal rights of way and the surrounding road network to accommodate growth planned within the Centre, including the identification of improvements that will be necessary;
- d. The identification of public transit, pedestrian and cycling routes and facilities;
- e. An investigation of potential archaeological resources;
- f. An Urban Design Framework that addresses urban structure, built form, massing, public realm including detailed open space and streetscape master plan and urban design including transit supportive design, sustainability and public art;
- g. A transition of height, massing and intensity of land use, as well as means of buffering or screening to protect the low density neighbourhood to the north from adverse impacts of noise, shading, or inappropriate overview;
- h. Determination of more detailed land uses, in relation to the servicing and transportation capacity of the site, and uses determined in the Hospital Master Plan, the adjacent low-rise neighbourhood and with respect to the intended function of a Centre;
- i. The compatible integration of land uses, pedestrian links and the vehicular system within the Hospital Precinct Plan; and,
- j. the proposed phasing of development.

13.6.9.3. Phasing will address the co-ordination of proposed development with the provision of infrastructure necessary to service that phase.

13.6.9.4. The approved Special Study shall be used as the basis for Zoning.

13.6.9.5. Evaluation of the Special Study shall be based on the Development Principles and Objectives of this Plan and its more specific policies.

### **13.6.10 Zoning & Holding By-laws**

13.6.10.1. In order to implement development within the Healthcare Campus Centre, Council will enact a By-law providing for zoning categories that are consistent with policies of this Plan and reflect the more detailed Hospital Precinct Plan and Special Study, as appropriate.

- 13.6.10.2. The City may, when enacting implementing zoning by-laws, designate a Holding Zone with the prefix “H” and specify the future uses of the land that are considered premature or inappropriate for development at this time due to anyone of the following reasons:
- a. the necessary studies outlined in this Plan have not been undertaken as contemplated by this Plan;
  - b. infrastructure such as water supply, sanitary sewers, stormwater management facilities, internal roads or access driveways, transit or external roads and necessary road and site improvements are not sufficient or have not yet been constructed to support the proposed development;
  - c. the number, design or location of vehicular access points to the site are not sufficient to function safely and effectively; and,
  - d. agreements have not been undertaken for site plan, funding of necessary infrastructure, or dedication of necessary land for parks, road widenings or other facilities necessary to support the proposed development.

### **13.6.11 Phasing**

- 13.6.11.1. The Hospital Precinct Plan shall be completed to the satisfaction of the City prior to undertaking the Special Study of the remainder of the lands.
- 13.6.11.2. Development applications may be phased in order to ensure that the necessary supporting infrastructure will be available to support the proposed development.

### **13.6.12 Interpretation**

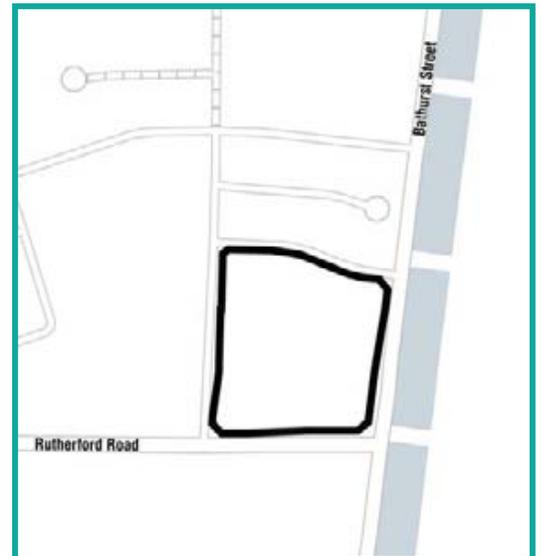
- 13.6.12.1. The areas of the Hospital Master Plan and the Special Study shown on Map 13.6.A are intended to show general areas. The boundary between these two areas will be established through the Hospital Precinct Plan exercise.
- 13.6.12.2. Site access points on Map 13.6.A are intended to show the general location. They may be adjusted in consultation with the Region of York, the City of Vaughan, and Ministry of Transportation, as appropriate.

## 13.7 Northwest Corner of Rutherford Road and Bathurst Street

### 13.7.1 Northwest Corner of Rutherford Road and Bathurst Street

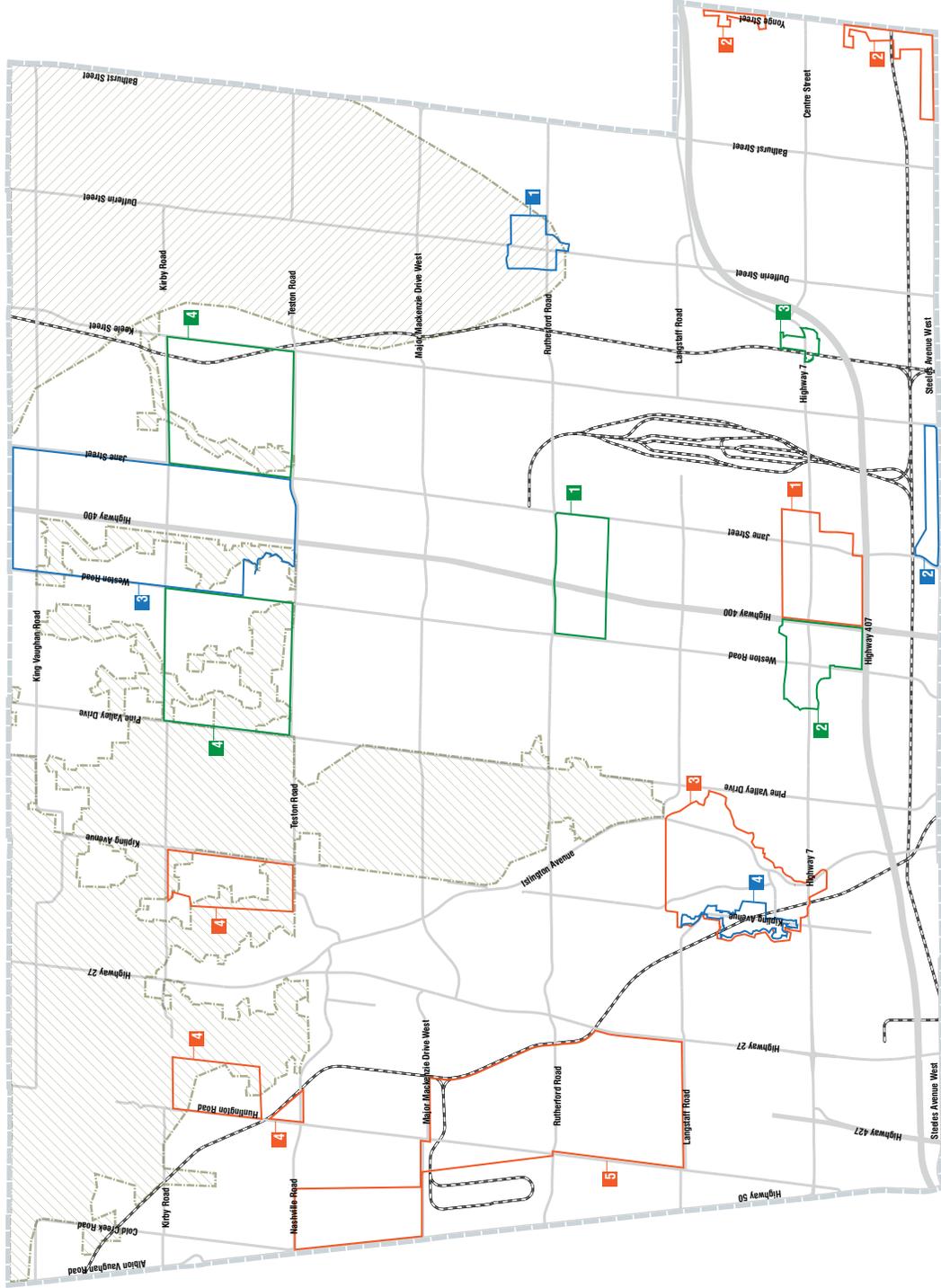
- 13.7.1.1. Notwithstanding policy 5.2.3.6, the maximum permitted Gross Leaseable Area shall be 26,800 square metres on the subject lands at the northwest corner of Rutherford Road and Bathurst Street, as shown on Map 13.7.A, being Block 23, on Plan 65M-3981, and comprising approximately 6.5 ha, in Part of Lot 16 Concession 2, City of Vaughan.

▼ Map 13.7.A:  
N-W Corner of Rutherford Rd. and Bathurst St.



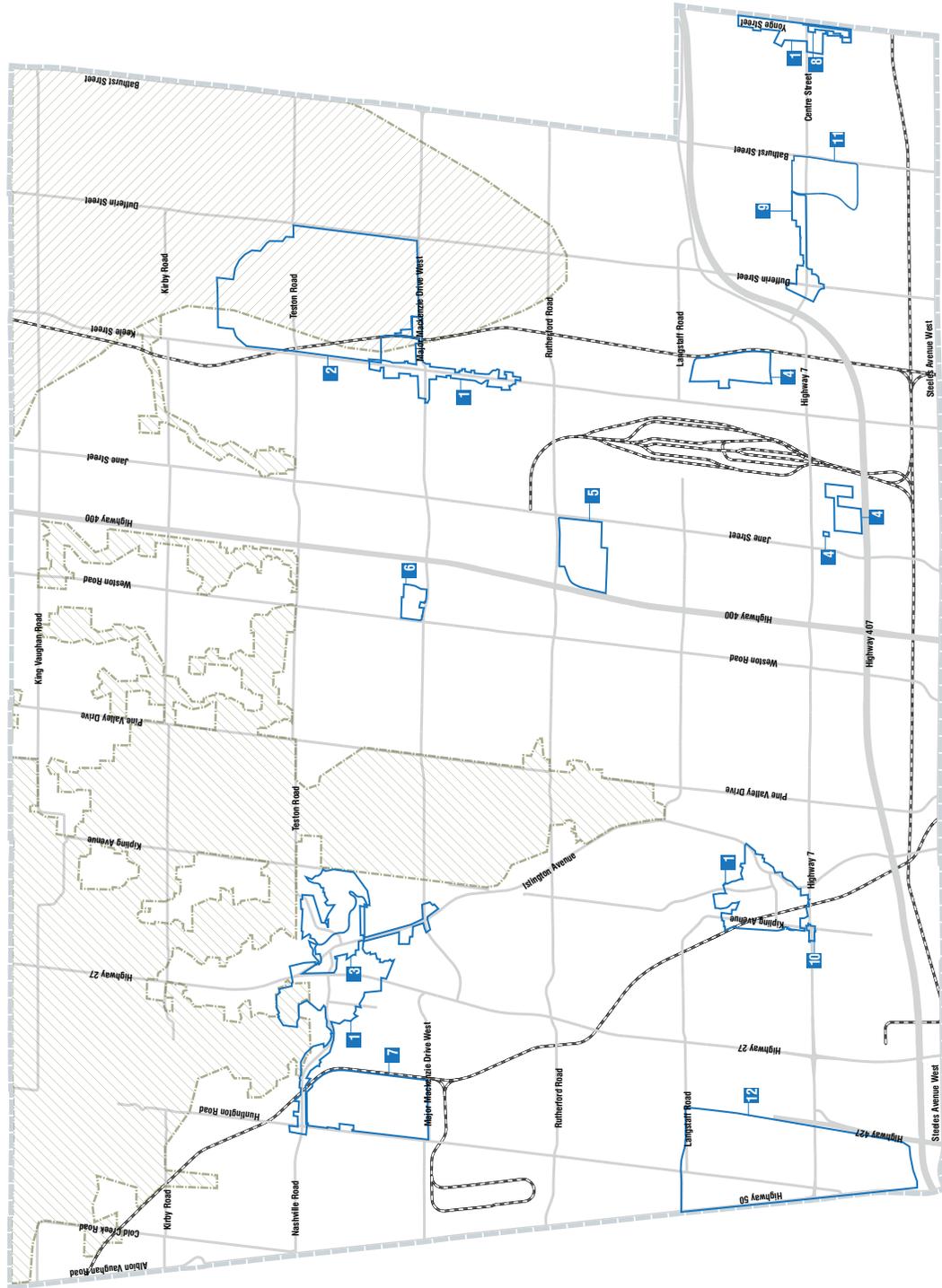
# SCHEDULE 14-A Areas Subject to Secondary Plans

- Secondary Plan Areas - Chapter 11
  - 1 Carrville Centre Secondary Plan - 11.2
  - 2 Steeles West Secondary Plan - 11.3
  - 3 Highway 400 North Employment Lands - 11.4
  - 4 Kipling Avenue - 11.5
- Secondary Plan Areas to be Approved
  - 1 Vaughan Metropolitan Centre
  - 2 Yonge Street Corridor
  - 3 Woodbridge Focused Area
  - 4 Kleinburg/Nashville Focused Area
  - 5 West Vaughan Employment Area
- Required Secondary Plan Areas
  - 1 Vaughan Mills Centre
  - 2 Weston Road and Highway 7
  - 3 Concord Centre
  - 4 New Community Areas (Blocks 41 & 27)
- Municipal Boundary



**SCHEDULE 14-B**  
**Areas Subject to**  
**Area Specific Plans**

- Area Specific Areas - Chapter 12
  - 1 Heritage Conservation Districts - 12.2
  - 2 Keele Valley Landfill Area - 12.3
  - 3 Kleinburg Core - 12.4
  - 4 Adult Entertainment Uses - 12.5
  - 5 Vaughan Mills Mall - 12.6
  - 6 N.E. Quadrant of Major Mackenzie Dr. and Weston Rd. - 12.7
  - 7 Block 61 West: "Nashville Heights" - 12.8
  - 8 Yonge Street Corridor in Thornhill - 12.9
  - 9 Centre Street Corridor - 12.10
  - 10 Kipling Avenue and Highway 7 - 12.11
  - 11 Bathurst and Centre Street - 12.12
  - 12 Huntington Business Park - 12.13
- Municipal Boundary



**SCHEDULE 14-C**  
**Areas Subject to**  
**Site Specific Plans**

- Site Specific Areas - Chapter 13
- 1 SW Corner of Bathurst St. and Teston Rd. - 13.2
  - 2 1125 Highway 50 - 13.3
  - 4 7242 Highway 27 - 13.4
  - 3 Thornhill Liberty Lands - 13.5
  - 5 Vaughan Healthcare Campus - 13.6
  - 6 NW Corner of Rutherford Road and Bathurst Street - 13.7

